

**COUNCIL MINUTES**  
**MONDAY 22 AUGUST 2016**

---

Cr Tini Athanasopoulos returned to the chamber at 7.35pm during discussion of Item 7 – Submission to the Yarra River Ministerial Advisory Committee.

**8        HAWKSBURN VILLAGE STRUCTURE PLAN**

**MOTION:**                **MOVED CR JOHN CHANDLER    SECONDED CR MATTHEW KOCE**

***That Council:***

- 1. Adopts the Hawksburn Village Structure Plan at Attachment 4 with the following changes:***
  - Malvern Road is nominated as the preferred primary vehicle access to Woolworths supermarket and the adjoining public car park.***
  - Strengthened objectives to ensure that commercial delivery vehicles do not have an adverse impact on amenity, traffic flow and road safety.***
- 2. Authorise Council officers to make these amendments and any other minor changes to the adopted Hawksburn Village Structure Plan.***
- 3. Acknowledges the input from stakeholders, registered and interested parties involved in the preparation of the Hawksburn Village Structure Plan and advise them of Council's decision to adopt the Hawksburn Village Structure Plan at Attachment 4.***
- 4. Authorises Council officers to undertake additional analysis to test the impact of proposed upper level setbacks on adjoining residential land and inform preparation of draft planning controls for Hawksburn Village.***
- 5. Authorises Council officers to investigate opportunities for reduced height and setbacks for the Western Precinct fronting Malvern and Williams Roads during the preparation of planning controls to implement the structure plan.***

***Carried***

**9        PRINCES GARDENS MASTERPLAN**

**MOTION:**                **MOVED CR MELINA SEHR    SECONDED CR TINI ATHANASOPOULOS**

***That Council:***

- 1. Endorse the Princes Gardens Framework Plan***
- 2. Note that a draft Masterplan to be developed from the Framework Plan objectives***
- 3. Note community consultation process on the draft Masterplan to commence in***

## **8. HAWKSBURN VILLAGE STRUCTURE PLAN**

**Acting Manager City Strategy: Rachel Ducker**  
**General Manager Planning & Amenity: Stuart Draffin**

### **PURPOSE**

The purpose of this Report is:

- To update Council on key issues arising from consultation on the Draft Structure Plan.
- For Council to consider adopting the final Hawksburn Village Structure Plan.

This item was considered at the Council meeting of 1 August 2016. The report is now re-presented to Council for further consideration.

### **BACKGROUND**

#### **Structure Plan Process**

In November 2014 Council appointed David Lock Associates to prepare the Hawksburn Village Structure Plan in collaboration with Charter Keck Cramer (economic analysis) and One Mile Grid (transport).

The Structure Plan proposes to:

- Establish a 10-20 year strategic framework for the future development of the activity centre.
- Provide a framework for the integration of Council's services and programs, existing policies and strategies, and
- Consider the role of the centre in the context of the broader municipality and the region.

A summary of the key stages of the Structure Plan development is included in Attachment 1.

#### **Draft Hawksburn Village Structure Plan**

On 14 December 2015, Council endorsed the draft Hawksburn Village Structure Plan for consultation. Consultation was undertaken in March 2016 and included:

- A mail-out of a summary brochure and questionnaire to over 5,000 residents, business owners and employees within the area.
- Advertisements in the Stonnington Leader, InStonnington and on Facebook.
- Two intercept activities
- Resident meetings
- Online information including background reports, brochure, questionnaire, previous consultation reports and frequently asked questions.

Over 290 completed questionnaires were received which is considered a high response rate.

The feedback has been summarised into aspects of the draft Structure Plan people most liked, those which were least liked, and additional aspects raised (Table 1). Attachment 2 outlines a response to those aspects of the draft Structure Plan least liked.

**Table 1: Feedback on Draft Structure Plan**

<b>Aspects of the draft Structure Plan <i>most</i> liked</b>
Improvements to the public realm including creating new open space (54)
The preferred maximum height of three storeys in the May Road car park area
Retaining and being sympathetic to the heritage in the area (24)
Maintaining the small scale, fine grain 'village' character (23)
Having a plan for the future of the Village which will provide residents with greater clarity of future change (17)
Developing an underground car park at May Road (15)
<b>Aspects of the draft Structure Plan <i>least</i> liked</b>
The preferred maximum height limits, particularly 4 and 5 storeys (46)
Increasing development, people and congestion in Hawksburn Village (20)
Having a bike lane on Malvern Road (13)
That there is no plan to address current traffic congestion on Malvern and Williams Roads (12)
Any proposals to allow for more car parking (11)
Loss of on-street car parking spaces with no substitute (9)
<b>Additional feedback</b>
Participants frequently raised the need to address current congestion issues on Malvern Road and lack of car parking in the Village. Participants also highlighted that increased density in the area would only make parking and congestion worse and these issues need to be addressed.

The consultation report is included as Attachment 3 and is available on Council's website 'Connect Stonnington'.

## **DISCUSSION**

### **Recommended Changes to the Structure Plan**

Following the close of consultation, the Structure Plan has been reviewed, taking into consideration feedback received (refer to Attachments 2 and 3), consistency with State and Local Planning Policy and other Council strategies, and the ability of the Structure Plan to guide planning controls. Based on this review, the final Structure Plan has been prepared.

Attachment 4 contains a copy of the Final Structure Plan proposed for Council adoption.

The key changes from the consultation draft made to the Final Structure Plan include:

- **Layout and format** - a reduction in background text and clearer figures and diagrams.
- **Section interface diagrams and proposed setbacks** - revised for clarity particularly the response to heritage and non-heritage sites. No changes to overall heights have been made as the proposed design response is considered appropriate for the context.
- **Strategic Redevelopment Sites** - changed to 'opportunity sites' to reflect their role in the context of a neighbourhood centre.
- **Woolworths/May Road car park opportunity site** - revised to make it clear that a key development outcome being sought for these sites is additional public open space. A recommendation has been included to investigate applying a planning mechanism requiring further planning for this site and the adjoining Council car park to achieve improved access, design and use of public land.
- **Public open space and public realm improvements** - revised to make it clear that the preferred location for additional public open space is the Woolworths/May Road car park opportunity site. Public realm improvements will be investigated for the northern end of Westbourne Street instead of full or partial road closure to create open space in response to resident concerns.

- **Hawksburn Railway Station** - revised to highlight the opportunity to improve way-finding to the station and undertake public realm improvements to Hobson Street. A potential pedestrian crossing has been included from Barnsbury Road to the station.
- **Employment** - the objectives to retain ground and first floor employment space have been strengthened given the strong support received during consultation.

### Planning Controls

Once the Structure Plan is adopted, a planning scheme amendment will be prepared to introduce new planning controls to implement the Structure Plan. When preparing these planning controls, the following matters will be considered:

- **The appropriateness of the current zoning of land within the activity centre.** Land is currently zoned Commercial 1, Mixed Use, Public Use, Residential Growth, General Residential and Neighbourhood Residential which all have different purposes and controls. A zone or height control change will be considered for the Howitt Street area within the activity centre boundary (currently 9 metres maximum under the General Residential Zone) for consistency with the Structure Plan. A Development Plan Overlay will be considered for the Woolworths and May Road car park opportunity site to influence the redevelopment of this site.
- **Management of development adjacent to the sensitive areas** (heritage, adjacent residential areas and neighbourhood character areas) through the use of built form controls.
- **Encouraging the retention of small-scale employment spaces** on first floors. Further investigation into a mechanism to retain the current level of employment floorspace will be undertaken.
- **Development of heritage buildings.** The Structure Plan refers to Council's heritage policy and guidelines as the most appropriate tool to manage appropriate development outcomes including upper level setbacks above the street wall. Exhibition of Amendment C132 (Heritage policy and guidelines) has recently concluded. Council is also progressing Amendment C223 (Glenferrie Rd and High St Activity Centre) which seeks to manage the development of heritage buildings. Consideration will be given to the outcomes of both of these amendments and the appropriateness of the use of Council's Heritage policy and guidelines for the Hawksburn Village context.

A planning scheme amendment to introduce new planning controls for Hawksburn Village is proposed to be presented to Council for consideration in early 2017.

### POLICY IMPLICATIONS

The proposed Structure Plan is consistent with the following Council Plan (2013-2017) objectives and strategies:

- **Liveability** - the most desirable place to live, work and visit. It aims to provide for population and housing growth while maintaining amenity, preserving character and improving access and public spaces.
- **Prosperity** - a prosperous community, recognised as a creative city, and a premier visitor and retail destination. The Structure Plan plans for long term sustainability and improvements in quality of retail precincts within an activity centre.
- **Environment** - a cleaner, safer and better environment for future generations to enjoy. The Structure Plan supports a shift towards the use of sustainable transport options and aims to increase the amount of quality open space.



The Structure Plan is consistent with the Metropolitan Planning Strategy (Plan Melbourne) which states that neighbourhood centres are to provide neighbourhood access to local goods, services and employment opportunities.

The Structure Plan is also consistent with Clause 21.04-1 of the Local Planning Policy Framework. This clause identifies future strategic work to achieve a network of sustainable and viable activity centres including preparing structure plans to guide future use and development of neighbourhood activity centres.

#### **FINANCIAL AND RESOURCES IMPLICATIONS**

The costs of preparing and consulting on the Hawksburn Village Structure Plan are consistent with the allocated budget for structure plans.

Indicative timeframe for preparation and exhibition of planning controls:

<b>August 2016</b>	<b>Late 2016</b>	<b>February 2017</b>	<b>Mar 2017</b>	<b>July 2017</b>
Adopt Structure Plan	Prepare planning controls	Authorisation	Exhibition	Panel

#### **CONCLUSION**

Preparation of the Hawksburn Village Structure Plan has been informed by three stages of consultation with residents, business owners and stakeholders. Feedback on the content of the Structure Plan has generally been positive.

Some changes have been made to the Structure Plan in response to community feedback and further analysis.

Following adoption of the Structure Plan, a planning scheme amendment including planning controls to implement the objectives of the Structure Plan will be prepared. The draft planning scheme amendment will be brought to Council separately with a recommendation to commence a planning scheme amendment process. A planning scheme amendment process will invite further submissions on the proposed planning controls for Hawksburn Village.

#### **HUMAN RIGHTS CONSIDERATION**

This recommendation complies with the Victorian Charter of Human Rights and Responsibilities Act 2006.

#### **ATTACHMENTS**

- |   |          |
|---|----------|
| 1. Attachment 1 - Summary of key stages | Excluded |
| 2. Attachment 2 - Key issues raised     | Excluded |
| 3. Attachment 3 - Consultation Report   | Excluded |
| 4. Attachment 4 - Final Structure Plan  | Excluded |

## **RECOMMENDATION**

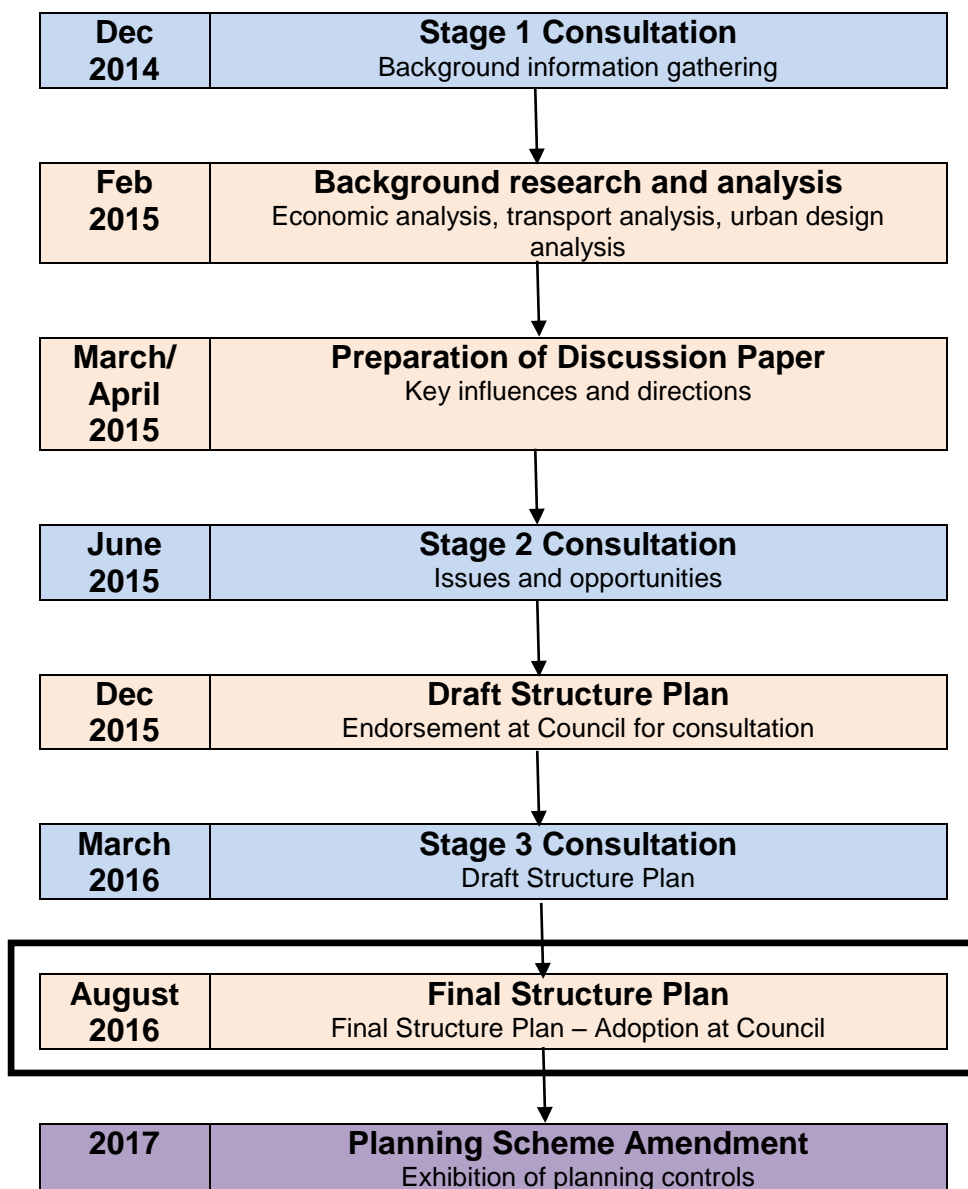
### ***That Council:***

- 1. Adopts the Hawksburn Village Structure Plan at Attachment 4.***
- 2. Authorise Council officers to make minor changes to the adopted Hawksburn Village Structure Plan.***
- 3. Acknowledge the input from stakeholders, registered and interested parties involved in the preparation of the Hawksburn Village Structure Plan and advise them of Council's decision to adopt the Hawksburn Village Structure Plan at Attachment 4.***

Item 8

Attachment 1 Attachment 1 - Summary of key stages

## The Structure Plan Process



Item 8

Attachment 2 Attachment 2 - Key issues raised

## Draft Structure Plan Feedback – Key Issues Raised

Feedback on what was least liked about the Draft Structure Plan?	Response
<p>Concern for the impact of the preferred maximum height limits, particularly 4 and 5 storeys (46 participants)</p>	<p>The Structure Plan provides a reasonable balance between planning objectives which seek to direct growth to activity centres (appropriate to the context) away from the residential hinterland, close to public transport and protecting a reasonable level of amenity for residents.</p> <p>A maximum of 4 storeys in the eastern precinct is proposed, stepping down to a 2 storey street wall with setbacks at the upper levels and at the rear. A maximum of 3 storeys in the eastern precinct May Road car park area is proposed, also stepping down to a 2 storey street wall. This is consistent with the neighbouring residential zone (Neighbourhood Residential Zone) which has a maximum height limit of 9 metres.</p> <p>A maximum of 5 storeys in the western precinct is proposed stepping down to a 2 or 3 storey street wall with setbacks at the upper levels and at the rear. This is consistent with recent development in the area.</p> <p>In both precincts, the primary volume of heritage buildings is to be retained excluding the Leonard Joel Auction House which is an A1 graded place and of state significance.</p>
<p>Concern for the impact of increasing development, people and congestion in the Village (20)</p>	<p>The Structure Plan is consistent with state and local policy that directs growth to activity centres. In response to feedback, the built form and heritage objectives have been amended to ensure the intent to protect heritage is stronger and to ensure the character and amenity of adjacent residential areas is maintained. The Structure Plan also includes an objective to reduce private vehicle travel over time.</p>
<p>Concern about the impact of having a bike lane on Malvern Road (13)</p>	<p>An action in the Implementation Plan is to liaise with VicRoads to advocate for and investigate the opportunity to install peak hour bicycle lanes to make cycling an option more desirable from a safety perspective. Making it easier and safer for people to cycle to and from activity centres can encourage more people over time to cycle and reduce local traffic in the area, reducing overall congestion.</p> <p>Earlier feedback on the issues and opportunities of the Structure Plan also suggested cyclists were using the footpaths of Hawksburn Village instead of Malvern Road for safety which resulted in pedestrian/cyclist conflict and accidents.</p>
<p>That there was no plan to address current traffic congestion on Malvern and Williams Roads (12)</p>	<p>The Hawksburn Village neighbourhood centre is located at the intersection of Williams and Malvern Roads.</p> <p>Malvern Road is nominated as a Tram and Bus Priority Route and Pedestrian Priority Route in the Stonnington Planning Scheme. It is also a VicRoads Primary Arterial Road (or high capacity urban road) and Bicycle Priority Route.</p> <p>A key issue identified by the Structure Plan is the limited cross-section of Malvern Road and the ability to accommodate its multi-functional role. The Implementation Plan includes actions to liaise with VicRoads and Yarra Trams to capitalise on future works (DDA compliant tram stops) and existing infrastructure to decrease single occupancy vehicle trips over time.</p> <p>All future development proposals will be assessed in accordance</p>

	with the provisions of the planning scheme (and future planning controls for Hawksburn Village) which support the provision of sustainable transport alternatives to the car, and the provision of an appropriate number of car parking spaces having regard to the demand likely to be generated.
Any proposals to allow for more car parking (11)	<p>As above, the Structure Plan is consistent with state and local policy that directs growth to activity centres. In the short term, all future development proposals will be assessed in accordance with the provisions of the planning scheme (and future planning controls for Hawksburn Village) which support the provision of an appropriate number of car parking spaces having regard to the demand likely to be generated.</p> <p>In the long term, State and Local Policy and the Structure Plan encourages sustainable transport alternatives to the car.</p>
Loss of on-street car parking spaces with no substitute (9)	There is an action in the Implementation Plan to investigate the ability for Malvern Road to accommodate bicycle lanes during peak hour in the long-term which may result in a temporary loss of on-street parking during peak hours. This project is subject to detailed investigation and further consultation. The wider car parking requirements for Hawksburn Village would also be considered.
Concern regarding the proposal to investigate closing the northern end of Westbourne Street to create more open space (8)	The Structure Plan has been revised to suggest public realm improvements at the northern end of Westbourne Street in response to feedback. There is a shortage of public open space in the Hawksburn Village area, in particular the eastern precinct. The Structure Plan has been revised to suggest that the best location for additional public open space is in the Woolworths/May Road car park opportunity site.
Concern regarding loss of car parking as a result of having an accessible tram stop (7)	The majority of respondents (53%) were in support of advocating to State Government for quality, integrated and accessible design of their disability compliant tram stops. PTV is committed to providing an accessible public transport system within Victoria and will continue to implement accessible tram stops throughout the network. Should accessible tram stops become a PTV priority in Stonnington, there is an opportunity to ensure their design considers the character of the area and is integrated with local movement aspirations as outlined in the Structure Plan.
Concern regarding the impact of a Laneway Precinct (McKillop Street) on neighbourhood character and vandalism (5)	<p>The majority of respondents (69%) were in support of enhancing McKillop Street as a laneway precinct to promote small business and add character to Hawksburn Village. The proposal suggests that as buildings in McKillop Street develop, uses could face and spill out into the Street. Public realm improvements are also suggested.</p> <p>This proposal is subject to further investigation and consultation, however the objective is to create a unique space and destination off the main thoroughfare of Malvern Road.</p>

Item 8

Attachment 3 Attachment 3 - Consultation Report







STAGE 3 CONSULTATION REPORT

# Hawksburn Village Structure Plan

## Privacy

Capire Consulting Group and any person(s) acting on our behalf is committed to protecting privacy and personally identifiable information by meeting our responsibilities under the Victorian Privacy Act 1988 and the Australian Privacy Principles 2014 as well as relevant industry codes of ethics and conduct.

For the purpose of program delivery, and on behalf of our clients, we collect personal information from individuals, such as e-mail addresses, contact details, demographic data and program feedback to enable us to facilitate participation in consultation activities. We follow a strict procedure for the collection, use, disclosure, storage and destruction of personal information. Any information we collect is stored securely on our server for the duration of the program and only disclosed to our client or the program team. Written notes from consultation activities are manually transferred to our server and disposed of securely.

Comments recorded during any consultation activities are faithfully transcribed however not attributed to individuals. Diligence is taken to ensure that any comments or sensitive information does not become personally identifiable in our reporting, or at any stage of the program.

Capire operates an in-office server with security measures that include, but are not limited to, password protected access, restrictions to sensitive data and the encrypted transfer of data.

For more information about the way we collect information, how we use, store and disclose information as well as our complaints procedure, please see [www.capiire.com.au](http://www.capiire.com.au) or telephone (03) 9285 9000.

## Stakeholder Engagement

Unless otherwise stated, all feedback documented by Capire Consulting Group and any person(s) acting on our behalf is written and/or recorded during our program/consultation activities.

Capire staff and associates take great care while transcribing participant feedback but unfortunately cannot guarantee the accuracy of all notes. We are however confident that we capture the full range of ideas, concerns and views expressed during our consultation activities.

Unless otherwise noted, the views expressed in our work represent those of the participants and not necessarily those of our consultants or our clients.

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# Contents

1	Introduction	5
1.1	Project background	5
1.2	Purpose of report	6
1.3	Limitations	6
2	Key findings	9
3	Community engagement	11
3.1	Overview of activities	11
3.2	Who participated	13
4	Detailed findings	15
4.1	Built form and heritage	15
4.2	Land use activities	17
4.3	Access and movement	18
4.4	Public realm and open space	19
4.5	General support and additional themes raised	20
	Appendix	22

# 1 Introduction

The City of Stonnington (Stonnington) is preparing a Structure Plan (the Plan) for Hawksburn Village, *'Hawksburn. The Village. The Future'*. The Plan will guide how change and growth is managed in Hawksburn Village (the Village) over the next 20 to 30 years.

## 1.1 Project background

The Plan will consider environmental, social and economic sustainability of the Village. It will include directions for access, movement, land use activities, public realm, open space, built form and heritage. Once the final Plan has been adopted by Council, it will be incorporated into the Stonnington Planning Scheme through a formal amendment process.

Four stages of community consultation will inform the preparation of the Plan:

- Stage 1 - Information gathering and visioning (December 2014)
- Stage 2 - Key directions (June 2015)
- Stage 3 - Draft Structure Plan (March 2016)
- Stage 4 - Statutory exhibition (2016/2017)

The first two stages of consultation informed the preparation of the Draft Plan and established the following shared vision for the Village in 2040:

**“Hawksburn Village will continue to thrive as a welcoming and vibrant centre of quality retailing known for its distinctive charm and village feel, and safe, green, and walkable street network.”**

In Stage 3 of consultation the community were invited to provide feedback on the Draft Plan via survey, submission and resident meetings. Input was sought from residents, traders, visitors, landholders and other key stakeholders from 1 March to 31 March 2016.

The feedback received during this stage will help to finalise the Plan and prepare draft planning controls for the centre. This will be subject to a public exhibition stage in 2016/2017.



## 1.2 Purpose of this report

Capire Consulting Group (Capire) was engaged by Stonnington to support consultation activities and prepare a report for Stage 3 of consultation. The purpose of this Report is to provide a summary of the community consultation that took place in Stage 3 and feedback received on the Draft Plan via survey and email submissions. Any comments made during resident meetings and social media are not included in the following analysis.

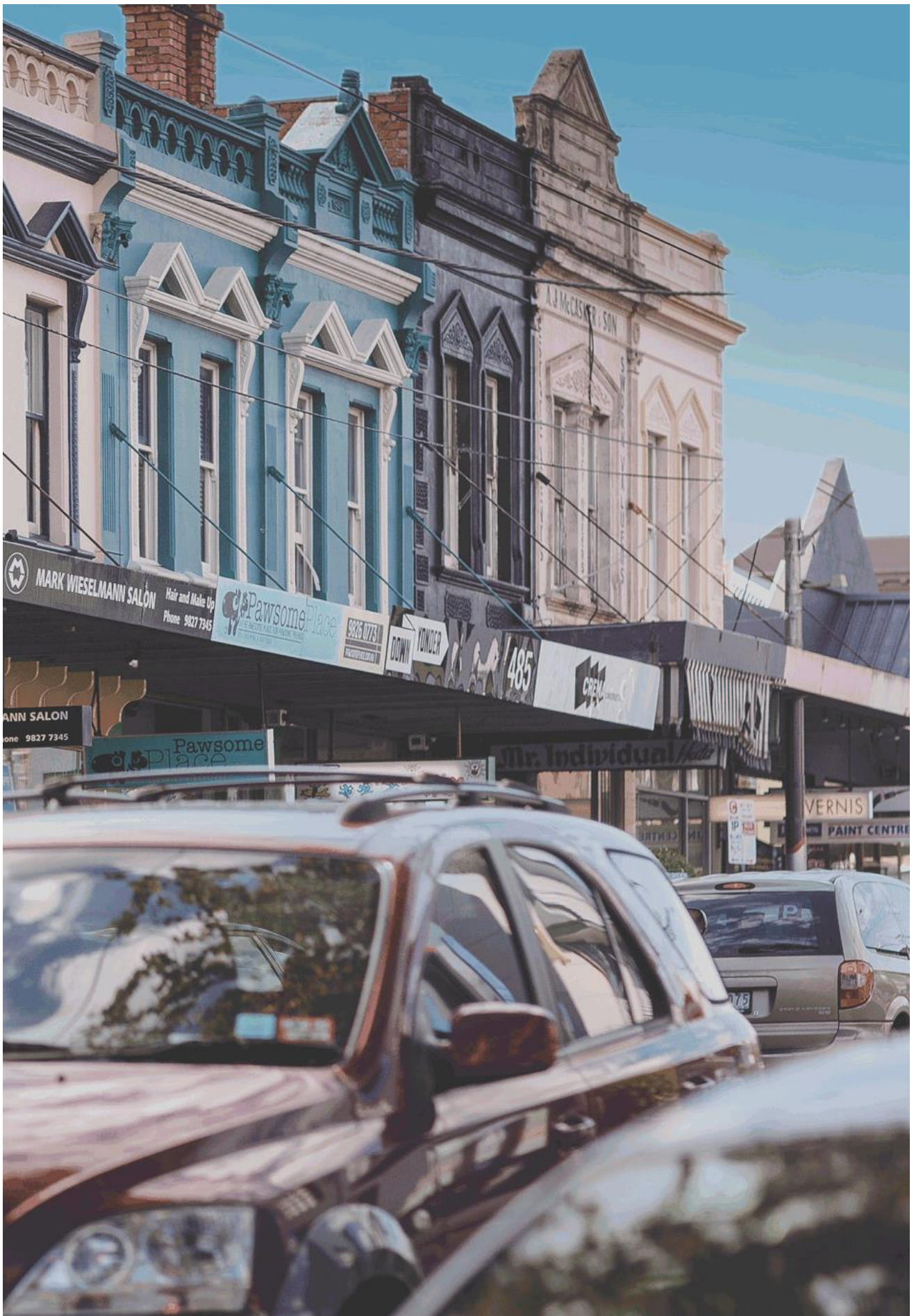
Seventeen surveys were received after the close of the consultation period. The feedback from these surveys is included in Appendix A.

## 1.3 Limitations

As with all consultation programs, there were limitations associated with the community engagement that should be acknowledged. These are:

- Not all participants chose to respond to all questions. As such, some surveys have incomplete responses and some questions received more responses than others. All responses for each question have been included in the findings of this report.
- Participants had varying levels of understanding of the planning process. This may influence a participant's capacity to answer some questions, particularly in the self-guided online and household surveys.
- There was an error in the online survey for the question about overall support for the Draft Plan. The question appeared twice. This error was accounted for in the analysis of results by taking the percentage from the total number of responses to this question. Only 20 per cent of surveys were completed online, minimising the impact of this error.

Despite the limitations, we are confident that the findings contained within this Report accurately reflect feedback provided by the participants throughout the consultation.







## 2 Key findings

Feedback on the Draft Plan was provided by 304 people, predominantly local residents. Overall there was support for having a Plan to direct change and growth, particularly as it will provide greater clarity for residents in the future.

**The key findings were:**

- The proposal to have more open space and greenery in Hawksburn Village received the greatest level of support from participants.
- The proposed preferred height limits were the most contentious element of the Draft Plan. While there were participants who supported increasing heights above the proposed limits, a larger proportion of participants wanted to see proposed height limits reduced. Their preferences were three storeys in the eastern section of the Village and four in the western section.
- The impact of increased density on neighbourhood character, sunlight and traffic congestion were the most common reasons given for opposition to the proposed preferred height limits.
- Participants consistently felt that retaining heritage character is important. However, they would like to have greater clarification about what 'sympathetic additions' would entail. Some participants felt that the industrial character was not worth retaining.
- Access and movement directions that might impede traffic flow such as bicycle lanes, an accessible tram stop and a pedestrian crossing were less likely to be supported than other proposed directions.
- There was universal support for retaining small scale employment in Hawksburn Village. There was also a high level of support for enhancing McKillop Street as a 'Laneway Precinct'.
- Proposed improvements to public realm and open space were strongly supported, however participants occasionally raised concerns about maintenance of open spaces and reduced traffic access.
- Participants frequently noted that directions for managing traffic congestion and car parking pressures were not provided in the Draft Plan.





## 3 Community engagement

### 3.1 Overview of activities

Consultation activities for Stage 3 of consultation on the Draft Structure Plan for Hawksburn Village took place between 1 March and 31 March 2016.

Feedback was collected through a survey that was available online and in hard copy. Stonnington also prepared a Brochure and Frequently Asked Questions sheet which included information on the process, how people could provide their feedback, the previous consultation stages and how the Draft Plan responds to community input.

**Table 1: Engagement activities that took place in Stage 3 - Draft Structure Plan**

ENGAGEMENT ACTIVITIES	DESCRIPTION
<b>Promotion</b>	<p>Advertisements for Stage 3 consultation activities were distributed in the:</p> <ul style="list-style-type: none"> <li>• <i>Stonnington Leader</i> - 2 March 2016</li> <li>• <i>InStonnington</i> - March/April edition</li> <li>• <i>Stonnington Facebook Page</i> – 3 March 2016 (23,833 people reached)</li> </ul>
<b>Survey mailout</b>	<p>A hard copy of the survey and brochure was mailed out to 5,045 residential and business owners and occupiers within a five-minute walk of the Village.</p> <p>Community members were encouraged to complete the survey and return it by mail or drop it off at the Stonnington City Centre. 243 completed surveys were returned.</p>
<b>Online</b>	<p>An online hub was available throughout the consultation at <a href="http://www.connectstonnington.vic.gov.au/hawksburn">www.connectstonnington.vic.gov.au/hawksburn</a>. The following was accessible through the hub:</p> <ul style="list-style-type: none"> <li>• Frequently Asked Questions</li> <li>• Background reports including urban design, economic and transport analysis, and discussion paper</li> <li>• Full Draft Plan</li> <li>• Brochure</li> <li>• Online and hard copy survey</li> <li>• Consultation reports for Stages 1 and 2</li> <li>• Timeline</li> </ul> <p>In total, the online hub was visited 773 time during Stage 3. Information about the project was downloaded by 308 visitors.</p>

ENGAGEMENT ACTIVITIES	DESCRIPTION
<b>Face-to-face activities</b>	<p>Two intercept activities were held during Stage 3.</p> <ul style="list-style-type: none"> <li>• Tuesday 22 March 2016, 5.30–7.30pm, Hawksburn Woolworths Supermarket, 2x Capire &amp; 1x Stonnington staff.</li> <li>• Thursday 24 March 2016, 8.30–10.30am, Cromwell Park, 365 Malvern Road, 2x Capire, 1x David Lock Associates, 3x Stonnington staff.</li> </ul> <p>The intercept activities were an opportunity for community members to learn more about the Draft Plan, complete a survey or take a hard copy survey and brochure with them.</p> <p>A Capire staff member visited every business in the Hawksburn Village during the intercept activities to inform staff about the Draft Plan and how they could provide feedback. Most businesses had already received a survey in the mail and staff/owners took the opportunity to ask further questions.</p>
<b>Resident meetings</b>	<p>Residents were able to request a meeting with council officers to discuss the Draft Plan. Three meetings were held.</p>
<b>Email submissions</b>	<p>Community members were able to provide feedback via email. Eight email submissions were received.</p>

Stonnington has continued to liaise with key stakeholders throughout the stages of consultation including Public Transport Victoria, Yarra Trams, VicRoads and the Department of Lands Water and Planning.

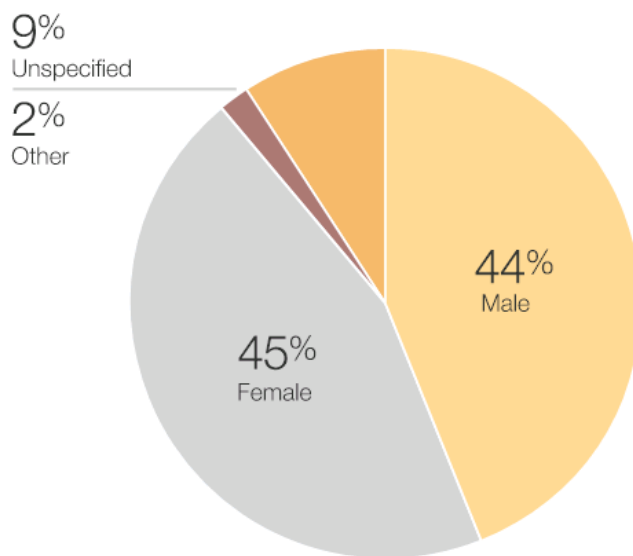
### 3.2 Who participated

Altogether, 304 people participated in Stage 3:

- 293 people took part in the survey,
- 8 people provided submissions, and
- 3 people had resident meetings.

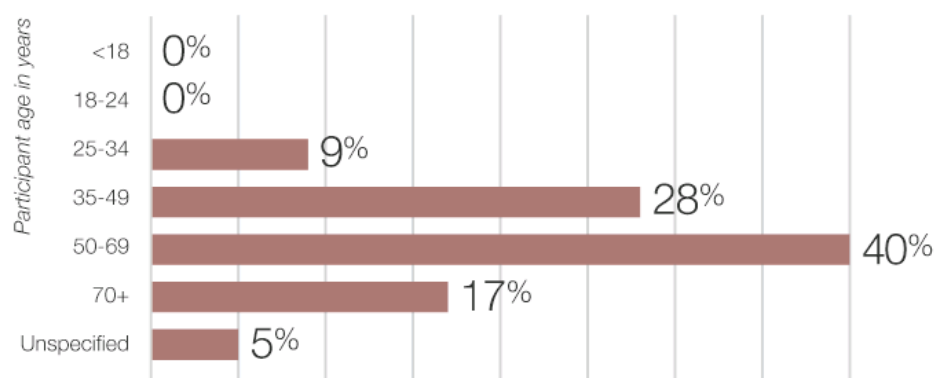
Gender, age and relationship to Hawksburn Village was collected in the survey. There was an even distribution of male and female participants, as illustrated in Figure 1.

**Figure 1: Participants by gender**



As illustrated in Figure 2, a large proportion of the participants were between 50-69 years of age.

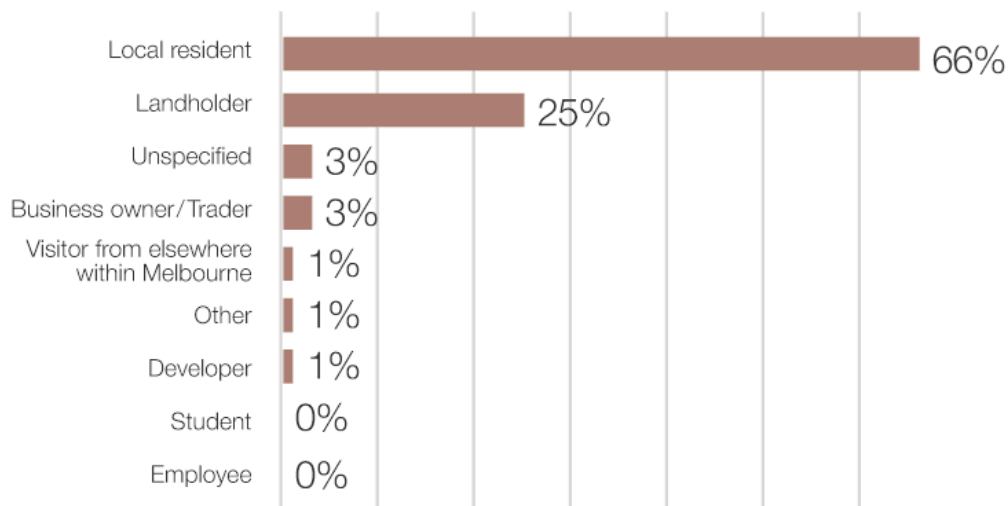
**Figure 2: Participants by age group**





Participants were asked to provide their relationship to the Village, with the option of selecting multiple options. Figure 3 shows that the majority of participants were local residents. No participants identified themselves as a student or employee of a local business.

**Figure 3: Participants relationship to Hawksburn Village**



Of the eight written submissions received during Stage 3, seven were prepared by local residents and/or landowners. One submission was received from a developer. Many people who provided a submission indicated that they had also completed a survey. The submissions generally expanded upon point made in their survey responses.



## 4 Detailed findings

The following section provides the detailed findings from the surveys and submissions. Findings are divided into the following four themes to reflect the survey questions:

- built form and heritage
- land use activities
- access and movement
- public realm and open space.

General support for the Draft Plan and any additional themes raised outside of the proposed directions in the the survey have also been included where relevant.

### 4.1 Built form and heritage

**Figure 4: Participant support for the proposed built form and heritage directions**

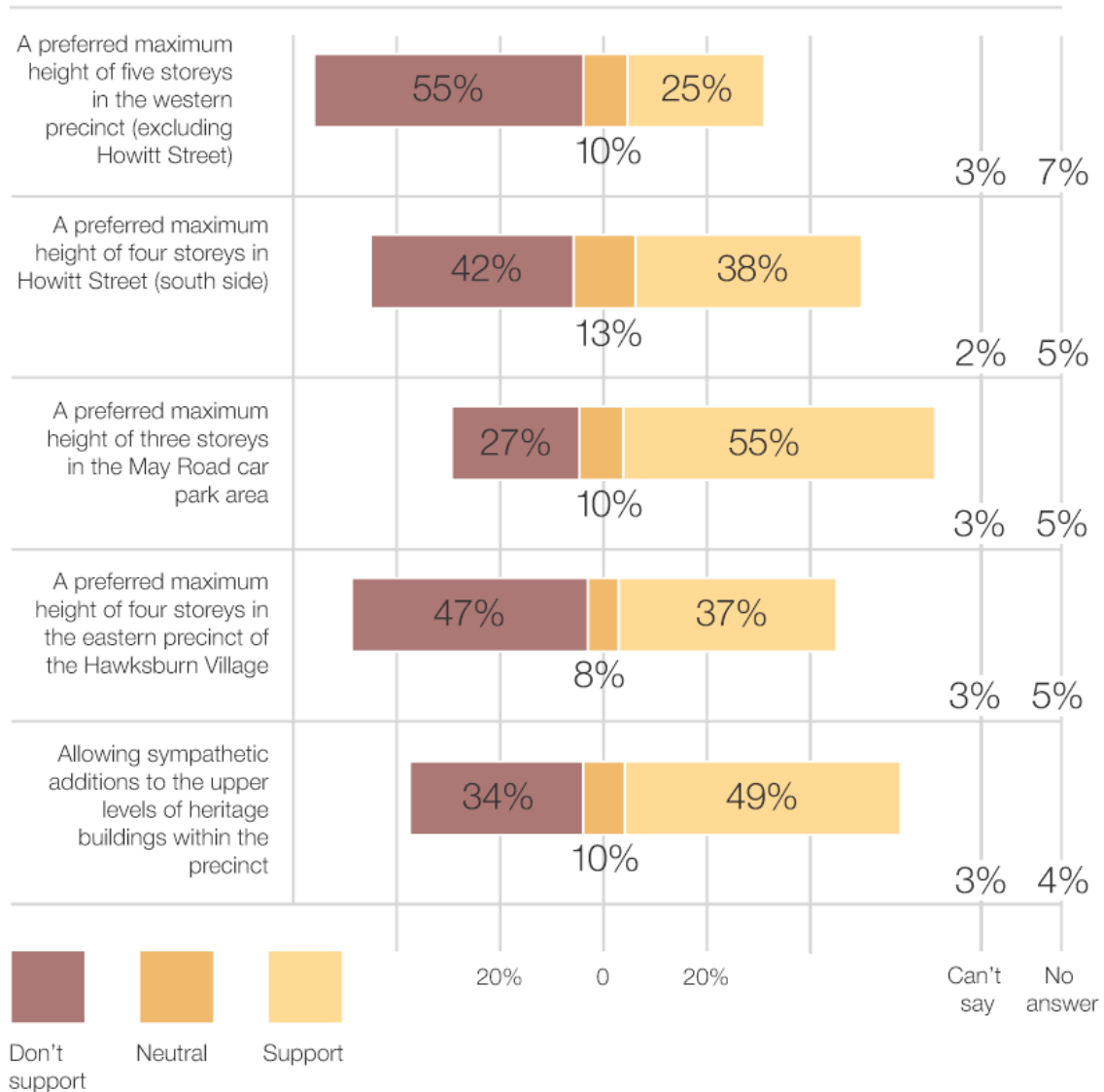




Figure 4 illustrates participants' levels of support for the built form and heritage directions proposed in the Draft Plan. Where participants provided justification for their response, this has been detailed below.

Participants frequently raised concerns around the flexibility in defining 'sympathetic additions', suggesting that altering heritage buildings in any way could impact the character of the neighbourhood. They wanted more information about what 'sympathetic' would entail, particularly with respect to maximum heights (a three storey maximum was suggested several times) and setbacks.

Many participants commented that a maximum height of three storeys was preferred for the eastern precinct of the Hawksburn Village, rather than four. Overshadowing and loss of village character were the primary concerns, others also raised increased traffic and congestion in the area. However, there was support for increasing the density of this precinct by several participants.

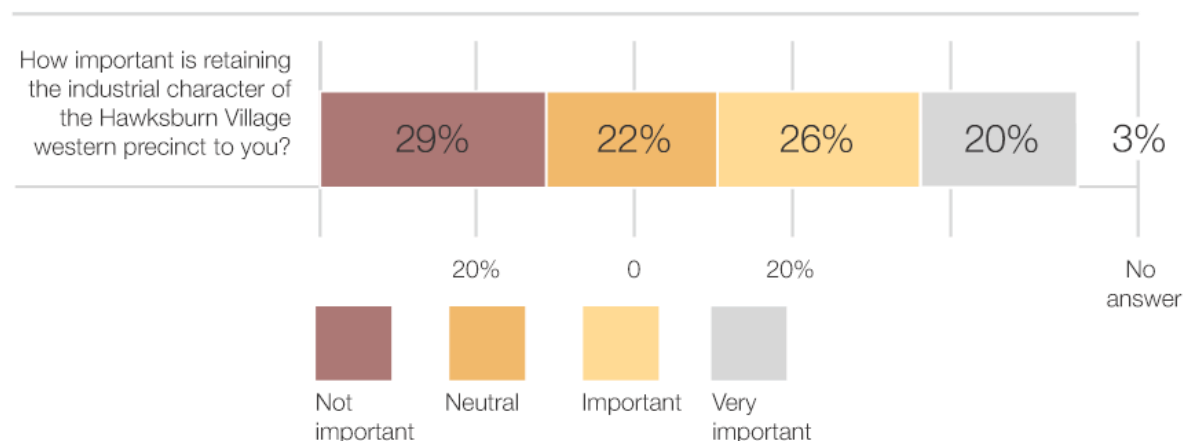
Most participants suggested a preferred height limit of three storeys in Howitt Street, rather than four. The main concern was that four storeys would affect the 'village atmosphere' of the street.

Concerns about potential impacts of a preferred height limit of five storeys in the western precinct could have on the village character of Hawksburn were raised. While increased density in the western precinct was generally supported, there were concerns around the quality and design of new developments, and provision of supporting infrastructure such as, public transport and car parking.

The impact that increased development could have on traffic congestion and car parking in Hawksburn Village was consistently raised. This was often cited as a reason for opposing heights.

Participant opinions were divided as to whether a height limit of three storeys is appropriate for properties facing May Road. Residents of May Road in particular were concerned about possible impacts that expanding the Woolworths car park would have on traffic in May Road. In their submissions participants suggested that a large car park structure could affect the neighbourhood character of the May Road area and that it is important to retain the native trees in the car park.

**Figure 5: Participant responses to the importance of retaining industrial character**

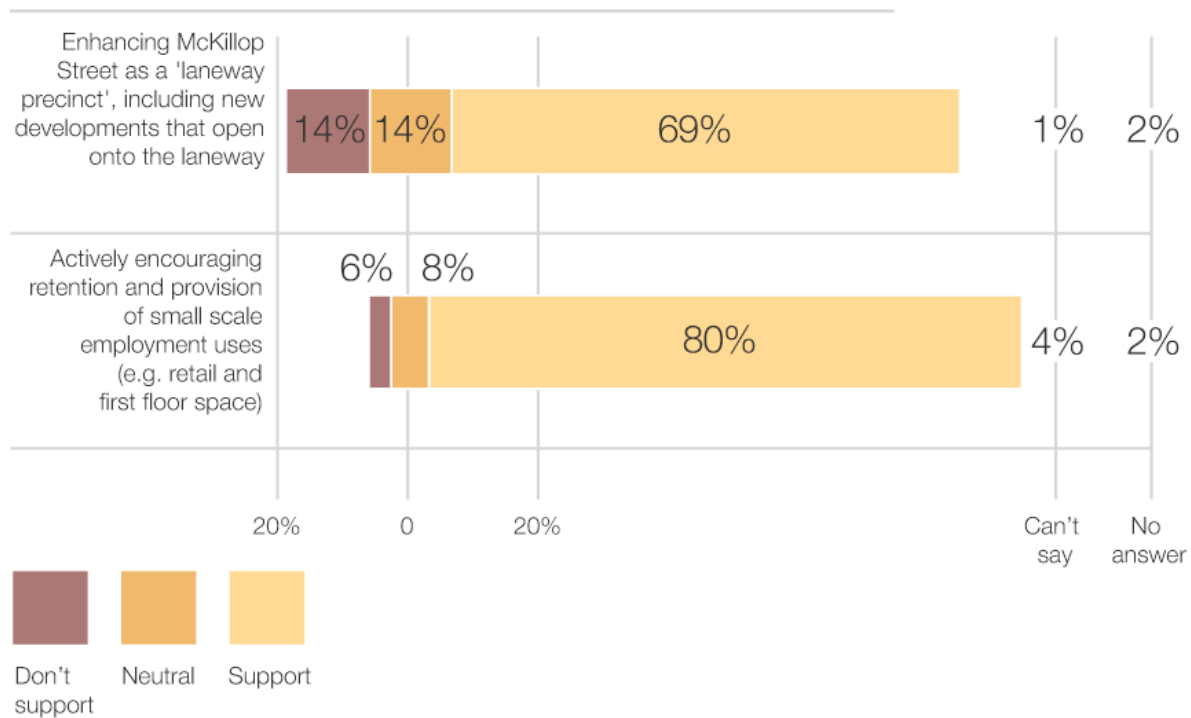


Participants had a range of views towards retaining the industrial character of the western precinct. Whilst many discussed the importance of preserving the industrial facades to keep the unique style and character of the precinct, several participants did not see these buildings as attractive enough to warrant preservation.

There were comments around the use and scale of future residential and retail developments. Participants discussed the importance of maintaining a fine grain with cafes and shops rather than large shopping mall style developments.

## 4.2 Land use activities

**Figure 6: Participant responses to the land use activity directions**

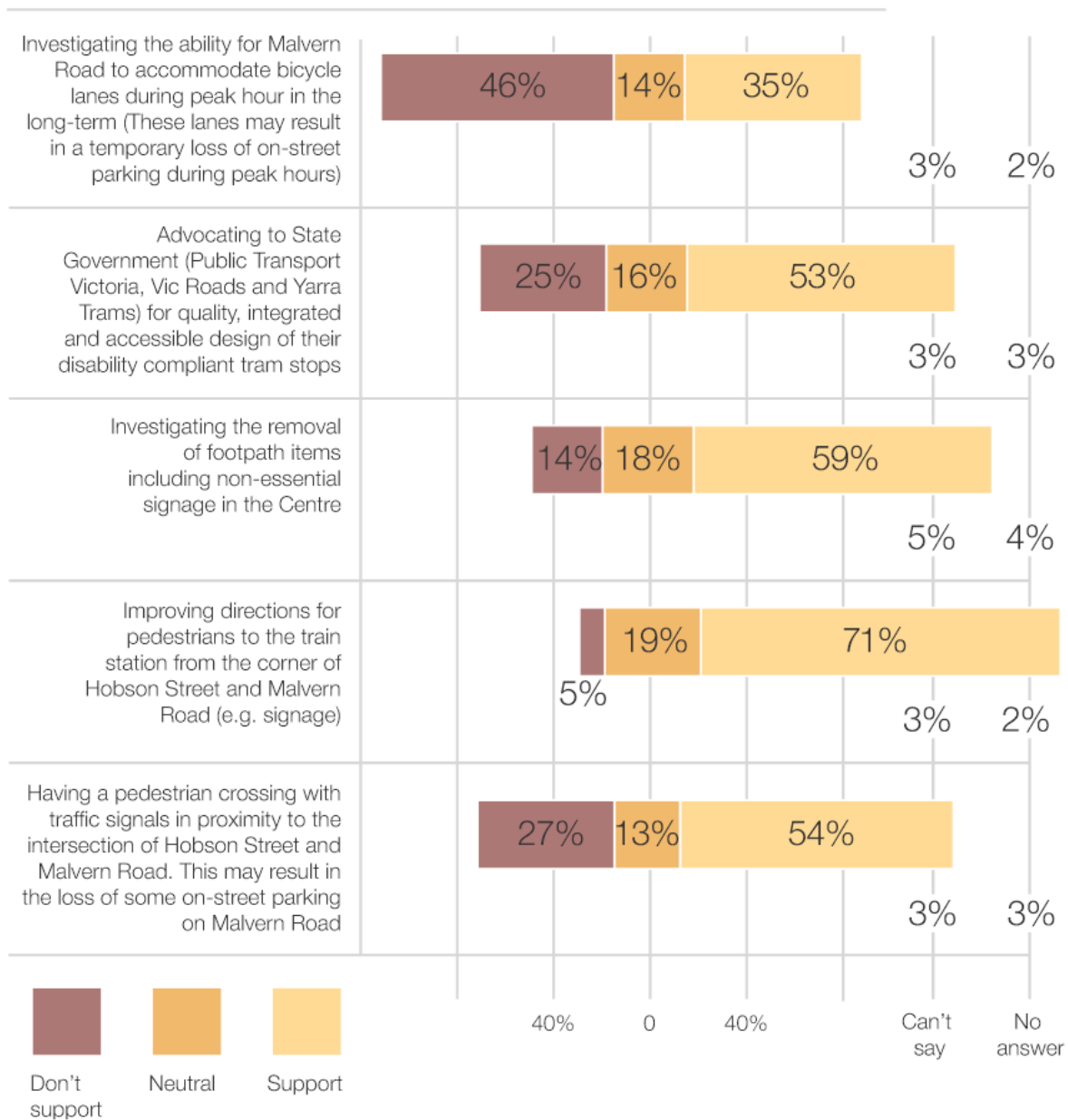


There was a high level of support for enhancing McKillop Street as a laneway precinct. Participants liked the idea that a laneway precinct could promote small businesses and add character to the village. The participants who did not support the proposal want to see McKillop Street remain residential as a commercial laneway could be unsympathetic to the existing neighbourhood character of the Village and encourage vandalism. A local business who provided a submission expressed concern about the loss of access to the rear of their business for waste removal and deliveries.

Actively encouraging retention and provision of small scale employment uses was strongly supported by survey participants. The small scale, intimacy and uniqueness of Hawksburn Village was viewed as an asset. The small number of people who did not support this proposal were divided in their reasons, some wanted to see demand drive land use in the Village, the others preferred only residential.

## 4.3 Access and movement

**Figure 7: Participant responses to the access and movement directions**



Seventy-one per cent of participant supported investigating the removal of footpath items. However, some business owners and residents were concerned that this could mean a loss of signage and outdoor dining space that could affect their business.

Participants were supportive of improving directions to the train station from the corner of Hobsons Street and Malvern Road. Several participants also suggested signage to the station to and from Williams Road.

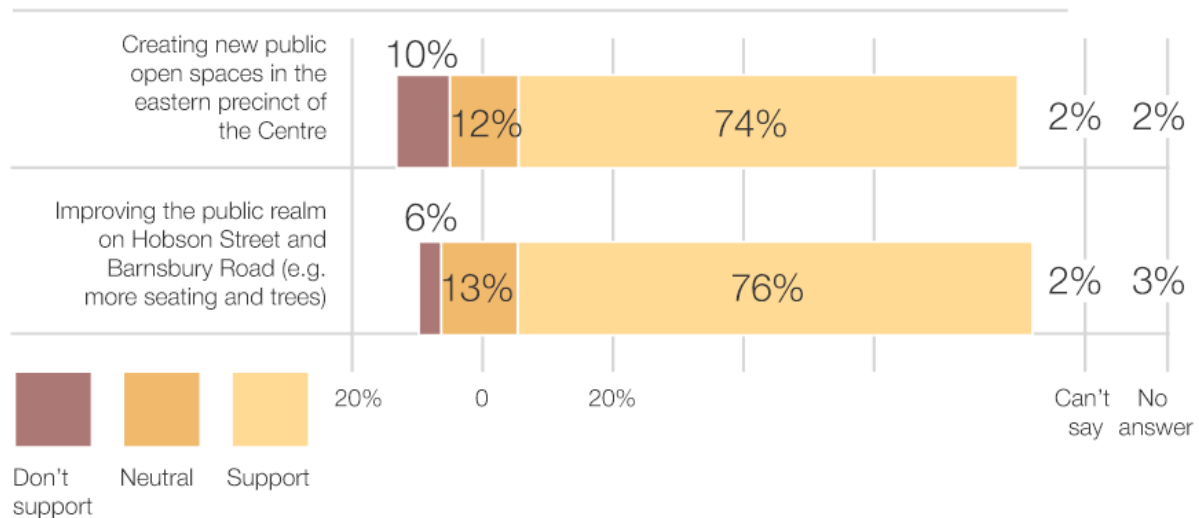
There was support for a pedestrian crossing with traffic signals in proximity to the intersection of Hobson Street and Malvern Road. Participants commented that this area is currently dangerous for pedestrians crossing Malvern Road and that traffic signals would improve safety. Those who did not support the crossing were concerned about increased traffic congestion and loss of on-street parking on Malvern Road.

Overall investigating the ability for Malvern Road to accommodate bicycle lanes was not supported. Whilst some participants commented that provision of bicycle lanes could encourage more cycling in the area and ease traffic, many believed that bicycle lanes would worsen vehicle congestion on Malvern Road.

The majority of participants supported advocating for accessible design for disability compliant tram stops. There were comments suggesting that well designed tram stops could encourage more public transport use to and from Hawksburn Village. The main concerns raised were around loss of on-street car parking spaces with no replacement proposed in the Draft Plan.

## 4.4 Public realm and open space

**Figure 8: Participant responses to the public realm and open space directions**



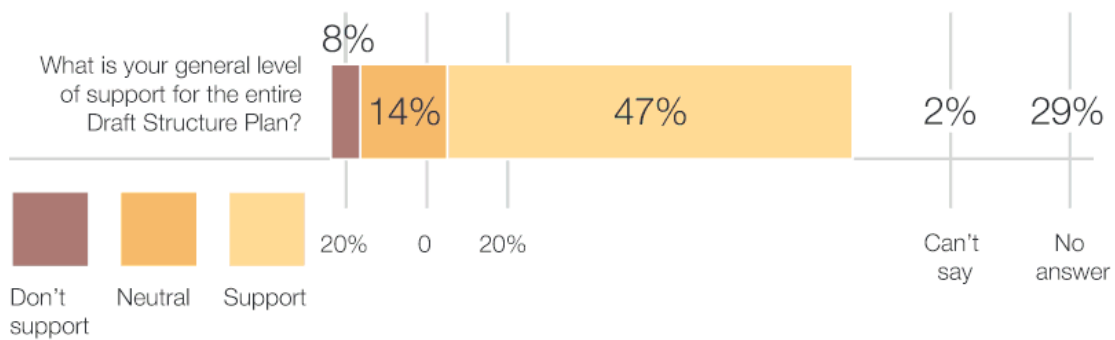
Creating a new public open space in the eastern precinct was strongly supported, particularly opportunities for green space such as a small park. However, several residents of Westbourne Street expressed concern about the proposal to close their Street. They were concerned about the traffic impact and anti-social behaviour that could occur with the provision of a public toilet. There were also concerns that the greater provision open space could mean loss of car parking and would require maintenance from council.

A large majority supported public realm improvements on Hobson Street and Barnsbury Road. Participants frequently commented that these areas are currently underused and any improvements would be welcomed, especially planting more trees.



## 4.5 General support and additional themes raised

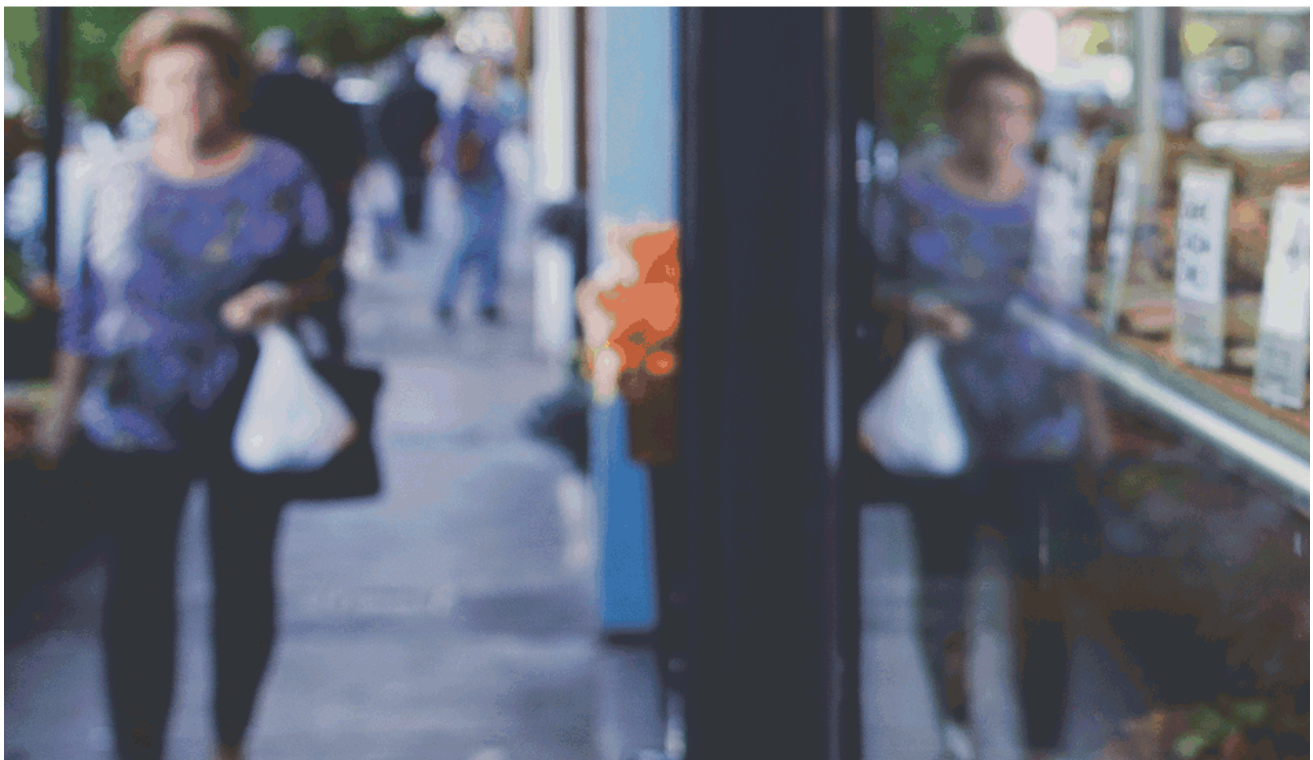
**Figure 9: Participants general levels of support for the Draft Plan**



Approximately half of participants supported the overall plan. However, this question had a low response rate (71 per cent) which may be attributed to an error in the online version of the survey which made this question hard to read. Participants who supported the Draft Plan were glad there would be a plan for future growth and change in Hawksburn Village and felt this was a 'step in the right direction'.

Participants were asked to provide comments on the aspects of the Draft Plan they felt most strongly about. Their responses are summarised in Table 2.

Overall more participants chose to list aspects they liked about the Draft Plan than disliked. Improvements to public realm, particularly more greenery, was the aspect most commonly listed as 'most liked' by participants. The preferred maximum height limits (particularly four and five stories) was the aspect that participants most commonly 'liked least'.



**Table 2: What participants liked most and least about the Draft Plan**

What were the aspects of the Draft Structure Plan people <i>most</i> liked? ✓	What do people <i>least</i> like about the Draft Structure Plan? ✗
<ul style="list-style-type: none"> <li>Improvements to public realm particularly more greenery <b>(57 participants)</b></li> </ul>	<ul style="list-style-type: none"> <li>The preferred maximum height limits, particularly 4 and 5 stories <b>(46 participants)</b></li> </ul>
<ul style="list-style-type: none"> <li>Retaining and being sympathetic to the heritage in the area <b>(24)</b></li> </ul>	<ul style="list-style-type: none"> <li>Increasing development, people and congestion in the Village <b>(20)</b></li> </ul>
<ul style="list-style-type: none"> <li>Maintaining the small scale, fine grained 'village' character <b>(23)</b></li> </ul>	<ul style="list-style-type: none"> <li>Having a bike lane on Malvern Road <b>(13)</b></li> </ul>
<ul style="list-style-type: none"> <li>Having a plan for the future of the Village which will provide residents with greater clarity of future change <b>(17)</b></li> </ul>	<ul style="list-style-type: none"> <li>That there was not plan to address current traffic congestion on Malvern and Williams Roads <b>(12)</b></li> </ul>
<ul style="list-style-type: none"> <li>Developing an underground car park at May Road <b>(15)</b></li> </ul>	<ul style="list-style-type: none"> <li>Any proposals to allow for more car parking <b>(11)</b></li> </ul>
<ul style="list-style-type: none"> <li>Having a laneway precinct in McKillop Street <b>(15)</b></li> </ul>	<ul style="list-style-type: none"> <li>Loss of on-street car parking spaces with no substitute <b>(9)</b></li> </ul>
<ul style="list-style-type: none"> <li>The pedestrian crossing at Hobson Street and Malvern Road <b>(11)</b></li> </ul>	<ul style="list-style-type: none"> <li>The proposal to close Westbourne Street <b>(8)</b></li> </ul>
<ul style="list-style-type: none"> <li>Placing height controls in the area <b>(11)</b></li> </ul>	<ul style="list-style-type: none"> <li>Having an accessible tram stop <b>(7)</b></li> </ul>
<ul style="list-style-type: none"> <li>Developing the Western Precinct <b>(9)</b></li> </ul>	<ul style="list-style-type: none"> <li>Proposal to have a pedestrian crossing at Malvern and Hobson Streets <b>(5)</b></li> </ul>
<ul style="list-style-type: none"> <li>Making public transport more accessible <b>(8)</b></li> </ul>	<ul style="list-style-type: none"> <li>The McKillop Laneway Precinct <b>(5)</b></li> </ul>
<ul style="list-style-type: none"> <li>Having a bicycle lane on Malvern Road <b>(7)</b></li> </ul>	<ul style="list-style-type: none"> <li>Developing on top of heritage buildings <b>(4)</b></li> </ul>
<ul style="list-style-type: none"> <li>Removing non-essential obstacles on pedestrian footpaths <b>(3)</b></li> </ul>	

Although it is outside the scope of this consultation, participants frequently brought up the need to plan for future traffic congestion. They felt that this was a missing in the Draft Plan, particularly given current congestion issues on Malvern Road and lack of car parking in the Village. They highlighted that increased density in the Village would only make this worse and therefore needed to be addressed.

Comments on the proposal to have an underground car park off May Road are included in Built Form and Heritage feedback in section 4.1.

A few participants felt that there are inconsistencies in the management of heights in Miller Street. They pointed out that there are currently five-storey buildings in the street and that applying a height limit now will unfairly disadvantage remaining landowners who want to develop in the future.

# Appendix 1

Seventeen surveys were provided after the closing date for Stage 3. The following is a summary of the responses in these surveys. They were not included in the detailed findings in the main body of this report.

## 1. Relationship to Structure Plan area

Local resident	12
Business owner/trader	3
Landholder	6
Developer	
Employee	
Student	
Visitor	1
Other	

## 2. Built form and heritage – level of support for built form and heritage direction

	Don't support	Neutral	Support	Can't say
Allowing sympathetic additions to upper levels of heritage buildings	6		11	
<b>Reasons for level of support:</b>				
Once heritage building is altered it is no longer heritage.				
Keeping the heritage buildings is paramount.				
Natural part of what is already there.				
Affecting sun, wind and experience of openness.				
Unless the heritage part of the building are completely preserved interiors and not only facades				
Support as long as 1st and 2nd floors are setback from facade of ground level				
A preferred maximum height of four storeys in the eastern precinct	9	4	3	1
<b>Reasons for level of support:</b>				
Way too high				
Ugly and shade producing				
Less already congested and too dense.				
But 2 storeys at back adjacent to residents				
In order to retain the amenity of a 'village' precinct in this eastern part should be no more than 3 storeys and 1st and 2nd floors setback from facade of ground level				
A preferred maximum height of three storeys in the May Road car park area	6		10	1
<b>Reasons for level of support:</b>				
Out of keeping with housing in May Road				
Two storeys is enough				
Two would be preferable and include basement car parking				
Support as long as 1st and 2nd floors are setback from facade of ground level				
A preferred maximum height of four storeys in Howitt Street	10	1	5	1
<b>Reasons for level of support:</b>				
Out of keeping with height of housing opposite.				
Two storeys is enough				
Less blocks sun				
Support as long as no more than 3 storeys in eastern precinct and 1st and 2nd floors are setback from facade of ground level				
A preferred maximum height of five storeys in the western precinct (excluding Howitt Street)	10	2	4	1
<b>Reasons for level of support:</b>				
Way too high, the village is being ruined				
Less creates wind tunnel. It is scary at night.				
This height is counterproductive to the professed 'village' plan. Max of four storeys in this western precinct since 'cat already out of bag'				



### 3. Industrial character – level of support for encouraging the retention of the industrial character within the western precinct

	Not important	Neutral	Important	Very important
How important is retaining the industrial character of the western precinct?	1	5	4	7
<b>Reasons for level of support:</b>				
It is the fabric of our village				
The "Character"				
Keep it cool!				
I think that approval of large apartment complexes removing existing features would degrade the area.				
In the scheme of things the 'industrial' heritage of this area is low on people's priority or they are oblivious to it – irrelevant to everyday life!				

### 4. Land use activities – level of support for land use directions

	Don't support	Neutral	Support	Can't say
Actively encouraging retention and provision of small scale employment uses (eg retail and first floor office space)	1	2	14	
<b>Reasons for level of support:</b>				
I support small business				
I don't feel confident chain stores will be prevented from leasing these retail spaces (ie Chapel Street northern end)				
It is what makes a village!!				
Provided open recreational space is included in project or is provided by having a new small park/garden area				
Necessary for continued commercial viability of village but need dual development of infrastructure to go hand in hand – e.g. car parking (off street)				
Enhancing McKillop Street as a 'Laneway precinct' including new developments that open onto the laneway		2	14	1
<b>Reasons for level of support:</b>				
Walkable and convenient				
Support on the condition that new developments does not mean demolition of existing buildings.				
Will create a 'safe' route through to railway and also reduce ugly 'laneway' mentality of rubbish and graffiti				
It will enhance the charm of Hawksburn (what's left of it)				

## 5. Access and movement – level of support for access and movement proposals

	Don't support	Neutral	Support	Can't say
Pedestrian crossing in proximity to intersection of Hobson Street and Malvern Road. May result in the loss of some on-street car parking.	5	4	8	
<b>Reasons for level of support</b>				
Parking on Malvern Road plays an essential role for traders survival. There is not enough foot traffic to justify the expense				
Should not interfere with Malvern Road traffic so close to Malvern/Williams Road intersection.				
It will slow traffic even more.				
May create traffic flow issues, buses, trams etc				
Slows traffic				
We like it as it is				
Safe pedestrian access to supermarket needs to be improved				
Increase of amenity and safety for pedestrians should be a priority but not just here-also at Mathoura Road				
Improving directions for pedestrians to the train station form the corner of Hobson Street and Malvern Road.		4	13	
<b>Reasons for level of support</b>				
Parking on Malvern Road plays an essential role for traders survival. There is not enough foot traffic to justify the expense				
Makes sense				
No more signs needed				
Traffic light crossing should form a traffic flow for egress from any car parking options				
Obviously beneficial if trying to encourage people to use public transport but station has to be safe for use				
Investigating the removal of footpath items including non-essential signage in the Centre.	1	6	10	
<b>Reasons for level of support</b>				
Don't know what will be removed				
Since pedestrian traffic is prime in village – too congested for easy access from shops / vehicles etc and multiple trip hazards with boards /tables/produce eg fruit shop				

**6. Access and movement – level of support for access and movement proposals**

	Don't support	Neutral	Support	Can't say
Advocating to State Government for quality, integrated, accessible design of disability compliant tram stops	5	2	8	2
<b>Reasons for level of support</b>				
Waste of taxpayers money. We can't justify a tram stop. Also look what has happened to Bridge Road Richmond. It has killed all the traders.				
Too much disruption for little improvement.				
Impede traffic flow too many tram stops now.				
It is already congested.				
Super stops ruin the street				
Tram facilities are important but also need to protect tram users getting on and off				
Investigate ability for Malvern Road to accommodate bicycle lanes during peak hour in the long-term.	4	5	7	2
<b>Reasons for level of support</b>				
Absolutely not!!! Your survey points out high end fashion and food outlets including restaurants, the demographics of the surrounding suburbs – do not demonstrate people shopping on bikes				
Not sure if these would be a safe space – too narrow.				
Can't you learn from Fitzroy Street Kilda				
Encourage safe riding is a positive				
Provide safe bike lanes where there are T junctions with roads eg Mathoura Road				
Support providing there is some change to the laws regarding bike use ie registration etc. Currently they ride on footpaths/ ride dangerously amongst cars /beside cars/into pedestrians – all with impunity because in the event of an incident there is no identification possible				

## 7. Public realm and open space – level of support for public realm and open space proposals

	Don't support	Neutral	Support	Can't say
Improving Hobson Street and Barnsbury Road public realm		1	15	
<b>Reasons for level of support</b>				
Trees and seating are always appealing				
Any improvement of streetscape is desirable. Stonnington has dropped the ball completely over past decade re street cleaning/landscaping/graffiti/rubbish removal				
Creating new public open spaces in the eastern precinct of the centre	2		13	
<b>Reasons for level of support</b>				
No need! If they want to sit, they will sit in the cafes.				
Absolutely necessary.				
Very important for liveability. Include more trees and green space				
Desirable but not much point in this if pedestrians find themselves in canyons of tall overscale development casting street and neighbouring properties into shadows viz corner (north side) Hobson and Malvern				

## 8. Aspects of the Structure Plan *most* liked

Nothing!! Bike lanes in Hawksburn is not for the people of the surrounding areas. Malvern road has a clearway on both sides, hence this is sufficient for bicycles and vehicles.

More open space in the eastern precinct

Protection of heritage shops / buildings

Keeping the heritage buildings is imperative

Pulling down the charming block of flats on corner of Grandview and Chatsworth Road is short sighted and will be detrimental to the whole village.

I think you are on the right path. Need more open space, maybe cover train line at station (Malvern)

1. Undergrounding the May road car park is necessary in light of the increased traffic, but entry and exit? Perhaps 2 levels.

2. More 'greening' of the areas would be wonderful.

It is a common sense not too drastic plan. I would call it more an upgrade rather than a Structure Plan.

McKillop Street lane development, trams stops, open spaces, green areas, better traffic flow.

Public open spaces!

Keeping Hawksburn family friendly and no late night venues.

Preservation of heritage precinct in east precinct.

Maintain the Victorian and Edwardian character and others that are 3 or 4 storey buildings

Keep all shop fronts in both Eastern and Western precincts wherever possible. This architecture provides the fabric of the unique experience of Hawksburn Village and should be preserved

Proposal to limit development heights – just got wrong numbers of storeys

Improve pedestrian access to public transport / declutter footpath and safety – traffic lights

Clean up neighbourhood – landscaping/seating etc

Improve parking congestion

## 9. Aspects of the Structure Plan *least* liked

Bicycle paths – sorry bike lanes in Hawksburn is not for the people of the surrounding areas. Malvern Road has a clearway on both sides, hence this is sufficient for bicycles and vehicles.

Development height in Howitt Street and May Streets out of keeping with opposite residential nature of street

Where's the parking plans?

Too much expansion – its a village, not a mecca

No buildings over 3 storeys in eastern precinct or over 4 storeys in western precinct

Exacerbating congestion in traffic. The maximum height in 3 stories on Malvern Road.

The potential for high rise buildings would be a mistake long term.

No bicycles lanes please. No more tram stop alteration. Fight for new design tram's that have elevating doors for disabled like in San Francisco. Do not eliminate chairs and tables from the village.

High rise development

Building heights

There needs to be stricter restrictions about 2 storey limit no super tram stops on Malvern Rd.

Problems with closing Wesbourne Street, access needed for off street parking in garages off the lane. More demands on parking in residential streets will occur – perhaps the 2 hour parking limit could be extended for Sat and Sundays and until 9pm.

The supermarket site loading and parking options should access and egress from Malvern Road preserving the local residential amenity. All deliveries should be from Malvern Road only. I am concerned about the overdevelopment of the supermarket site which might set a benchmark for other commercial buildings.

All new developments or redevelopments should include mandatory accessible underground parking with a proportion of car parking spaces for visitors. No access to supermarket site from May Road

Delusion that all proposals are going to improve amenity is that excessive development in highrise scale will not impact of the 'village' character of hawksburn which makes it pretty unique in Metropolitan Melbourne and in company e.g. Maling Road Canterbury

Not everything is measured in concrete and steel

## 10. General level of support for entire Draft Structure Plan

	Don't support	Neutral	Support	Can't say
<b>General level of support</b>	4	2	9	2
It looks like it has been designed without the consultation of locals nor traders. 191 responses is not adequate.				
Where are the designated parking areas? Such as existing sawfay car park on May Road? Will all multi storey developments have adequate onsite parking? Otherwise spill over in residential streets that are already at capacity.				
Depends how tastefully it is carried out				
Access for parking in our street				
Agree with managing and controlling the amount of development that occurs – limit heights to 4 storeys etc and setbacks abutting residential properties				
Support with qualifications – if Council is genuinely wanting to preserve the 'village' amenity especially in eastern precinct, it ought to reduce its proposed development heights				

## 11. Further comments

Scrap it!

The charm of the village is being torn out with the huge developments that will dwarf our little precinct.

Buildings too high – 3 storeys in eastern precinct, 4 storeys in western precinct

Any Plan must be very sensitive to a shopping strip with an unique village atmosphere.

Very worthwhile to look to the future colours of buildings – subdued colours are more attractive to the eye but not black. Also, a pitched roof is more appealing than those 'shoeboxes' with their flat tops.

Please do not change the village to the point that it loses its village life. But try to build more parking space.

Westbourne Street – Strongly against making one way and addition of toilet block etc. Need permit zone only parking in street as per most councils and streets abutting shop centres – Parking has become impossible in this street most of the time!

To improve the area remove power lines and put them underground. Do not go and spend our rates on installing pointless signs with a commissioned Hawksburn logo (like Stonnington did for Glenferrie Road precinct).

Needed keep clear on Malvern Road at the end of Lorne Rd.

- Extension of residential parking hours.
- Keep Westbourne Street open as access required both ends of the lane ways.

It would be a good idea to take the 40 mph speed limit back towards Orrong Rd. Cars are finding it hard to slow down to the limit because of the down-hill run. They are often fast at the Chatsworth Road function and Lorne Road function onto Malvern Road.

Create more green space, manage traffic flows, remove waste collection options from rear of commercial properties.

Enforce use of commercial land according to permits ie delivery times to supermarkets.

Diesel pollution from trucks running their engines to be limited adjacent to residential areas

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#### Mathoura Road / 7 Malvern Road Intersection

- Six years have now lapsed since restricted 40km zone was implemented. During that period, conditions for safety of all pedestrians have deteriorated
- The speed restrictions have had no impact on reduction of speed by road traffic in this street
- Current installation of the 'speed advisory trailer' is an expensive waste of time and money, a token gesture without the back-up enforcement of a 'speed camera' actually causing some financial pain to repeat offenders
- Mathoura Road continues to be habitual 'ran run' for through traffic seeking to avoid delays of traffic lights at Williams Road (at both intersections of Malvern and Toorak Roads) – these drivers continue to make right hand and left hand turns into the street from Malvern Road and the same at Toorak Road without stopping, disregarding stop signs and irrespective of whether pedestrians are crossing these intersections
- Have been numerous near misses at both ends of the street
- Marked pedestrian crossing at Brookville Road is unsafe with vehicles gathering momentum at this point to speed to either end of the street
- Last year a neighbour was in the process of crossing Mathoura Road (east to west) when he was run over by a female driver in a 4wheel drive vehicle on her mobile, turning left without stopping at the stop sign into Malvern Rd
- If Council is genuine in wanting to protect the safety and ease of access for pedestrians then any draft Hawksburn Village Structure Plan should also include a serious restructuring of the road traffic controls at the intersection of Mathoura and Malvern Rds (and similarly at intersection of Mathoura and Toorak Rds)

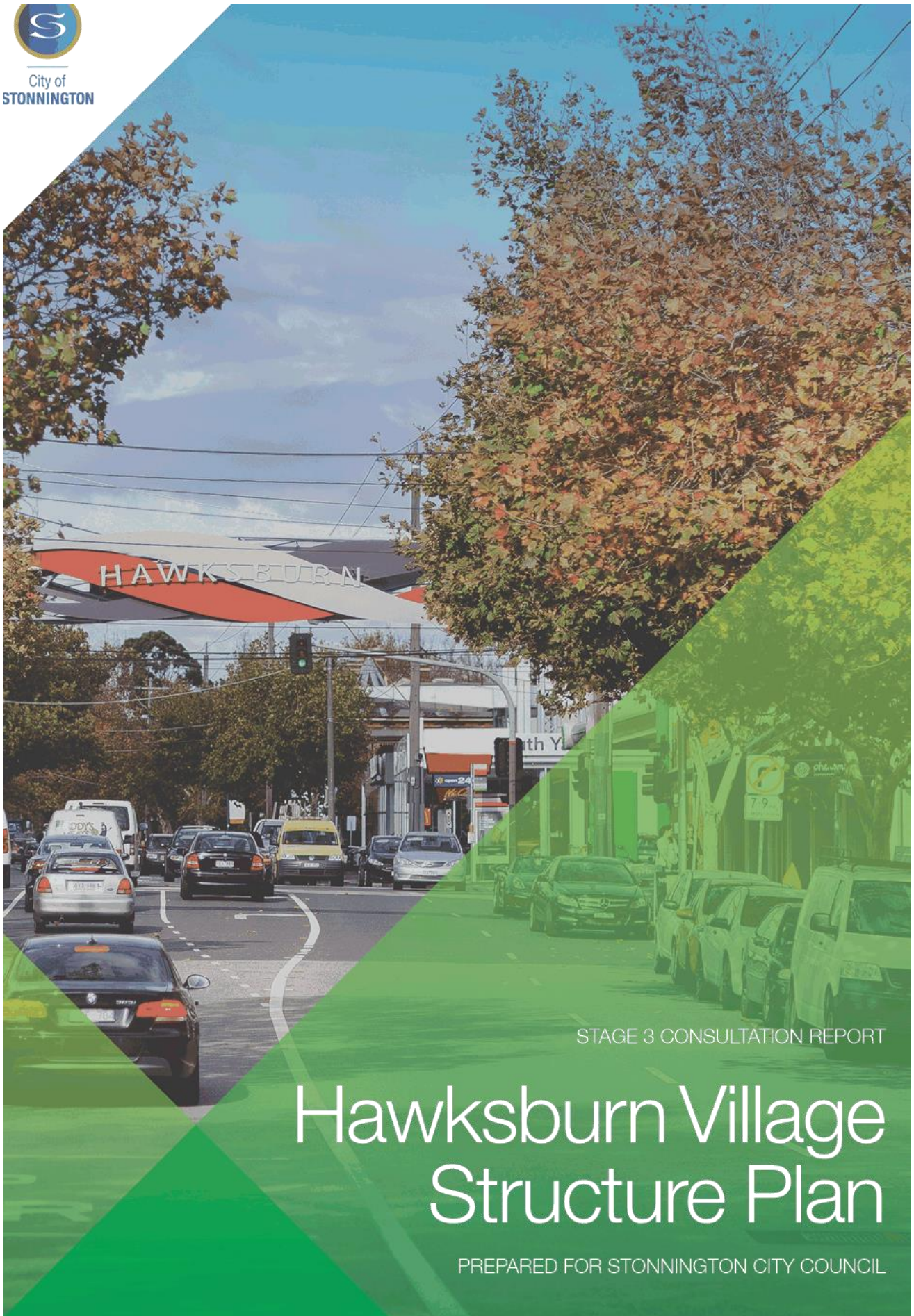
To discourage use of Mathoura Road as rat run, the following suggestions are made:

- Create a 'u' shaped traffic lights and pedestrian crossings at the intersection of Mathoura and Malvern Rds around the current 'keep clear' zone
- Move the current pedestrian traffic lights at the intersection of Malvern Road and Street John's Lane (which are not used efficiently because people will not walk out of their way to cross here and then walk back to say the tram stop opposite Mathoura Rd) further to the west; and relocate them at the actual corner of Mathoura and Malvern Rds, with a 'turn right arrow' to control west-bound traffic entering Mathoura Rd, and allowing pedestrians to cross both Malvern and Mathoura Rds with the lights
- At the same time install traffic lights on the west of this intersection, with a turn left arrow to control east-bound traffic entering Mathoura Rd, and again allowing pedestrians to cross both Malvern and Mathoura Roads with the lights (and to access both tram stops at this intersection)
- The same arrangement should be applied to Toorak Road end as well to work in conjunction with the existing pedestrian traffic lights and 'keep clear' zone

Under these conditions, the residents and pedestrians of Mathoura Road and its environs could have some assurance that –

- (a) Stonnington council and vicroads are genuinely serious on the matter of road safety /pedestrian safety/ speed restrictions; and
  - (b) Have the confidence to utilise the pedestrian footpaths and crossings in a manner that is conducive to both their individual health and that of the environment and
  - (c) These changes are crucial to any proposals to increase both vehicular and pedestrian and resident numbers into Hawksburn Village as envisaged by this draft Plan.
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STAGE 3 CONSULTATION REPORT

# Hawksburn Village Structure Plan

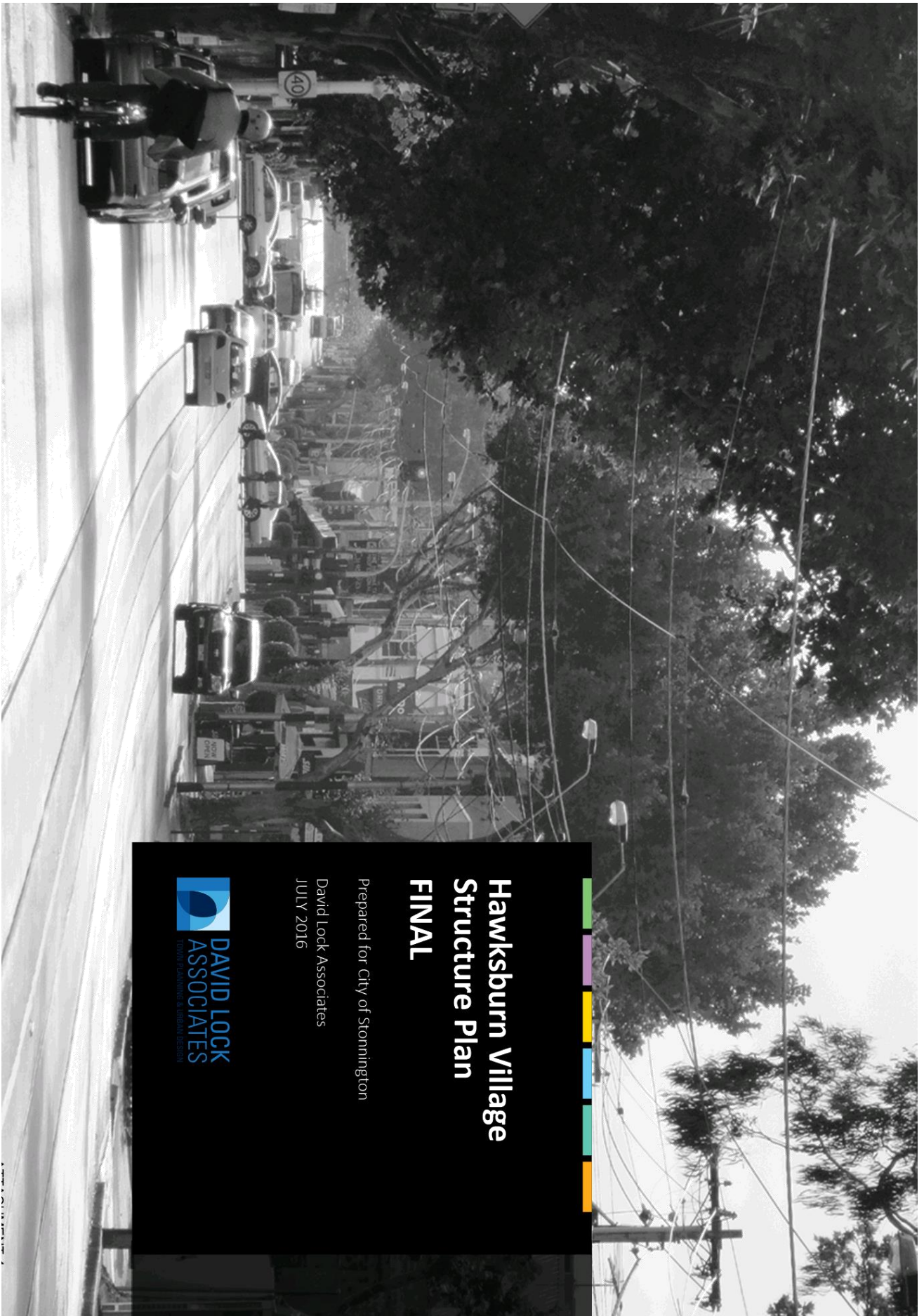
PREPARED FOR STONNINGTON CITY COUNCIL

Item 8

Attachment 4 Attachment 4 - Final Structure Plan







Contents

1.0 Introduction.....3

2.0 Study Area .....5

3.0 Background Analysis.....6

4.0 Community Feedback.....11

5.0 The Vision & Structure Plan Framework .....12

6.0 Precincts and Guidelines .....24

7.0 Implementation and Action Plan.....36



# 1.0 Introduction

## 1.1 Introducing Hawkeburn Village

Located 4.5km from the Melbourne CBD and abutting the Chapel Street Activity Centre, Hawkeburn Village is a desirable inner urban area with a local village feel. It has important physical, social, and economic links to both these areas of activity. See Figure 1.

Hawkeburn Village is a unique, local, shopping strip that serves not only the local community but brings shoppers from elsewhere to peruse the gourmet foods, fashion boutiques, and home-wares on offer. The village itself predominantly lies on Malvern Road and is dissected by Williams Road.

As a Neighbourhood Activity Centre (NAC) within the Stonnington Planning Scheme, Hawkeburn Village provides daily needs to its surrounding trade area. The character of the Village differs across the centre. East of Williams Road is valued for its traditional fine grain character, strong heritage features that underpin its character and sense of place and its diverse retail and commercial offer.

West of Williams road is an eclectic mix of larger format light industrial buildings and uses interspersed with retail, commercial, and residential uses. Together these areas form a vibrant and successful local centre within the City of Stonnington.



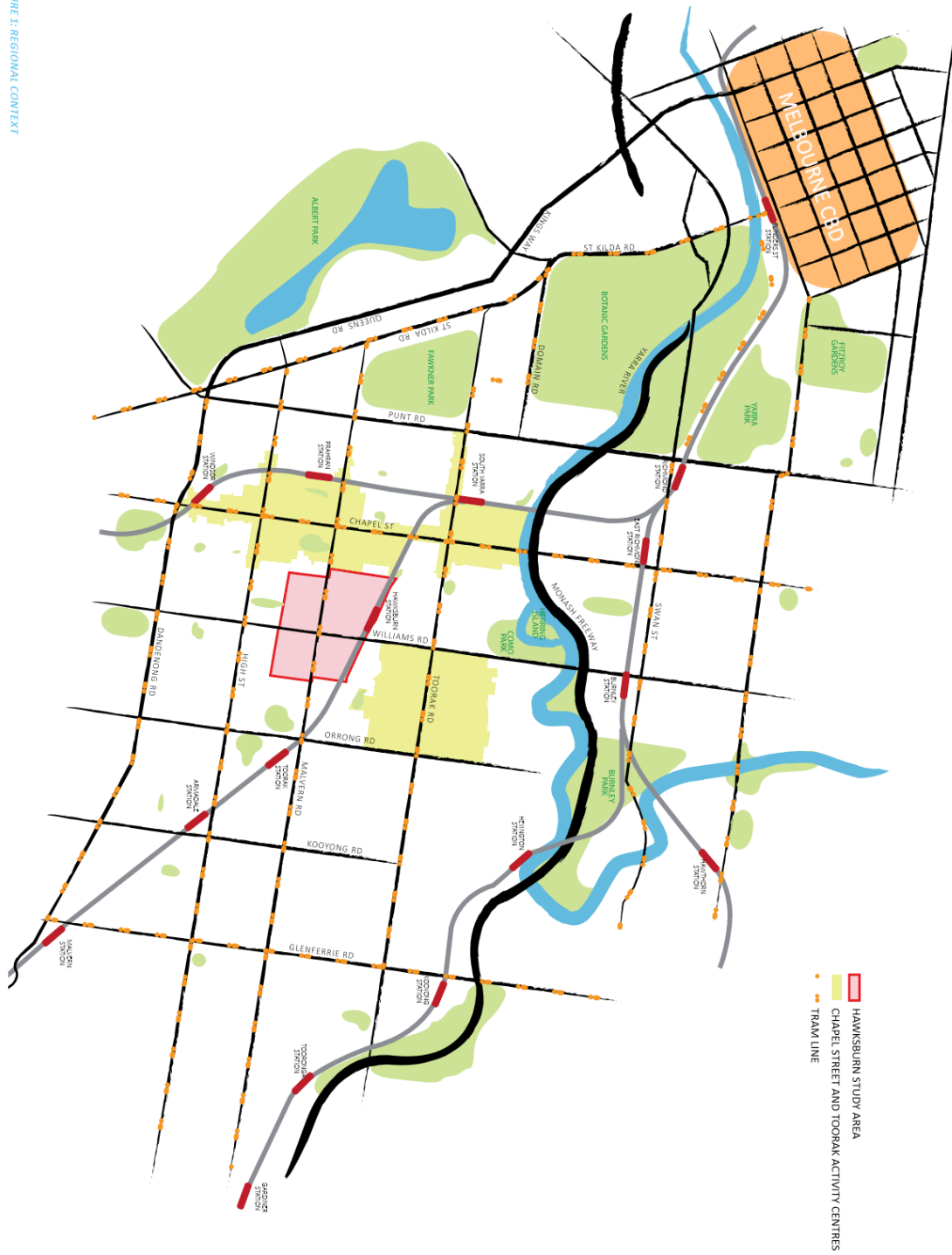


FIGURE 1: REGIONAL CONTEXT

1.2 Aim and Purpose

Stonnington City Council has prepared the Hawkeburn Village Structure Plan (Structure Plan) for the Hawkeburn Neighbourhood Activity Centre. The key aim of the Structure Plan is to develop a shared vision for the activity centre to 2040, and to identify the type and scope of change projected within the activity centre over that time.

The purpose of the Structure Plan is to:

- Provide a framework for land use, built form, movement and open space for the activity centre; and
- Establish objectives, strategies and guidelines relating to land use, built form and heritage, access and movement, public realm and open space, and sense of place.

1.3 Preparation of the Structure Plan

The preparation of the Structure Plan has been undertaken in four stages:

Stage 1: Analysis, Opportunities and Constraints

Urban design, economic and transport background analysis was undertaken by the consultant team. This analysis provided information on:

- Land use;
- Built form;
- Public realm and open space;
- Sense of place;
- Retail and commercial existing conditions and potential; and
- Car parking, public transport, cycling and pedestrian infrastructure existing conditions.

Stage 1 of community and stakeholder consultation was undertaken to gather information about the centre. The analysis and consultation was interpreted to define a draft structure plan vision and objectives based upon the themes of Land Use and Activities, Built Form, Public Realm and Open Space and Access and Movement.

Stage 2: Key Directions

Stage 2 community and stakeholder consultation was undertaken to seek feedback on the vision and set objectives and key directions for the Draft Structure Plan.

Stage 3: Draft Structure Plan

The Draft Structure Plan was prepared and included:

- A summary of feedback received from the community and stakeholders;
- A vision for the activity centre;
- Guiding themes and objectives to provide the framework and direction of the Structure Plan; and
- Recommendations for action areas which present particular opportunities for change.

Refer to background reports in Chapter 3.

A Social Impact Assessment was undertaken in October 2015 to inform the Draft Structure Plan.

Stage 3 community and stakeholder consultation on the Draft Structure Plan has informed the Final Structure Plan.

Stage 4: Final Structure Plan

The final Structure Plan was prepared following community consultation on the draft Structure Plan.

1.4 Reading this Document

This Structure Plan sets out a proposed vision for the future of the activity centre and identifies a series of objectives and strategies that outline the way the vision will be achieved.

Parts 1 – 4 of the document outline the study area and provide the existing context and issues facing the area.

Part 5 identifies the 'Vision', Objectives, Strategies and Structure Plan Framework. This is broken into 5 guiding themes under which objectives and strategies are articulated. These themes are:

- Theme 1: Land Use Activities
- Theme 2: Built Form and Heritage
- Theme 3: Access and Movement
- Theme 4 : Public Realm and Open Space
- Theme 5: Sense of Place

Part 6 identifies specific precincts within the activity centre and the objectives, strategies and design guidelines required to achieve the vision.

Part 7 takes the objectives and strategies and articulates how to implement them in a way that will achieve the vision.

Figure 2 illustrates the structure of the structure plan.

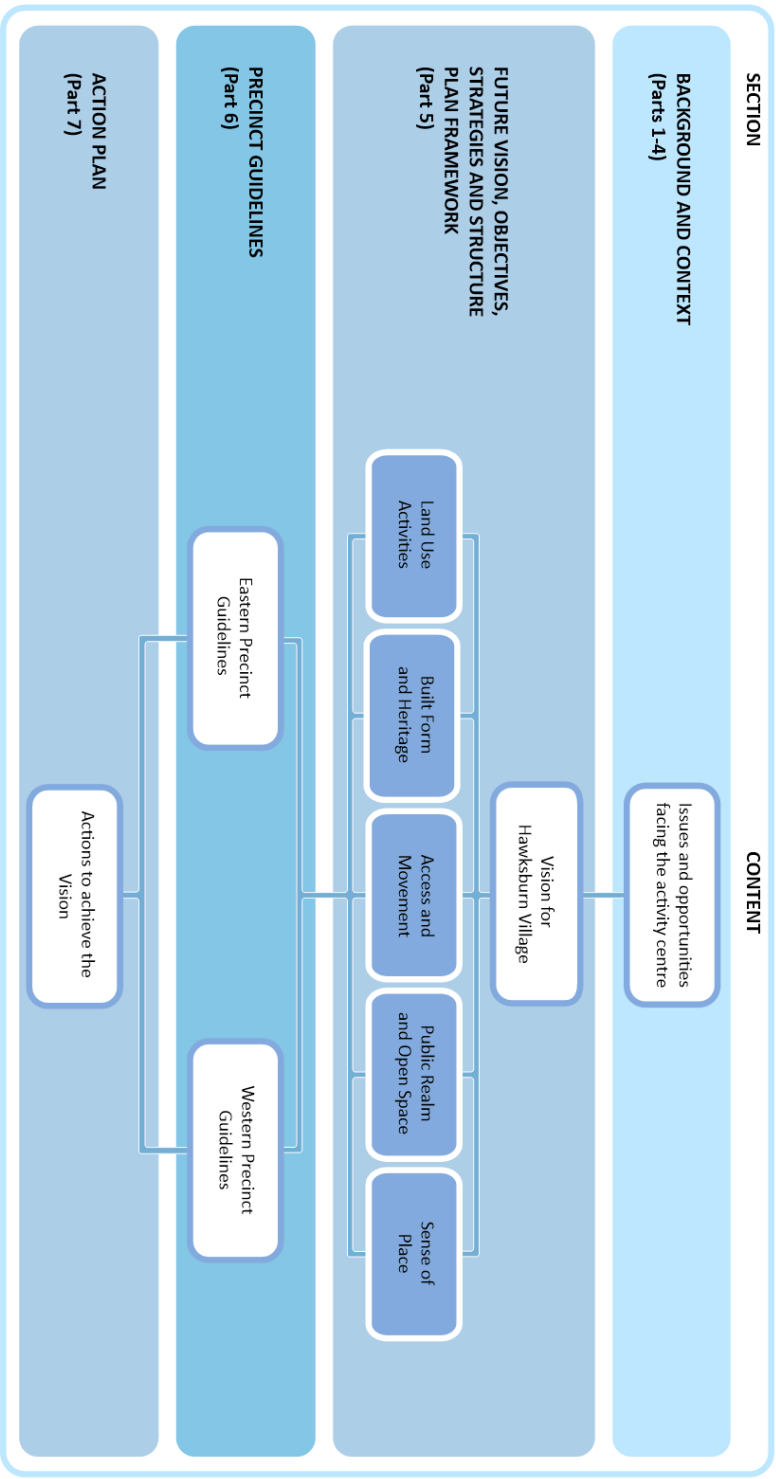


FIGURE 2: STRUCTURE

## 2.0 Study Area

### 2.1 Introduction

A study area was nominated in the original scope for the purpose of undertaking the structure planning process. (Figure 3). The study area is bounded by Surrey Road to the west, Mathoura Road to the east, Murray Street and Wrights Terrace to the south and the railway line to the north (including Hawksburn Station).

The 'initial' activity centre boundary was developed through desktop analysis applying the Practice Note 58 activity centre boundary criteria. The activity centre boundary as displayed at Figure 3 was established to reflect existing land use patterns and urban form. Land with a strong functional inter-relationship with the centre has also been included and areas which are designated for limited change have been excluded.

The Structure Plan focuses on the commercial and mixed use zones of the centre and some small areas of residentially zoned land which have a strong inter-relationship with the centre. The centre's defining elements are as follows:

- A successful retail core associated with diverse retail, business and light industrial precincts.
- A high level of public transport access with Hawksburn Station within a 5 minute walk of the centre and frequent tram and bus services extending along Malvern Road.
- A local village feel created by late Victorian, Edwardian and interwar heritage buildings, fine grain retail shop fronts, detailed and articulated façades and a leafy green public realm.

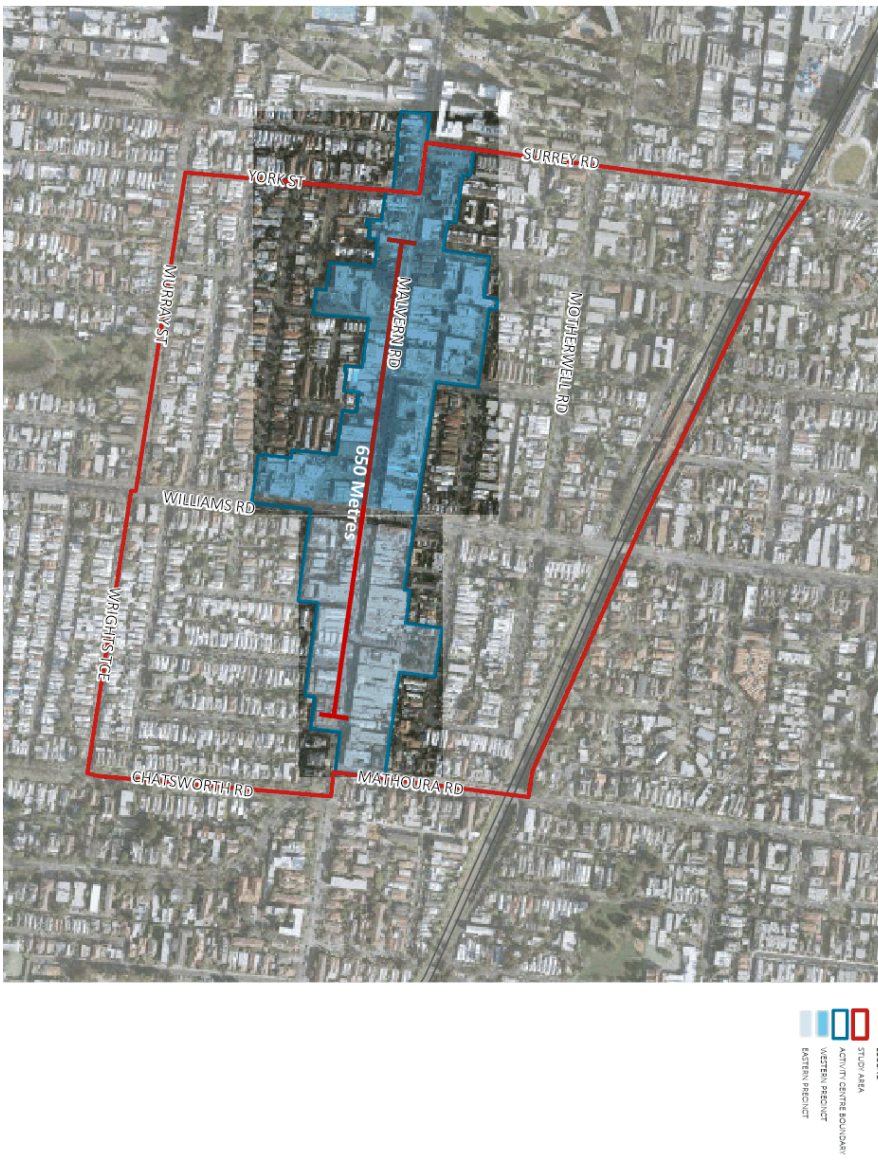


FIGURE 3: STUDY AREA



## 3.0 Background Analysis

### 3.1 Introduction

In addition to the brief background analysis below, further detailed information is provided within the background analysis reports.

### 3.2 Brief History

The historical development of Hawksburn Village is important in understanding the current conditions and issues faced today.

The Structure Plan area was originally occupied by the Woi Worrung (Wurundjeri) and Boon Wurrong (Bunurong) people. During Melbourne's expansion in the 1830s and the surveying of the Hoddle Grid in 1837, the Wurundjeri and Boon Wurrong regularly camped along the south banks of the Yarra River.

By the mid-1850s the area was still largely uncleared bushland. At this time, commercial development in the centre began when a hotel was built on a site of the present Bush Inn.

The area was particularly spurred on by the construction of the Oakleigh Rail Line in 1879 which lead to the eventual development of Hawksburn Station. Following this, retail development around the Bush Inn took place and most of the existing shops on the northern side of Malvern Road were built in this period to cater for residents in the newly formed Hawksburn and Bush Inn Estates. The Prahran School (Hawksburn Primary School) opened in 1874. The Hawksburn Tennis Club was opened in 1913.

### 3.3 Community Profile

Analysis identified an indicative Trade Area (ITA) for Hawksburn Village. The community profile of this ITA can be summarised as follows:

- From 2011 to 2031 an increase of approximately 4400 residents are anticipated (i/d Consultants) within the ITA.
- The age distribution is anticipated to shift slightly from the 25-29 year age group to the middle age groups (35-54 years).
- With the relatively younger age profile of residents within the ITA, there are fewer residents living in marital or de facto arrangements, and a high proportion in either lone person or group households when compared to the wider Stonnington municipality.
- There is a small proportion of residents aged less than 15 years, indicating a small number of traditional family households.
- Within the ITA and the wider Stonnington municipality, there is a lower proportion of residents working full-time that earn less than \$1,000 per week and a much higher proportion earning more than \$2,000 per week when compared to metropolitan Melbourne.
- The average household size in the ITA is 1.75 people per dwelling.
- Projected housing growth within the ITA is an additional 2,000 dwellings by 2031.

### 3.4 Strategic Policy Context

The current Metropolitan Planning Strategy 'Plan Melbourne' was released in May 2014. Centres such as Hawksburn Village are referred to as neighbourhood centres and their importance to local communities in terms of access to services, employment opportunities and the liveability of their neighbourhoods is recognised. Plan Melbourne also states that the planning of neighbourhood centres that maintain their 'village' character and feel, while enabling a mix of goods and services, is a key role for local government working with their communities.

The State Planning Policy Framework (SPPF) refers to the importance of building up activity centres as the focus for high-quality development, activity and living for the whole community by developing a network of activity centres offering in size and function. Undertaking strategic planning for the use and development of land in and around activity centres is encouraged.

The SPPF also aims to create liveable communities and neighbourhoods where

people have safe and convenient access to the goods and services they need for daily life within 20 minutes of where they live travelling by foot, bicycle or public transport.

The Strategic Framework Plan at Clause 21.03 of the Stonnington Planning Scheme (the Scheme) identifies an Activity Centre Hierarchy within Stonnington. Hawksburn is identified as a large Neighbourhood Activity Centre which is situated below the Principal Activity Centres of Chapel Street and Chadstone and the Major Activity Centre of Glenferrie Road/High Street. The role of large neighbourhood activity centres is to cater for everyday needs and wider specialty retail, office, and service markets. The strategic direction for Hawksburn Village is predominantly retail, with a food shopping character, plus a wider mix of office and services uses at the western end.

Council is in the process of preparing an Activity Centre Strategy. The project involves a review of activity centres and hierarchy. The Strategy will provide an overarching plan for the balance of Council's activity centres.

### 3.5 Analysis of Structure Plan Themes

The following section provides a brief summary of the current conditions and issues identified under each theme that helped shape the response outlined in this Structure Plan.

#### 3.5.1 Land Use and Economy (Theme 1)

##### Retailing

Future retailing opportunities within the Hawksburn activity centre are expected to relate to food retailing activities reflecting the existing designation of the centre within the Stonnington retail hierarchy. These opportunities are described within the Eastern and Western precincts of Hawksburn Village, as outlined at Figure 3.

The Eastern Precinct includes a Woolworths supermarket along with a limited range of food specialties which are in a strong trading position most likely due to a strong connection with the local community. The potential for an expansion of traditional food retailing within the Eastern Precinct is limited by the absence of appropriately sized sites elsewhere within the centre. The existing Woolworths has a floor space of approximately 1,100m<sup>2</sup> and with the projected future population growth, this is expected to increase to a full-line supermarket.

In the Western Precinct, there is the opportunity to complement key food retailing destinations such as Prahran Market through focusing upon niche segments such as gourmet and ethnic food retailing. Existing industrial

buildings offer the opportunity to accommodate medium sized food retailers that are sufficiently unique to attract visitors from across a wider region, which in turn will provide exposure for more generic activities such as cafes and restaurants.

Another niche area of retailing that Hawksburn performs strongly in is high-end fashion. This is due to its central location within Melbourne's relatively affluent inner south-eastern suburbs which provide a strong basis for supporting specialty and niche retailing activities. A key challenge remains in strengthening the core specialty retail focus in the Eastern Precinct whilst further intensifying the variety of uses in the Western Precinct therefore encouraging more activation and vibrancy.

#### Office

The Hawksburn activity centre currently includes a mix of purpose-built office space in various formats and a number of offices above retail premises. These shop-top spaces provide alternative and in some cases preferred accommodation for smaller niche businesses.

Enabling further office development within the study area would be beneficial to not only achieve higher job yields but it also has the added benefit of bringing more diverse jobs closer to people. However, significant amounts of new office development within Hawksburn Village would be expected to be limited by:

- Apartment projects representing a higher value based upon current property market conditions and construction costs.
- Strong competition from recognised office precincts within Melbourne's inner south and south-east regions including South Yarra and St Kilda Road.
- Available floor space and size of floor plates.
- Higher car parking rates and therefore increased development costs.

There may however be a willingness from some developers to include commercial floorspace where they may wish to diversify their exposure away from the residential apartment market. This may provide the opportunity to address the loss of employment identified by SSS Planning and Economics in their analysis undertaken for the Chapel revision Structure Plan.

#### 3.5.2 Built Form and Heritage (Theme 2)

The study area is characterised by a relatively compact urban form that becomes more spacious in parts of its residential hinterlands. It provides a variety of built form types ranging from low-rise residential to fine grain retail areas in the Eastern Precinct to medium to larger commercial and light industrial buildings in the Western Precinct. Redevelopment must be sensitive to the existing urban form, with particular consideration given to the challenge of integrating new built form within an often constrained and sensitive context.

The study area is incredibly architecturally diverse with buildings dating as far back as the 1850's. The diversity spreads into the residential hinterlands which includes pockets of heritage places that are intrinsic to the character of the study area. A large proportion of the residential areas abutting commercial and retail areas are under heritage overlays, and as such, minimal change is anticipated. Malvern Road itself consists of Victorian era shops and houses with ornate facades and high ceilings. The interface between these heritage places and areas of potential redevelopment in adjoining commercial and retail areas are considered sensitive.

Heritage places play a significant role as contributors to the character and identity of the study area, warranting specific design parameters to avoid the depletion of their significance. Buildings fronting Malvern Road in both the Eastern and Western Precincts of the centre are included within Heritage Overlay 'H0142 Hawksburn Retail Precinct'. There are many elements contributing to the significance of the heritage overlay including the consistent one or two storey scale of buildings.

The Eastern Precinct has a traditional streetscape character created by fine grain 1-2 storey late Victorian and Edwardian retail and commercial buildings on the northern side and 2 storey interwar retail buildings on the southern side. Of particular note is the Victorian Italianate style pub on the north-east corner of Malvern and Williams Roads. Commercial development in the precinct began when this hotel was built. The heritage buildings, narrow lot widths, and intimate built form all contribute to the fine grain and small scale nature of the Eastern Precinct which is a strong character element distinct to Hawksburn and worth retaining. A group of six single storey shops immediately west of Lorne Road are a particular feature of the area and should be retained to continue to mark the entry to Hawksburn Village from the east.

The Western Precinct includes a group of 1-2 storey Victorian commercial buildings on the western side of Williams Road which are included within the Hawksburn Retail Precinct Heritage Overlay. This Heritage Overlay extends

further north along Williams Road including a row of single storey Edwardian shops. The former Hawksburn Primary School, which is currently utilised as the Leonard Joel Auction House, is of State heritage significance and marks the beginning of the activity centre. Due to the building's distinct heritage features and unique design, it presents a particular challenge within any future conversion of land use, adaptation and re-use of the existing building. There are some buildings on the southern side of Malvern Road between Francis and Miller Streets which hold an industrial character worth respecting or retaining. Their built form typology is distinctive from the Eastern Precinct with medium grain, large but typically plain parapets with wide door and window frames.

#### Building Height

The study area predominately comprises low scale buildings ranging in height from 1-4 storeys. Slight increases up to 5 storeys have occurred through recent approvals along Malvern Road. Hawksburn Village itself includes heights that vary between precincts. The Eastern Precinct of Malvern Road has a traditional streetscape character with prevailing heights of approximately between 8 and 9 metres including parapets. The Western Precinct of Malvern Road consists of a mix of medium grain light industrial, commercial and mixed use developments. In contrast to the Eastern Precinct, it has a less dominant street wall character.

The heights proposed, 5 storeys for most areas in the Western Precinct and 4 storeys in the Eastern Precinct, respond to the prevailing character whilst allowing for adequate growth and revitalisation. Based on a capacity calculation undertaken as part of this Structure Plan, which was calculated on a series of assumptions, it was determined that the application of these heights proposed as part of this Structure Plan could provide approximately 500 new dwellings within the Activity Centre. The addition of approximately 500 dwellings will generally enable the housing growth projections anticipated by the Economic Analysis and also provide an adequate balance between change, growth accommodation and the retention of Hawksburn's unique village quality.

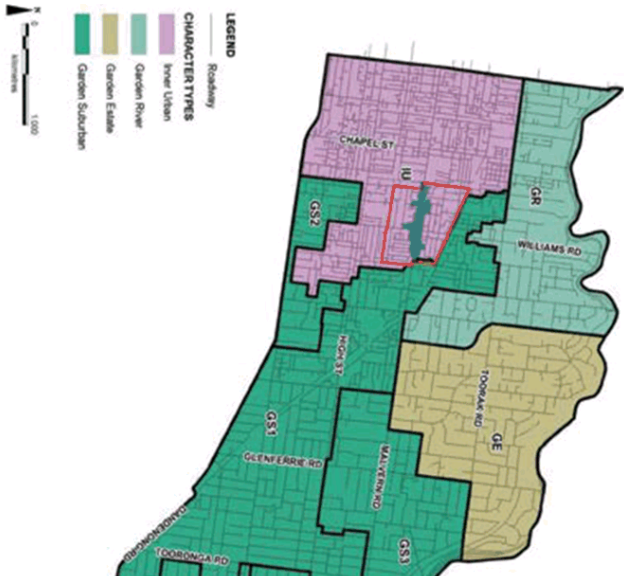


FIGURE 4: INNER URBAN NEIGHBOURHOOD CHARACTER PRECINCT MAP

**Access and Movement (Theme 3)**

The study area is well served by public transport including train, tram and buses. Williams Road and Malvern Road are the two main arterial roads that run through the study area. Williams Road caters for through traffic. Malvern Road is a highly contested space with it functioning not only as a traffic route but also as the main route for public transport and pedestrian and cyclist movements within the study area. While Malvern Road is identified as a bicycle priority route, it has a distinct lack of cycling infrastructure.

There are opportunities to increase levels of sustainable travel behaviour through improvements to infrastructure. A significant shift from vehicular based travel to sustainable forms needs to be made to improve the amenity of the area. This modal shift will require a systematic approach to improving the urban environment and prioritising pedestrian and cyclist movements over cars. As the traffic that occurs within the study area is partly generated by demand from outside suburbs, other traffic calming measures may need to be applied outside the precinct to improve the amenity for pedestrians and cyclists within the precinct. Hawkesburn Station is relatively isolated from the other forms of public transport. There is no direct access to the train station from Malvern Road, where the majority of pedestrian activity is located. To make it easier for people to use the train to get to and from the centre, the connection to the station needs to be improved.

The study area has a significant amount of laneways. Their narrow and intimate nature and bluestone features help to define the local character and sense of place within the study area. The majority of retail and residential properties along Malvern Road are accessed by lane-way. The redevelopment of these sites will place pressure on the laneways. How they function and manage deliveries and rubbish collection will need to be addressed. A key issue will be managing outcomes achieved in laneways within the study area.

**Public Realm and Open Space (Theme 4)**

The design of streets and landscaping in the activity centre is important in creating a strong sense of place and improving the vibrancy of the centre by promoting walking and cycling and enhancing amenity. The quality of the public realm across the study area varies. There are relatively narrow footpaths along Malvern Road which accommodate kerbside dining and sales as well as providing pedestrian access. There is a strong presence of transport infrastructure along Malvern Road.

Hobson Street forms an important pedestrian link from the train station. Its intersection with Malvern Road does not include a signalised crossing point for pedestrians heading north/ south or signage directing pedestrians towards the train station at this point. Ensuring streets and key routes are safe, pedestrian friendly and attractive and investigating opportunities for small-scale improvements is a key consideration of this Structure Plan.

There is a lack of public open space within the Study Area. More than 70% of the properties to the east of Williams Road are outside a 400m walking distance to public open space. A 400m walking distance is an important benchmark to measure the distribution and accessibility of public open spaces and in particular neighbourhood spaces. There is a need to identify opportunities for new public open spaces in the study area and in particular the Eastern Precinct where footpaths are also particularly narrow. Due to the lack of vacant land within the study area, finding more innovative ideas for public open space is key to addressing the shortage.

**Sense of Place (Theme 5)**

There are many unique characteristics within the study area that contribute to Hawkesburn Village's sense of place and identity. These include the fine grain retail focus in the Eastern Precinct which evokes a village character feel unique to Hawkesburn Village, the variety of retail and commercial activities along Malvern Road, Hawkesburn Station, the Hawkesburn gateway across Williams Road, the coarser grain of the western precinct and key landmark buildings including the former Hawkesburn Primary School. The residential hinterlands can also be broken into distinct character precincts which should be recognised and built upon through localised landscaping responses and public realm improvements. A key issue will be retaining the distinctive elements that define the identity of Hawkesburn whilst allowing for change and growth.

## 4.0 Community Feedback

Community consultation was undertaken in three stages, with Stage 1 occurring at the commencement of the structure planning process in October 2014 and Stage 2 in June/July 2015. Stage 3, consultation on the Draft Structure Plan, was undertaken over March 2016. The following is a summary of the main consultation events undertaken and the feedback received from the community.

### 4.1 Previous Consultation

Stage 1 community and stakeholder consultation was undertaken to gather information about the centre. The above analysis and consultation was interpreted to define a draft structure plan vision and objectives based upon the themes of Land Use and Activities, Built Form, Public Realm and Open Space and Access and Movement.

The Stage 2 consultation period sought to understand stakeholders' views on the following:

- The vision statement
- The draft proposed activity centre boundary
- Areas of opportunity to manage developmental pressure
- Built form height requirements
- Neighbourhood character
- Open space
- Movement around Hawksburn

#### 4.1.1 Vision Statement

The majority of respondents agreed with the vision statement, with some suggestions to include promotion of retail variety.

#### 4.1.2 Activity Centre Boundary

A majority agreed with the proposed activity centre boundary, with some concerns and comments expressing conditions to agreeing with the proposed boundary.

#### 4.1.3 Built Form and Character

The majority of respondents agreed that future development should be concentrated in the western precinct, with new development responding to the different character of the eastern and western precincts. Respondents'

understanding of the term "slightly taller development" translated to between 3-4 storeys. There was also strong support of the statements "new development allows for adequate sunlight, access to the footpath and public open spaces", and "new development enhances existing streetscape, character and heritage".

#### 4.1.4 Access and Movement

Respondents tended to favour the provision of underground parking in relation to the Woolworths supermarket. Respondents also indicated a desire for more bike lanes, bike parking areas, as well as a better pedestrian environment (i.e. improved footpaths, lighting, crossings etc).

#### 4.1.5 Public Realm and Open Space

There was general consensus from respondents for an improved pedestrian environment with connections to Hawksburn Station, with a range of proposals suggested from CCTV improvements to improved footpaths.

Opportunities for new open space were suggested at the rear of the Prahran Police Station and/or the Woolworths car park. While a significant proportion (28%) agreed to either of the choices, or both, the majority of comments (32%) suggested other locations for the development of open space.

### 4.2 Stage 3 Consultation

Council engaged Capire Consulting Group (Capire) to support consultation activities and prepare a report for this stage. Feedback was sought on the Draft Structure Plan via survey and email submissions.

In summary, the key findings were as follows:

- The greatest level of support observed was the proposal to incorporate more open space and greenery in Hawksburn Village.
- The most contentious element related to the proposed preferred height limits. Specifically, while some respondents supported height increases over the proposed limits, the majority favoured reducing the proposed limits. The most common reasons for the opposition to the proposed limits were the impacts increased density would have on neighbourhood character, sunlight and traffic congestion.
- Heritage was an important factor to respondents, though clarification was sought on the definition of "sympathetic additions". There were some suggestions that the industrial character was not worth retention while others supported it.

- Access and movement directions that might impede traffic flow such as bicycle lanes were less likely to be supported than other proposed directions. Additionally, respondents frequently noted that directions for managing traffic congestion and car parking demand were not addressed.
- There was universal support for retaining small scale employment in Hawksburn Village. There was also a high level of support for enhancing McKillop Street as a 'Laneway Precinct'.
- Proposed public realm and open space improvements were strongly supported, with some concerns about maintenance of open spaces and reduced traffic access.



## 5.0 The Vision & Structure Plan Framework

### 5.1 Vision

The Vision Statement underpinning the Hawksburn Structure Plan is as follows:

*“Hawksburn Village will continue to thrive as a welcoming, vibrant and diverse centre, and be known for its distinctive charm and village feel, and safe, green, and walkable street network.”*

### 5.2 Themes and Objectives

The Vision for Hawksburn Village is achieved by the realisation of objectives and strategies grouped under the five themes of the structure plan. When brought together in a cohesive and legible way they provide the key ingredients of a highly liveable, vibrant and economically viable inner urban area:

#### Theme 1: Land Use Activities

Overarching objective: ‘A strong local economy that serves the community’

- To strengthen the neighbourhood role of the activity centre ensuring it continues to function as a sustainable and viable centre that meets the needs of local residents.
- To establish a more diversified land use mix within the precinct to enliven the centre.
- To increase Hawksburn’s reputation as a specially retailing destination.
- To retain employment areas on ground and first floors and encourage residential uses above these.

#### Theme 2: Built Form and Heritage

Overarching objective: ‘Facilitate development whilst respecting the heritage and unique character’

- Retain and enhance the village character of Malvern Road.
- Recognise, conserve, and enhance the architectural, cultural and historic significance associated with Hawksburn Village.
- Encourage infill development that retains and enhances the scale, materiality and rhythm of Hawksburn’s architectural style.
- In the western precinct, encourage infill that responds to the industrial character features.
- Ensure the character and amenity of adjacent residential areas is maintained.
- Ensure that any additions, alterations and replacement buildings are sympathetic to the heritage.

#### Theme 3: Access and Movement

Overarching objective: ‘An accessible place for all’

- To improve legibility, safety, convenience and amenity for pedestrians and cyclists moving within, and around, Hawksburn Village.
- To create a high quality pedestrian environment.
- To consolidate, connect, and improve access to public transport.
- To retain and improve laneway connectivity and amenity.
- To reduce private vehicle travel to or around the centre and encourage people to use public transport, walk, and cycle as viable alternatives.

#### Theme 4: Public Realm and Open Space

Overarching objective: ‘Enduring and engaging streets and public spaces’

- To enhance the green character of Hawksburn Village.
- To provide opportunities for additional public open space.
- To identify opportunities for laneway precincts.

- To improve legibility and way-finding.

- To establish a high quality pedestrian oriented public realm along Malvern Road, Hobson Street, and Barnsbury Road.

- To improve north-south pedestrian connectivity.

- Ensure streets and public open spaces in Hawksburn Village are safe, accessible, and have a high level of amenity during day and night time.

#### Theme 5: Sense of Place

Overarching objective: ‘A place with well-defined and unique identity’

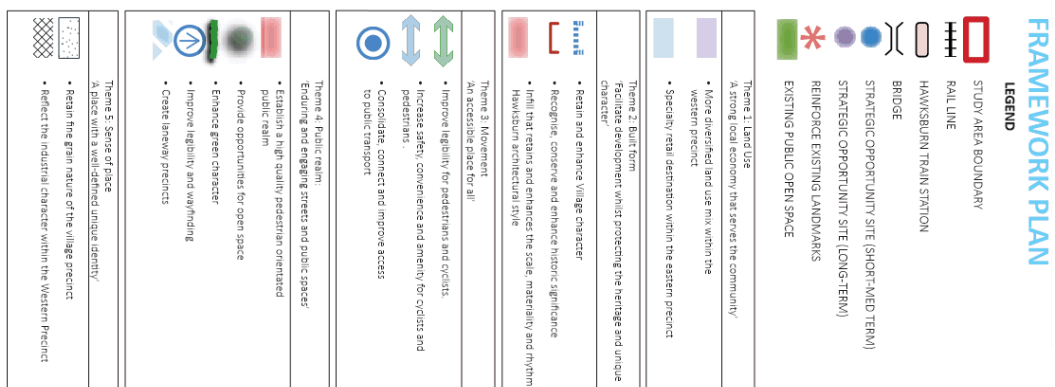
- To enhance locally distinctive patterns of development, landscape and culture.
- Ensure a high standard of design that promotes the unique identity of Hawksburn Village and its residential character precincts.
- To reflect and celebrate the industrial character existing within the Western Precinct.
- To retain the fine grain nature of the Eastern Precinct.
- To enhance, support and celebrate the character of the historic railway infrastructure and its transit function.

- The Framework Plan outlines the overall spatial framework for development in the Structure Plan area. Further guidance is provided within each framework theme as outlined in the following sections of the Structure Plan.

### 5.3 Framework Plan

To articulate the overall spatial framework for the Structure Plan area, a Framework Plan has been developed. The Framework Plan illustrates how development of each precinct combines to deliver a potential development outcome. It articulates the objectives that can be graphically demonstrated in plan form. Further guidance is provided for each framework theme. This is outlined in the following sections of the structure plan.





## 5.4 Land Use Activities

The existing land use pattern is not proposed to be significantly altered as part of this Structure Plan. Rather, it seeks to increase the vibrancy and viability of the Hawkesburn Activity Centre through both supporting the existing mix of employment uses and introducing activities and land uses that expand the function of the centre and better service the demographic now and in the future. This will not only enable the centre to better service local residents and support local businesses, it will also attract ongoing investment to the centre.

### 5.4.1 Retailing

Future retailing opportunities within Hawkesburn Village are expected to relate to food retailing activities reflecting the existing designation of the centre within the Stonnington activity centres' hierarchy.

The well-established Eastern Precinct is expected to continue as a retail location in its current form, reflecting its existing role of neighbourhood centre, serving the local community. Its supply of small format shops contributes to the diversity of offer in the area.

The Western Precinct is distinctly different to the remainder of the centre reflecting its traditional role as a location for semi-industrial and peripheral activities. This precinct is expected to see an increase in retail related activities that are currently not being delivered in the Eastern Precinct. This may include medium sized food retailers that are unique to the region which would in turn increase exposure to more generic uses to occur such as cafes and restaurants.

Cafes and restaurants were identified by the community as a potential opportunity within the centre which would also be supported by continued population growth and the age and socio-economic profile of the local population. By introducing more lively activities to Malvern Road, the village will gain a more safe and vibrant environment both day and night.

There is one smaller format supermarket within the Eastern Precinct of the centre. At 1,100m<sup>2</sup> it is around one third the floor area of a typical full line supermarket. Any expansion of the supermarket would need to consider the availability of convenient access and parking and any amenity impacts on surrounding residential areas.

### 5.4.2 Office and employment space

A key issue identified by the Stonnington Planning Scheme is managing the amount and location of new residential development in activity centres to ensure it does not diminish future opportunities for retail and commercial expansion in core areas.

The existing purpose built and shop-top office space in Hawkesburn Village is a strong contributor to the self-sufficiency and viability of the centre and should therefore be supported and further strengthened through this Structure Plan. With the intensification of sites within Hawkesburn Village, there will be new opportunities for office space at ground and first floors of mixed use development. The opportunities for larger office space may be limited in the Eastern Precinct due to lot size; however the retention of existing office space is strongly encouraged.

The Western Precinct and its larger lot sizes could allow for particular office typologies. A strategy is included that requires investigation into an incentive to incorporate office space via the planning scheme or other mechanism. To accommodate office floor space, the design guidelines provided within this Structure Plan allow for flexibility in the typology of uses on the ground and first floor through floor to floor heights of 4 metres.

### 5.4.3 Housing

The economic analysis undertaken for this Structure Plan suggests there will be a limited number of sites that may potentially be redeveloped for apartment projects given the size and accessibility of sites, the value of existing capital improvements and likely ownership by passive investors. Recent planning approvals within the precinct would suggest otherwise, with some sites within the Western Precinct approved for 5 storey apartment developments. This trend supports the viability of the Western Precinct for medium density redevelopment. The urban design parameters this Structure Plan applies will be the catalyst for the new character that will evolve within the Western Precinct.

### 5.4.4 Opportunity Sites

To contribute to the vibrancy and viability of Hawkesburn Village, the Structure Plan has identified sites that offer good access to services and transport and are capable of accommodating housing. This includes the Woolworths Supermarket and the only at grade car park within the activity centre, which is currently poorly designed in terms of pedestrian and vehicle accessibility and amenity. The car park is in both public and private ownership and has potential for consolidation with the adjoining supermarket site. The Structure Plan recommends investigating the redevelopment of this car park in conjunction with the supermarket site which could include replacement car parking and open space within the new development.

### Objectives

- To strengthen the neighbourhood role of the activity centre ensuring it continues to function as a sustainable and viable centre that meets the needs of local residents.
- To establish a more diversified land use mix within the precinct to enliven the centre.
- To increase Hawkesburn's reputation as a specialty retailing destination.
- To retain employment areas on ground and first floors and encourage residential uses above these.

### Strategies

- Develop land use provisions that support a diversity of employment opportunities.
- Encourage the provision of employment uses on ground and first floor levels in redevelopment proposals.
- Support the development of an evening economy in the Western Precinct suitable to the role and function of the centre.
- Enhance the centre's convenience retailing offer to ensure a high level of service for local residents.
- Provide the opportunity for a diversity of retail uses in the Western Precinct.
- Support high-end fashion continuing its role in the Village, in particular the Eastern Precinct.
- Encourage uses that extend the hours of activity of the Activity Centre and strengthen its function as a place for living and working at the street level.
- Allow opportunities for medium sized food retailers in larger sites in the Western Precinct.
- Undertake further planning investigations for the redevelopment of opportunity sites.

David Lock Associates

Hawksburn Village Structure Plan

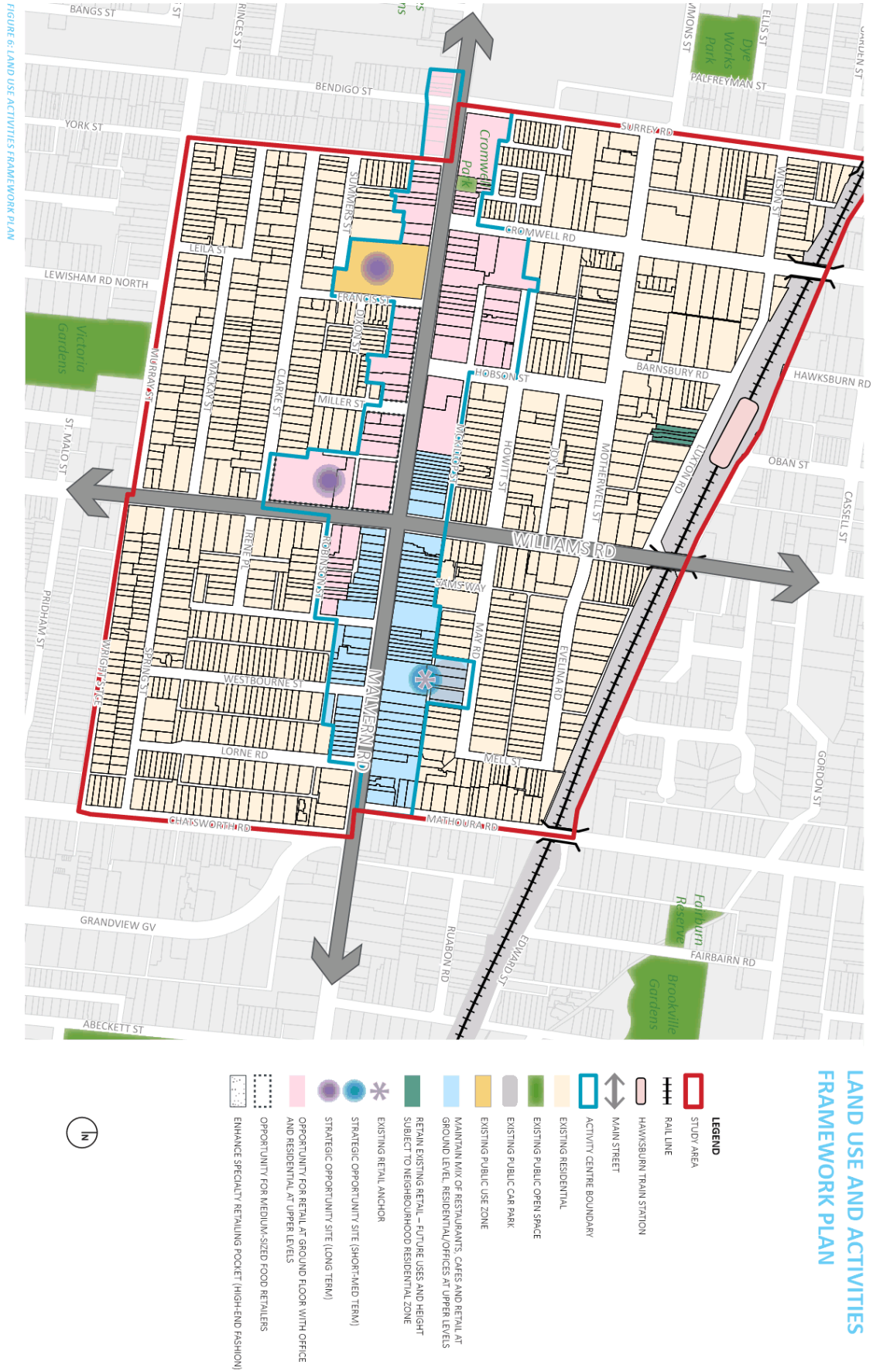


FIGURE 6: LAND USE AND ACTIVITIES FRAMEWORK PLAN



## 5.5 Built Form and Heritage

The study area is comprised of several large heritage precincts (including HO142, HO137, HO380 and HO370) which have contributed to a built form intrinsic to the character and sense of place in Hawksburn Village. The built form and heritage guidance within this Structure Plan seeks to reinforce the existing qualities of Hawksburn Village, whilst carefully managing change.

Given the status of Hawksburn Village in the Planning Scheme as a Neighbourhood Activity Centre, its anticipated future growth is modest compared to Major Activity Centres such as Chapel Street. The scale of built form proposed varies between precincts and is in line with the characteristics of Hawksburn Village including heritage and grain.

Building heights of up to 4 storeys in the Eastern Precinct are proposed. The Eastern Precinct has a strong existing and unique character of fine grain shops and heritage buildings that should be respected and retained. A group of single storey semi-detached buildings immediately west of Lorne Road contribute to the unique character of Hawksburn Village and establish a distinctive presence at a main entrance to the centre. Built in the early 1900s, these are of potential heritage significance. Further investigation should be undertaken to determine their heritage significance and to protect them from development.

In the Western Precinct the Structure Plan anticipates heights of up to 5 storeys. This precinct has a mixed character with identified pockets of wider format industrial buildings and heritage buildings. The Built Form Framework Plan identifies the areas of the centre that have an industrial character that should be referenced in any redevelopment proposal. The industrial character features include a medium grain, large but relatively plain parapets with wide door and window frames and limited ornate features. In these locations, new development should either retain or respond to the existing industrial street wall character.

When responding to heritage buildings, any additions should be set back behind an appropriate volume of the existing building in accordance with Stonnington's heritage guidelines.

There are two buildings identified as landmarks within Hawksburn Village. The Leonard Joel Auction House site (former Hawksburn Primary School) is included on the Victorian Heritage Register and the Bush Inn is located on the north east corner of Williams Road and Malvern Road. Both hold a significant presence in Hawksburn Village and any redevelopment must protect the heritage significance of these buildings.

Generally, where there are no heritage constraints, it is accepted that the street wall of any new development should respond to the surrounding street

wall character, applying a height of 2-3 storeys.

The Built Form Character Framework Plan depicts maximum building heights within each precinct and articulates where an individual design approach is required to consider existing heritage or character aspects. Maintaining reasonable amenity for residential properties adjacent to Hawksburn Village is also a key consideration. Taller development can adversely affect the amenity of adjacent residential properties through visual bulk, overlooking and overshadowing. Such impacts are to be reduced by setting back upper levels at the residential interfaces. The parameters for built form design are provided at Part 6 of this Structure Plan.

### Objectives

- Retain and enhance the village character of Malvern Road.
- Recognise, conserve, and enhance the architectural, cultural and historic significance associated with Hawksburn Village.
- Encourage infill that retains and enhances the scale, materiality, and rhythm of Hawksburn's architectural style.
- In the Western Precinct, encourage infill that responds to the industrial character features.
- Ensure the character and amenity of adjacent residential areas is maintained.
- Ensure that any additions, alterations and replacement buildings are sympathetic to the heritage.

### Strategies

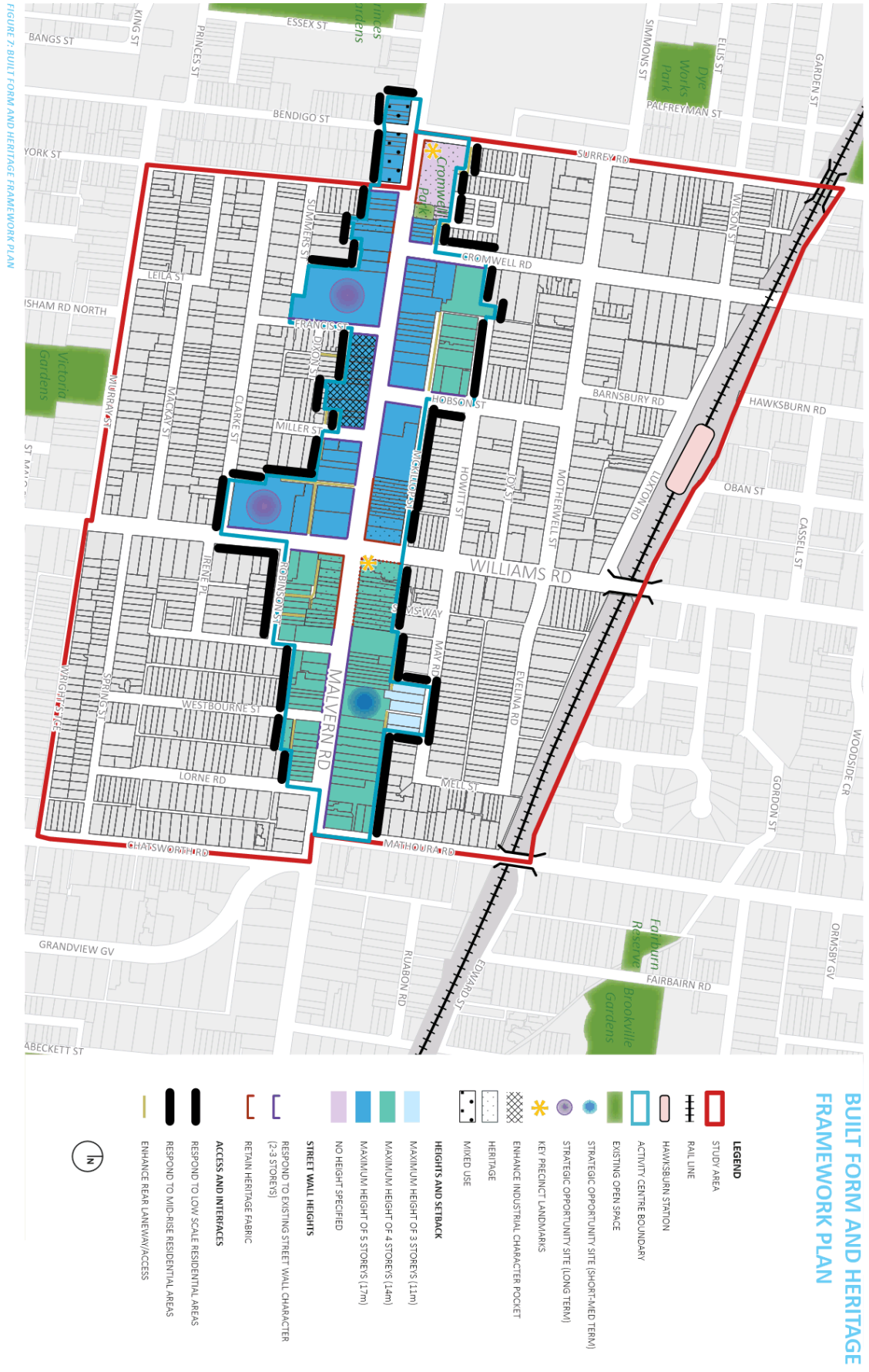
- Building heights to the west of Williams Road should be no greater than 5 storeys (excluding Howitt Street), and building heights to the east should be no greater than 4 storeys, adjacent to Malvern Road.
- Ensure buildings contribute to a vibrant and active streetscape on Malvern Road.
- The street wall height to Malvern Road should respond to the existing built form, creating a continuous built form scale with recessive upper levels.
- Heritage fabric must be retained in accordance with Stonnington's heritage policy and guidelines.
- Redevelopment in or adjacent to areas where industrial character has

been identified should retain or respond to its materiality.

- Ensure new development manages off site amenity impacts and provides a transitional building scale to sensitive interfaces such as open spaces, residential areas or heritage buildings.
- Ensure any future redevelopment of the Hawksburn Primary School is reflective of and celebrates its heritage features.
- Review the heritage significance of properties within the Activity Centre to determine if any heritage values should be further protected.
- Prepare an appropriate planning mechanism to manage built form outcomes in the Neighbourhood Activity Centre.
- Investigate an appropriate planning mechanism to apply to the Woolworths/ May Road car park opportunity site.
- If required, prepare a local planning policy that supports the implementation of the structure plan.
- Any future redevelopment of the 559-565 Malvern Road should be carefully managed to ensure it aligns with the design guidance as outlined within this Structure Plan.
- Ensure any future development of the long-term Opportunity Sites, 145 Williams Road and 396-400 Malvern Road are carefully managed and aligns with the design parameters as outlined within this structure plan.

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Hawkesburn Village Structure Plan





## 5.6 Access and Movement

Hawksburn Village is well serviced by public transport, with frequent tram and bus services running along Malvern Road and train services operating from Hawksburn Station. Malvern Road is nominated as a Tram and Bus Priority Route and Pedestrian Priority Route in the Stonnington Planning Scheme. It is also a VicRoads Primary Arterial Road and Bicycle Priority Route. State policy seeks to manage the road system to achieve integration, choice and balance by developing a safe network and making the most of existing infrastructure. As a neighbourhood centre with moderate growth projected, there are improvements and infrastructure upgrades that can be made to improve accessibility, ease of movement, and choice within the study area.

The key access and movement issues are:

- Lack of cycling infrastructure, including on-road cycle lanes and bike parking facilities;
- Poor connectivity between the train station and Malvern Road;
- Poor pedestrian amenity on Malvern Road due to narrow footpaths and limited signalised pedestrian crossing points; and
- The limited cross-section of Malvern Road and the ability to accommodate its multi-functional role.

There is a distinct lack of cycle lanes and bike parking within the study area.

As a Bicycle Priority Route, the implementation of cycle lanes on Malvern Road should be investigated. Cycle lanes on the most direct route to the train station, Hobson Street and Barnsbury Road, should also be investigated along with the provision of bike parking, including secure bike parking at the train station.

Within the activity centre, many of the lots are serviced by either residential or commercial access laneways. These not only improve permeability within the activity centre, they also hold certain characteristics that are intrinsic to inner Melbourne suburbs and the historical layout of Melbourne.

New development abutting a lane has the potential to affect the lane's functionality with respect to servicing and access, and its desirability as a pedestrian thoroughfare (in some locations). As development occurs there is an opportunity to enhance the amenity of these spaces and improve the perception of safety as well as ensuring service access is not adversely affected.

There is a low level of connectivity between the Station, Malvern Road and across Malvern Road in the western precinct. Establishing a clear and well signed route to the train station will support increased public transport

usage. Signage at the corner of Hobson Street and Malvern Road will help to direct pedestrians to the station. Furthermore, a signalised pedestrian crossing on Malvern Road in the vicinity of Hobson Street will further strengthen connectivity.

Malvern Road has a multi-functional role as the main activity spine for Hawksburn Village and a priority route for cars, cyclists, trams, buses, and pedestrians. Conflicts between these different modes can detract from the vibrancy, amenity, and identity of the area. To ensure the most effective use is made of the limited space available, and encourage people to use alternative transport options, it is important that Malvern Road is balanced in design. To achieve this more emphasis can be placed on pedestrians, cyclists, and public transport users, rather than cars.

### Objectives

- To improve legibility, safety, convenience, and amenity for pedestrians and cyclists moving within and around Hawksburn Village.
- To create a high quality pedestrian environment.
- To consolidate, connect, and improve access to public transport.
- To retain and improve lane-way connectivity and amenity.
- To reduce private vehicle travel to or around the centre and encourage people to use public transport, walk, and cycle as viable alternatives.

### Strategies

- Investigate removal of footpath clutter including superfluous signage and seating within the activity centre.
- As the transport mode shares alter in favour of walking, cycling and public transport, investigate options to widen footpaths in the western precinct.
- Investigate the provision of a signalised pedestrian crossing facility in close proximity to the intersection of Hobson Street and Malvern Road.
- Investigate the opportunity for improvements to the footpath along Hobson Street and Barnsbury Road, including footpath widening and additional landscaping.

- Investigate the opportunity for part road closure on Barnsbury Road between Luxton and Motherwell Streets to increase access to public open space.

- Retain and improve laneway connectivity and amenity through providing lighting, paving and landscaping, ensuring they are safe and inviting spaces. Development must not adversely affect the operation of laneways for servicing and access.

- Liaise with VicRoads to advocate for and investigate the opportunity to install peak hour bicycle lanes, with traffic lane and tram/ vehicle lane, and potentially off-peak parking lane, bicycle lane and tram/ vehicle lane.

- Liaise with VicTrack, PTV and Heritage Victoria when appropriate to enable delivery of cycling facilities including secure bike parking at Hawksburn Train Station.

- Provide bike parking at highly frequented facilities along Malvern Road including the Woolworths Supermarket.

- Create a clear cycling route from Malvern Road to the train station and onwards to the off/on road cycling route extending east/ west.

- Monitor PTV's implementation of DDA compliant tram stops in the activity centre and when appropriate advocate for quality, integrated and accessible design in consideration of connectivity within the activity centre.

- Liaise with PTV to secure additional way-finding on the corner of Hobsons Street and Malvern Road directing commuters to the train station.

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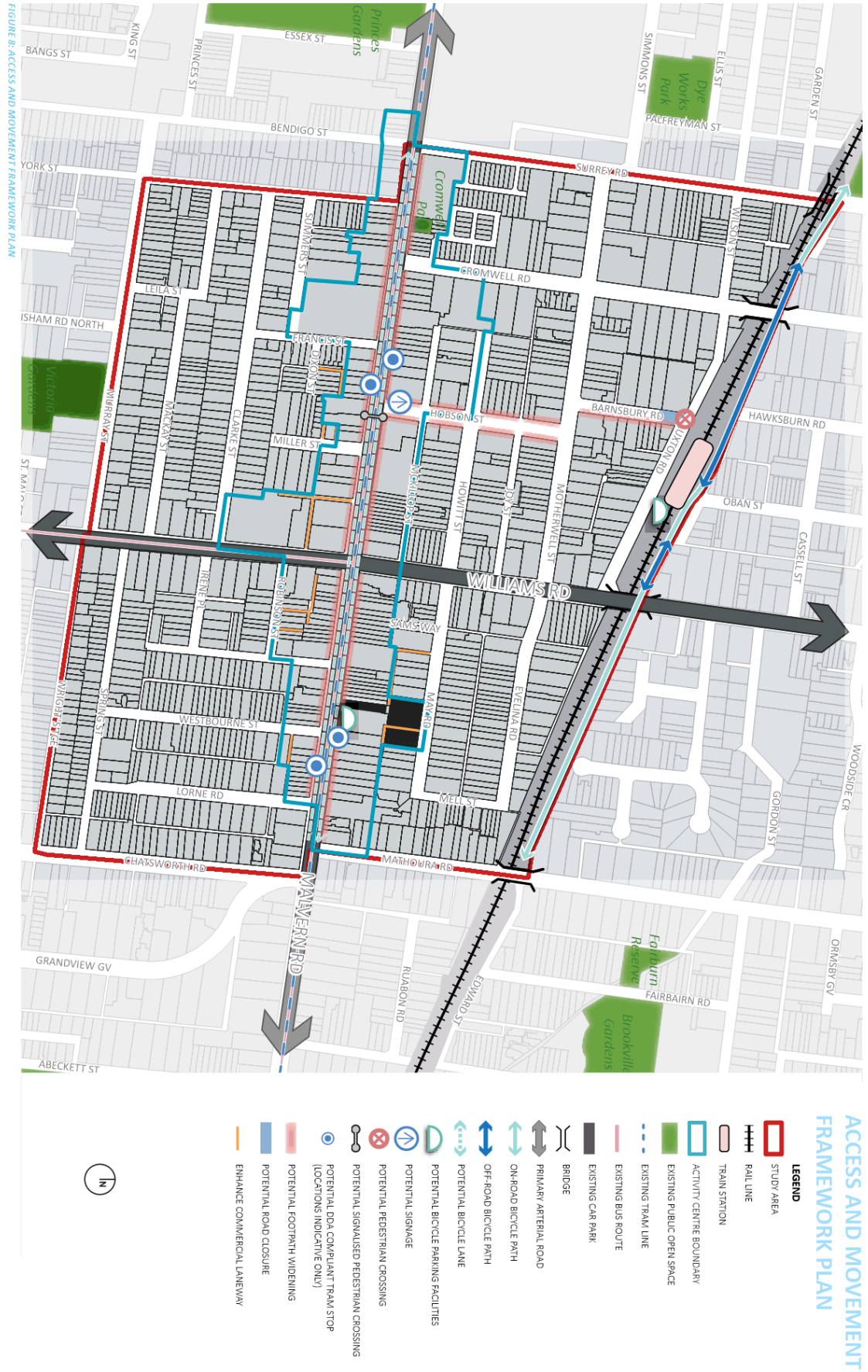


FIGURE 8: ACCESS AND MOVEMENT FRAMEWORK PLAN

5.7 Public Realm and Open Space

There is a shortage of public open space in the activity centre. This includes traditional public open space and space for relief or resting off the main footpaths within Hawksburn Village. With the moderate growth rates predicted within Hawksburn Village, the pressure on the public realm and the demand for new public open space will only increase.

Through the structure planning process and in consultation with the community, several opportunities were identified to create new public open space and improve the amenity and function of the public realm.

Within the Eastern Precinct, there are opportunities to create new public open space, relief, or resting space off the main footpaths and improve the public realm.

559-565 Malvern Road and the surface level car park (28, 32 and 44 May Road) have been identified as an opportunity site. As part of the future redevelopment of this site, there is an opportunity to provide both public open space and relief or resting space off the main footpath. Woolworths is the largest anchor within Hawksburn Village and therefore attracts a larger amount of pedestrian traffic. The site also sits within a pocket of the activity centre where approximately 70% of the dwellings are more than 400 metres walking distance from open space. The provision of open space within the site would address this shortage.

Design guidance is provided at Part 6 of this report.

There are other opportunities for public realm improvements and public open space within the Eastern Precinct which could be investigated including the closure or part closure of north-facing roads on the southern side of Malvern Road.

Within the Western Precinct, an opportunity to expand the small pocket park on the corner of Hobson and Howitt Streets has also been identified for further investigation. The property is privately owned. A further opportunity is for any redevelopment of the Prahian Police Station at 396-400 Malvern Road to consider improvements to its interface with Malvern Road.

A key concern for the community is the retention, and provision, of more greenery in the area. Streets that would benefit from landscape improvements include Clarke Street, Errol Street, Spring Street, Wrights Terrace, Hobson Street, Barnsbury Road, and Luxton Road.

Objectives

- To enhance the green character of Hawksburn Village.
- To provide opportunities for additional public open space.
- To identify opportunities for laneway precincts.
- To improve legibility and way-finding.
- To establish a high quality pedestrian oriented public realm along Malvern Road, Hobson Street and Barnsbury Road.
- To improve north-south pedestrian connectivity.
- To ensure streets and public open spaces in Hawksburn Village are safe, accessible and have a high level of amenity during and day and the night time.
- Investigate the development of McKillop Street as an active and vibrant laneway precinct strengthening the retail and commercial café culture of Malvern Road. Café and restaurant uses can be encouraged with associated streetscape enhancements can include improvements to pavements, activating frontages, lighting, outdoor seating and appropriate signage.
- Investigate lane-way and interface guidelines.

Strategies

- Implement landscaping improvements along Clarke Street, Spring Street, Wrights Terrace, Hobson Street, Barnsbury Road and Luxton Road.
- Investigate opportunities for creating open space in the activity centre including incorporating public open space into the opportunity site at 559-565 Malvern Road and 28-32 and 44 May Road, enlarging the pocket park on Hobson Street, and closure or part closure of north-facing roads.
- Provide a pedestrian link between May Road and Malvern Road through the opportunity site at 559-565 Malvern Road and 28, 32 and 44 May Road.
- Emphasise the connection to the train station along Hobson Street and Barnsbury Road through footpath widening, other public realm improvements and wayfinding measures.
- Investigate the provision of a signalised pedestrian crossing facility in close proximity to the intersection of Hobson Street and Malvern Road.
- Investigate the opportunity for a shared space outcome within Barnsbury Road.
- Investigate an opportunity for the police station site to improve its interface with Malvern Road through landscaping or other measures.



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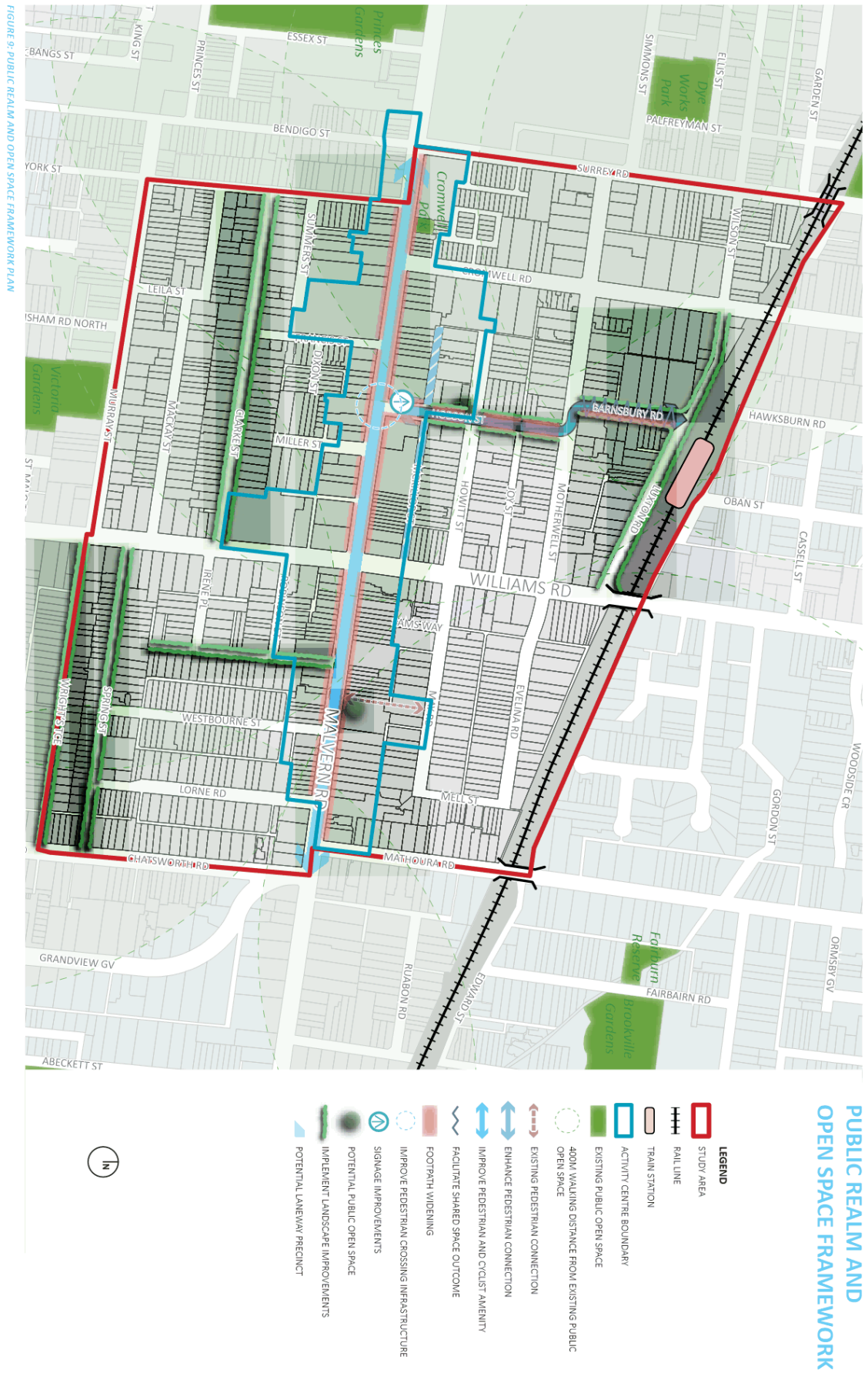


FIGURE 9: PUBLIC REALM AND OPEN SPACE FRAMEWORK PLAN

## 5.8 Sense of Place

Locally distinctive elements define the identity of a place and its look and feel. Hawksburn Village includes many elements that contribute to its sense of place and character which include the public realm, landscaping and built form. The study area has identifiable pockets that have a distinctive identity. Within the Activity Centre Boundary, the Eastern Precinct has an established character of fine grain shops, sculptured landscape and village feel, whilst the western precinct has a more mixed character including pockets of industrial buildings on larger lots and heritage buildings. The residential hinterland within the study area also consists of areas of distinctive character that define how it feels. These are represented on Figure 10 and described further below.

### 5.8.1 Public Realm and Landscaping

It was mentioned in community consultation that Hawksburn's leafy green public realm contributes to its sense of place. It is therefore of particular importance that these features are protected and enhanced moving forward.

Within the activity centre, there is a clear contrast between the public realm landscaping within the Eastern Precinct and the Western Precinct. The Eastern Precinct has a highly sculptured landscape with small, rounded trees that contribute to the village character. Any further landscape enhancements to the Eastern Precinct should reflect this feature. The western precinct has large plane trees which help to enclose the street and provide a distinct difference to the Eastern Precinct.

The residential hinterland includes a diversity of public realm and landscaping treatments. Character Precinct 1 includes narrow streets with smaller landscaping interventions. Spring Street, Wrights Terrace, and Errol Street all hold opportunities for small scale landscape interventions to strengthen the precinct's character. Character Precinct 2 includes wider streets which allow for a more generous landscaping response. Larger landscape interventions can be applied to Clarke Street. Character Precinct 3 includes Cromwell Street, a canopy lined streetscape, which should be enhanced. Character Precinct 4 includes some wider streets, with larger verges and therefore more dominant landscaping. As a main connector to the train station, the existing landscaping along Hobson Street and Barnsbury Road should be enhanced. Precinct 5 includes a highly landscaped public realm on relatively narrow streets.

### 5.8.2 Built Form

From a review of the established character, the built form to the east of Williams Road, which includes a fine grain subdivision with intricate and detailed facades, is a strong contributor to Hawksburn's sense of place. To address this, the structure plan provides design parameters to ensure the village atmosphere is retained. Any future development opportunities must be of a smaller scale, adapting existing spaces and retaining the fine grain nature and diversity in built form.

The western precinct has a contrasting character, which is the result of light industrial buildings, that results in a legacy of larger buildings with plainer detailing and a coarser grain. This in turn creates opportunities for larger development interventions and the ability to create a new sense of place.

The existing light industrial character contribution should not be lost entirely. The retention of certain industrial characteristics within redevelopment sites is a way of acknowledging the past whilst retaining that character type that is upheld in the facades of the industrial buildings.

There are certain landmarks within the precinct that also help to establish a clear arrival points to the precinct. The original Prahran Primary School (now Leonard Joel Auction House) located on the corner of Surrey Road and Malvern Road forms a prominent and unique feature building and marks the arrival point to the Hawksburn Village. Any redevelopment of this site should consider the important role this building plays anchoring the Activity Centre.

The group of six single storey shops to the west of Lorne Road mark the eastern arrival point to Hawksburn Village. This unique grouping contributes strongly to the character of this area and should be retained.

### Objectives

- To enhance locally distinctive patterns of development, landscape and culture.
- Ensure a high standard of design that promotes the unique identity of Hawksburn Village and its various residential character precincts.
- To reflect and celebrate the industrial character existing within the western precinct.
- To retain fine grain nature of Eastern Precinct.
- To enhance, support and celebrate the character of the historic railway infrastructure and its transit function.

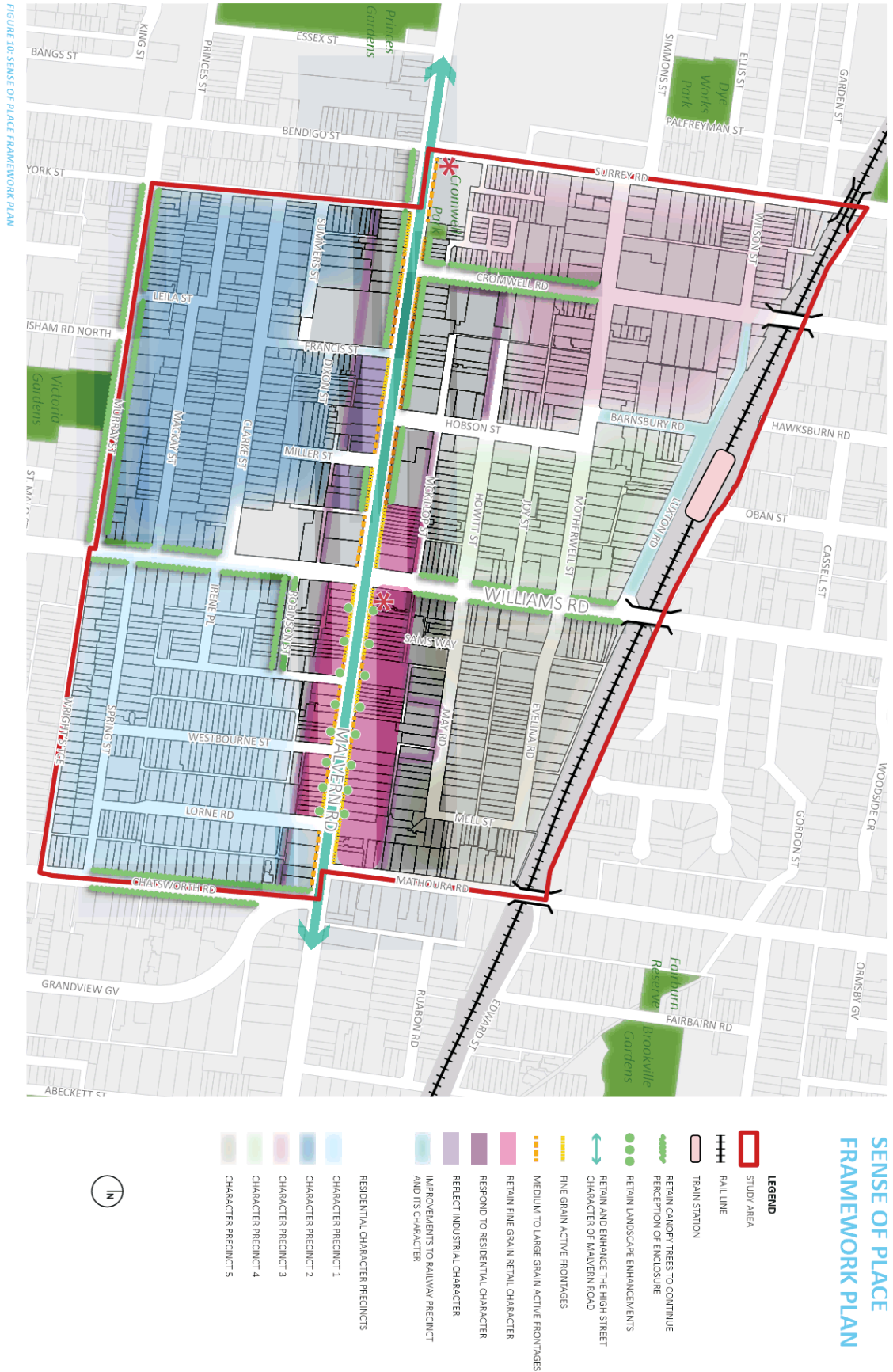
### Strategies

- Redevelopment proposals in the Eastern Precinct should respond to and retain the fine grain nature and diversity in built form.
- Redevelopment proposals in the Western Precinct should respond to and retain the light industrial built form and character including the medium to large grain frontages.
- Preserve and enhance landscape qualities distinct to the eastern and western precincts.
- Apply public realm and landscaping treatments that respond to the residential character precincts identified on Figure 10.
- Ensure improvements to the Railway Precinct to enhance its character and transit function.



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Hawkesburn Village Structure Plan



## 6.0 Precincts and Guidelines

### 6.1 Introduction

The study area comprises two main precincts - eastern and western - as shown at Figure 11. These precincts are envisaged to have a unique future character that is distinctive yet complementary to one another. Articulated in the following sections are visions, objectives, and strategies related to each precinct. Each section is further supported by design guidelines for built form heights and setbacks along with specific design parameters for the identified strategic redevelopment sites that are considered to have the most opportunity for change. The following montage of photographs, read in conjunction with Figure 11, give an indication of the character of each precinct.



1



5



9



2



6



10



3



7



11



4

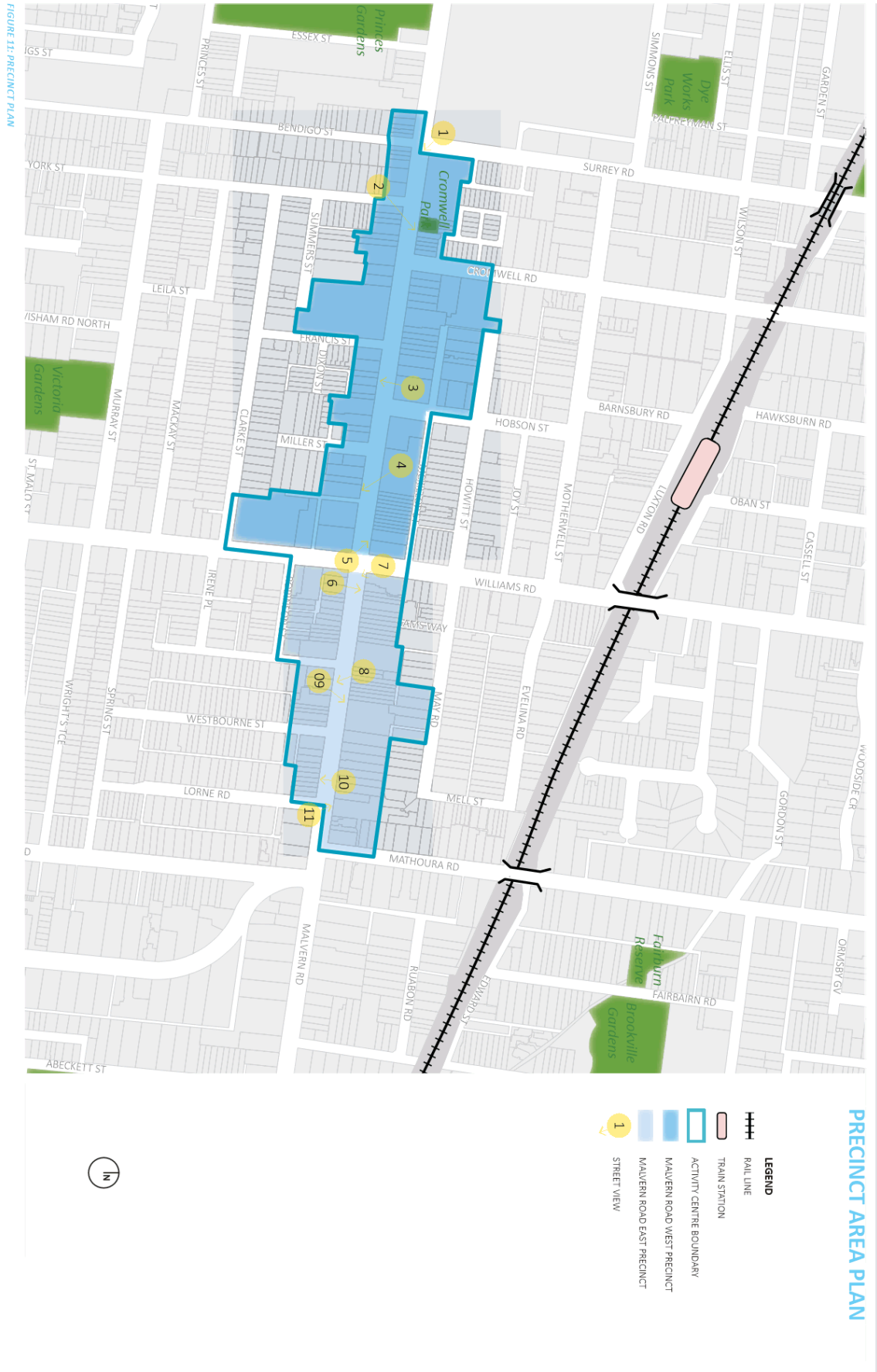


8



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Hawkesburn Village Structure Plan



## 6.2 Western Precinct

The following section identifies the Vision, Objectives and Strategies for the Western Precinct. Further, it identifies opportunity sites and a landmark site of importance outlining how these are to be addressed.

### Vision

*A vibrant retail and commercial precinct with gourmet food retailers unique to the area, cafes and restaurants alike. This will be strengthened by the introduction of a greater number and diversity of dwellings into the mix of uses. The previous light industrial uses are referenced in the buildings which reinforce its unique character.*

### Objectives

- To establish a more diversified land use mix within the precinct to enliven the centre.
- To reflect and celebrate the industrial character within the western precinct.
- To retain and improve laneway connectivity and amenity.
- To improve the connection to and from Hawkesburn Station.
- To consolidate, connect, and improve access to public transport.

### Strategies

- A more diversified land use mix in this precinct to enliven this precinct will be established by:
  - Appropriately designed residential apartments on upper levels;
  - Small-scale employment uses on first floor levels;
  - Retail, including cafes and restaurants, on ground floors; and
  - Redevelopment of underutilised larger sites on the south side of Malvern Road.
- Encourage new development that builds on the existing light industrial

character.

- New developments that access laneways, should provide laneway enhancements to improve amenity and safety.
- Investigate the potential of McKillop Street as a laneway precinct, between Williams Road towards Cromwell Road, to be supported by uses and built form that activate the edges, creating a vibrant mixed use precinct.
- Support additional retail including cafes and restaurants to enliven the precinct.
- Public realm, way finding, and connectivity improvements including from the south side of Malvern Road to Hawkesburn Station along Hobson Street.
- Investigate, consolidate, connect, and improve access to public transport.
- Retain Leonard Joel Auction House (Hawkesburn Primary School) as a key landmark and entry marker to the Western Precinct.
- Investigate opportunities for improving the public realm including enlarging the pocket park on Hobson Street.
- Establish clear design guidance for the Western Precinct including opportunity sites.
- Apply Stonnington's Heritage Policy and Guidelines to sites covered by Heritage Overlays.

#### 6.2.1 Opportunity Sites

To contribute towards the long-term vibrancy and viability of Hawkesburn Village, key sites have been identified in the activity centre boundary. Two are located within the Western Precinct.

##### Prahran Police Station Site (396-400 Malvern Road)

The Prahran Police Station has been identified as an opportunity site due to its significant site, prominent corner location, and rear service lane access. The site is currently Crown land. It is understood that the intention for the site is for the Police Station to remain and to further develop the site as a larger Police Station in the future.

Based on this information, the likelihood of this site being redeveloped in the next 10-20 years is significantly reduced. In light of this, the Structure Plan identifies the site as a Long Term Opportunity Site.

Nonetheless, the site's ability to accommodate development means that design parameters will become a necessity. Therefore, it is recommended that any future review of this structure plan revisits this site and its redevelopment potential at this time. Setbacks have been applied to abutting residential street interfaces in the interim to maintain the sensitive streetscape character. Any future redevelopment of the site must comply with the guidelines presented in Section 6.3. Furthermore, Section 5.9 'Public Realm and Open Space' requires investigation into improvement opportunities to the police station and its interface with Malvern Road.

##### BMW Dealership and Service Centre (145 Williams Road)

145 Williams Road has been identified as an Opportunity Site due to its size, prominent location, and rear service lane access. A recent extension has been undertaken to the building further establishing the car sales use in this location on Williams Road. Therefore the structure plan identifies the site as a Long Term Opportunity Site. Nonetheless, the sites ability to accommodate development means that design parameters will become a necessity. Therefore, it is recommended that any future review of this structure plan revisits this site and their redevelopment potential. Setbacks have been applied to the site where it interfaces with residential streets to maintain the sensitive streetscape character. Any future redevelopment of the site must comply with the guidelines presented in Section 6.3.

#### 6.2.2 Landmarks

##### Hawkesburn Primary School

The Hawkesburn Primary School, now occupied by the Leonard Joel Auction House, is a key landmark and entry feature upon arrival within the activity centre. Covered by Heritage Overlay 76 and listed on the Victorian Heritage Register, it is classified as Grade 'A' significance, is considered a building of state importance, and an irreplaceable part of Australia's built form heritage. Its grading means that it should be retained as a priority. Any demolition or removal of part of a building may be considered where the section of the building to be demolished is not visible from the street and does not contribute to the heritage value of the building and place. Due to the stringent heritage controls applying to the site, it is not necessary to provide any design parameters for the future development of the site. Any future redevelopment of the site will need to comply with the requirements of Heritage Victoria.





## 6.3 Western Precinct Built Form Guidelines

The following built form guidelines provide the design parameters to achieve the vision for the Western Precinct. The guidelines provide guidance for, and respond to, design constraints associated with the public realm, residential interfaces, overshadowing, and amenity. Various interface conditions are identified with guidance provided for them with both text and diagrams.

Areas of industrial character, heritage constraints, or non-heritage forms require different design responses.

Across the Western Precinct, new development should emphasise the existing dominant street wall character. Upper levels should be recessed behind the parapet adequately.

Residential Zone	Public Realm	Residential Interface
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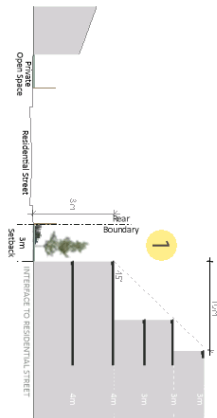


FIGURE 13: RESIDENTIAL STREET INTERFACE – REAR

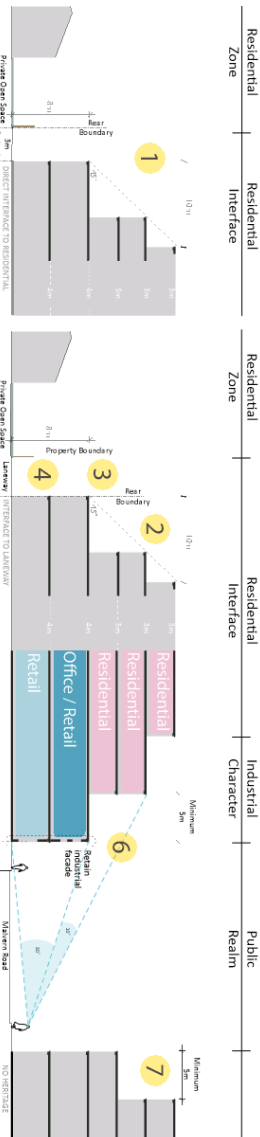


FIGURE 14: RESIDENTIAL DIRECT INTERFACE – REAR

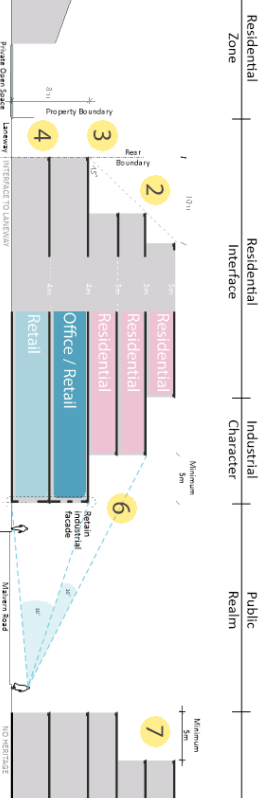


FIGURE 15: STREET SECTION AND LANEWAY INTERFACE – REAR

Rear lane treatments should be designed to respect existing residential amenity. Built form is to be setback to maintain and improve amenity and facilitate the ongoing function of the laneway. A maximum height of 5 storeys is applied across the precinct, subject to design constraints. A maximum height of 4 storeys is applied to buildings fronting Howitt Street.

### 6.3.1 Interface to direct residential or lane way abuttal

1. Rear elevations should step down to an 8 metre wall height to avoid amenity impacts adjacent to residential development outside of the Activity Centre or other sensitive interface. Where there is a direct residential abuttal (no laneway), built form should be setback a minimum of 3 metres from the rear boundary at ground level to manage visual bulk (this is vital for the Opportunity Sites identified). To reinforce existing streetscape character, direct residential street abuttal to the rear should be setback an average distance of the setbacks of the front walls of the existing properties abutting, or 3 metres, whichever is the lesser. No setback is required from a laneway with an existing minimum width of 4.5 metres.
2. Above the first floor level (2nd storey), rear elevations are to be generally setback behind a 45 degree plane (a dimension equivalent to its height) up to a maximum setback of 10 metres to manage visual bulk. Height should increase in 2 floor increments.
3. Laneways must be enhanced with lighting, paving, and landscaping to ensure they are safe and inviting spaces where activated by a new development or use. Development must not adversely affect the operation of laneways for servicing and access.
4. Rear access laneway width should be no less than 4.5 metres for pedestrian access, vehicle movement, and safety.

### 6.3.2 Response to Industrial Character

5. New buildings will retain, or reflect, existing industrial character evident in the facade, where possible. Features to be incorporated include:
  - the medium grain (in the order of 12-14 metres in width);
  - the large and simplistic parapet design in width; and
  - wide door and window frames.
6. The existing industrial street wall height and parapet level should be maintained.
7. To avoid an overbearing relationship with the street, upper levels must be setback 5m from the street wall. This will ensure that the upper levels only occupy no more than one quarter of the vertical angle defined by the whole building in the view from an eye level of 1.7 metres on opposite sides of the street. This ensures the pedestrian field of vision will view a distinct street wall facade.

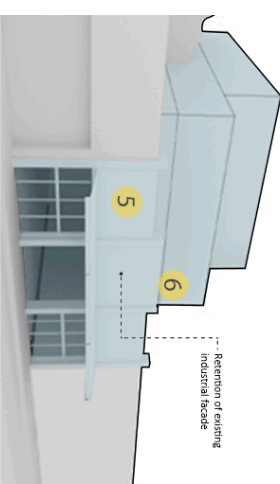


FIGURE 16: INDUSTRIAL FACADE RETENTION

### 6.3.3 Response to Heritage Places

**1.** Where an existing building is of heritage significance, the primary building volume and all significant heritage fabric should be retained in accordance with Stonnington's Heritage Policy and Heritage Guidelines.

**2.** To avoid an overbearing relationship with the street, upper levels must be set back appropriately. For heritage buildings, the upper levels should be set back the same depth as the heritage fabric of the existing building being retained.

The setback should also ensure that it occupies not more than one quarter of the vertical angle defined by the whole building in the view from an eye level of 1.7 metres on the opposite side of the street.

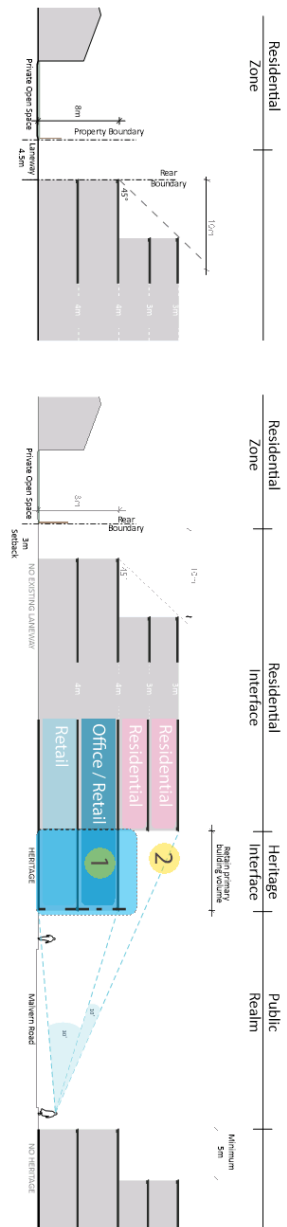


FIGURE 17: LANEWAY INTERFACE

FIGURE 18: STREET SECTION AND DIRECT RESIDENTIAL INTERFACE - HERITAGE

### 6.3.4 Non-Heritage Buildings

**1.** New infill development on infill sites with existing non-heritage buildings should be designed to respond to the existing parapet height on adjoining sites (2-3 storeys).

**2.** To avoid an overbearing relationship with the street, upper levels must be set back appropriately. Where no heritage is present, the upper levels should be set back a minimum of 5 metres from a street wall. The setback should also

ensure that it occupies no more than one quarter of the vertical angle defined by the whole building in the view from an eye level of 1.7 metres on the opposite side of the street.

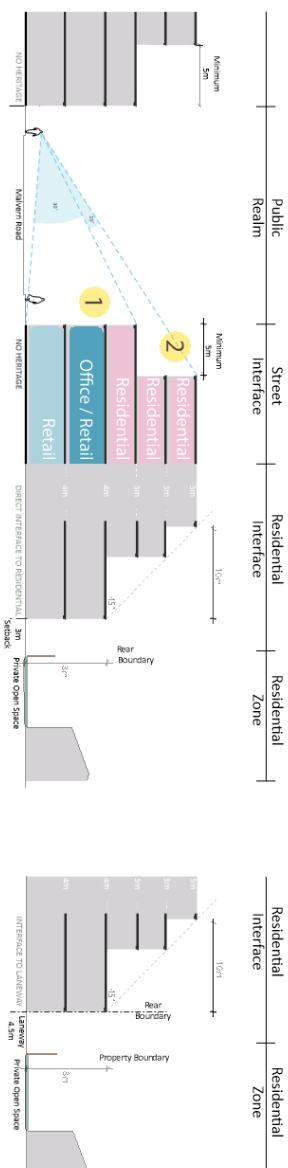


FIGURE 19: STREET SECTION AND DIRECT RESIDENTIAL INTERFACE - NON HERITAGE

FIGURE 20: LANEWAY INTERFACE

## 6.4 Eastern Precinct

The following section identifies Vision, Objectives, and Strategies for the Eastern Precinct. Further, it identifies an opportunity site of importance, outlining how this is to be addressed.

### Vision

*A thriving retail precinct to service the daily needs of local residents, workers, and visitors. The primary retail function is complimented by the precinct's role as a vibrant destination for high end fashion, specialist food stores, and cafes*

### Objectives

- To retain the primary role of Malvern Road as a retail precinct servicing the local community.
- To increase Hawksburn Village's reputation as a specialty retailing destination.
- To protect and enhance the heritage aspects of the Hawksburn Village.
- To retain the 'fine grain' character of the Eastern Precinct of the Village.
- To increase the quantity of public open space and improve the public realm.

### Strategies

- Increase Hawksburn's reputation as a specialty retail destination by:
  - Encouraging retention of fine grain sites and subdivision pattern; and
  - New development responding to the heritage significance of the place.
- Retain the village feel and character of the Eastern Precinct by:
  - New development responding to the existing historic and fine grain character including retention of narrow shop fronts and heritage fabric.
  - New development maintaining existing majority street wall height of 2-3 storeys.
- Retain the six single storey retail buildings immediately west of Lorne Road and fronting Malvern Road, marking the eastern entry to Hawksburn Village.
- Retain the small-scale employment uses on the first floors of buildings to contribute to the long-term viability of the centre.
- New developments accessing laneways should provide laneway enhancements to improve amenity and safety.
- Small scale public realm improvements and upgrades, utilising the existing street network.
- Consider opportunities to provide public open space.
- Any future development of the Bush Inn should reinforce its presence as a key landmark upon entrance to the Eastern Precinct, whilst avoiding an over development of the site.
- Investigate, consolidate, connect, and improve access to public transport.

### 6.4.1 Opportunity Site

To contribute towards the long-term vibrancy and viability of Hawksburn Village, Opportunity Sites have been identified in the Activity Centre Boundary. One is located within the Eastern Precinct.

#### Woolworths/May Road Car Park (559-565 Malvern Road and 28, 32 and 44 May Road)

This site includes the only at grade car park in Hawksburn Village at 28, 32, and 44 which is in both private and public ownership. 559-565 Malvern Road contains the Woolworths supermarket and some associated car parking on the western boundary of the site. This Opportunity Site has been identified because of its significant size when compared to other commercial properties along Malvern Road, its connection through to May Road, and its potential for redevelopment.

The site has a number of constraints including the significant slope running south to north towards May Road which may mean development could be more visible from the sensitive residential areas directly to the north, if not carefully managed. Access to the site needs careful consideration to ensure appropriate pedestrian and vehicle safety in view of the sloping site. The site contains large trees and sits between a heritage precinct to the east and a neighbourhood character overlay to the west.

Any future development of the site must respond to its context and the character of its surrounds including the character and sensitive residential nature of May Road. Further investigation into the issues and options for redevelopment will be required to ensure an appropriate built form outcome is achieved on the site. Guidelines have been developed to direct this investigation.

David Lock Associates

Hawkesburn Village Structure Plan



## 6.5 Eastern Precinct Built Form Guidelines

### 6.5.1 Built Form Guidelines

The following built form guidelines provide the design parameters to achieve the vision for the Eastern Precinct. The guidelines provide guidance for, and respond to, design constraints associated with the public realm, residential interfaces, overshadowing, and amenity. Various interface conditions are identified with guidance provided for them with both text and diagrams.

The Eastern Precinct is dominated by fine-grained buildings with a heritage overlay covering a large component of the precinct. The heritage components of the precinct require particular attention to avoid their loss and the associated loss of valued built form character. This means retention of the primary building volume of the heritage building which may be a limiting factor to the development potential of certain sites.

It is of high importance that the fine grain retail, that is so intrinsic to the character of the Eastern Precinct, is maintained.

New built form should respect the existing street wall and fine grain character that gives the Eastern Precinct its distinct character. New development should emphasise the existing dominant street wall character. Upper levels should be recessed behind the parapet. Rear lane treatments should be designed to address existing residential amenity and visual bulk. Built form is to be setback to maintain and improve amenity and facilitate the ongoing function of the laneway.

A maximum height of 4 storeys is applied across the precinct subject to design constraints. A maximum of 3 storeys is applied to the May Road car park.

### 6.5.2 Interface to direct residential or laneway abuttal

1. Rear elevations should step down to an 8 metre wall height to avoid amenity impacts adjacent to residential development outside of the Activity Centre or other sensitive interface. Where there is a direct residential abuttal (no laneway), built form should be setback a minimum of 3 metres from the rear boundary at ground level to manage visual bulk (this is vital for the Opportunity Sites identified). To reinforce existing streetscape character, direct residential street abuttal to the rear should be setback an average distance of the setbacks of the front walls of the existing properties abutting, or 3 metres, whichever is the lesser. No setback is required from a laneway with an existing minimum width of 4.5 metres.

2. Above the first floor level (2nd storey), rear elevations are to be generally set back behind a 45 degree plane (a dimension equivalent to its height) up to a maximum setback of 10 metres to manage visual bulk. Height should increase in 2 floor increments.

3. Laneways must be enhanced with lighting, paving, and landscaping to ensure they are safe and inviting spaces where activated by a new development or use. Development must not adversely affect the operation of laneways for servicing and access.

4. Rear access laneway width should be no less than 4.5 metres for pedestrian access, vehicle movement, and safety.



### 6.5.3 Response to Heritage

5. Where an existing building is of heritage significance, the primary building volume and all significant heritage fabric should be retained in accordance with Stonnington's Heritage Policy and Heritage Guidelines.

6. To avoid an overbearing relationship with the street, upper levels must be set back appropriately. For heritage buildings, the upper levels should be set back the same depth as the heritage fabric of the existing building being retained.

The setback should also ensure that it occupies not more than one quarter of the vertical angle defined by the whole building in the view from an eye level of 1.7 metres on the opposite side of the street.

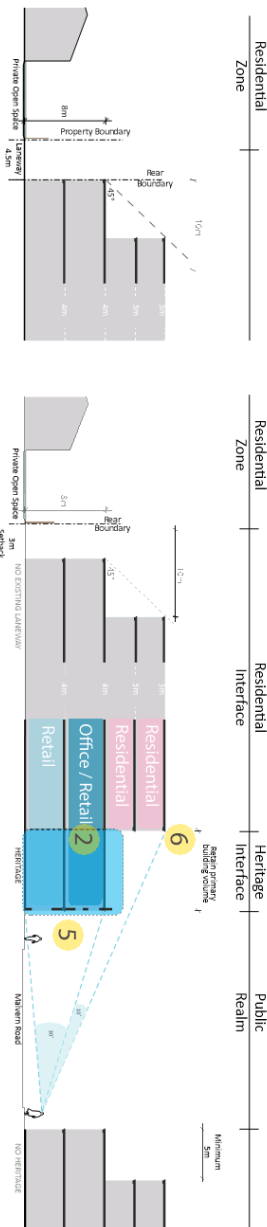


FIGURE 22: LANEWAY INTERFACE

FIGURE 23: SECTION AND DIRECT RESIDENTIAL INTERFACE - REAR

### 6.5.4 Non-Heritage Buildings

7. New infill on infill sites with existing non-heritage buildings should be designed to respond to the existing parapet height on adjoining sites with a street wall height no greater than 2 stories.

8. To avoid an overbearing relationship with the street, upper levels must be set back appropriately. Where no heritage is present, the upper levels should be set back a minimum of 5 metres from a street wall. The setback should also

ensure that it occupies no more than one quarter of the vertical angle defined by the whole building in the view from an eye level of 1.7 metres on the opposite side of the street.

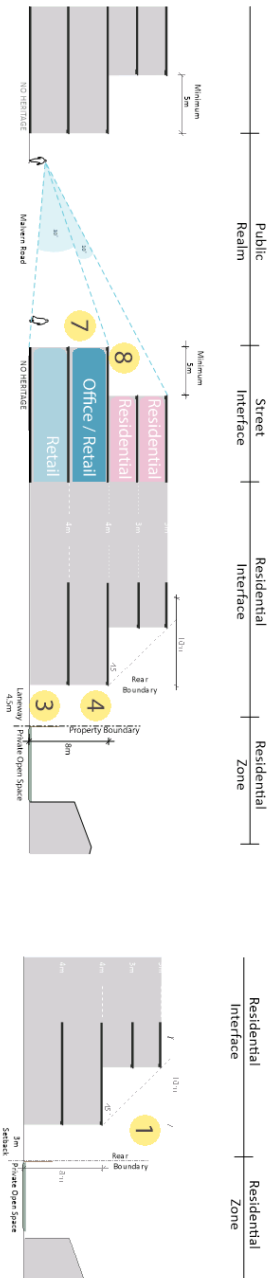


FIGURE 24: STREET SECTION AND LANEWAY INTERFACE

FIGURE 25: DIRECT RESIDENTIAL INTERFACE - REAR

6.5.5 Woolworths/May Road Car Park Opportunity Site

Guidelines

1. Facilitate a consolidated redevelopment proposal for the sites at 559-565 Malvern Road and 28, 32, and 44 May Road.
2. Consider an appropriate design response to the sensitive residential interfaces on May Road.
3. Incorporate public open space on the site. The opportunity to provide a landscaped plaza for pedestrians in addition to a larger area of public open space should be considered.
4. Investigate options to incorporate basement car parking within the site, ensuring no net loss of existing spaces and retaining access for servicing.
5. A maximum height of 4 storeys is applied to new built form on 559-565 Malvern Road. A maximum height of 3 storeys is applied to new built form on 28, 32, and 44 May Road.
6. Investigate an appropriate integrated transport response for the site that considers safe pedestrian and vehicle access, how the site will be accessed by heavy vehicles, opportunities for loading and unloading, and the impact on the local street network.
7. Investigate opportunities to maximise access by walking, cycling, and public transport.

David Lock Associates

Hawkeburn Village Structure Plan

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## 7.0 Implementation and Action Plan

### 7.1 Introduction

The following section establishes an outline for implementation of the Hawkeburn Structure Plan including the key considerations and basis for:

- Planning Scheme Implementation;
- Funding Mechanisms; and
- the Action Plan.

### 7.2 Planning Scheme Implementation

To realise the objectives of this Structure Plan, appropriate planning controls to implement the overarching directions of the Structure Plan will be required. This includes a range of implementation options, including local policies and specific design controls which require formal implementation into the Stonnington Planning Scheme.

Parts of land within the proposed Activity Centre boundary are still within the General Residential Zone (GRZ) and the Neighbourhood Residential Zone (NRZ). To enable the intensification specified within the Activity Centre, alternative zones could be investigated.

### 7.3 Funding Mechanisms

Identifying and securing funding sources will be essential to the implementation program for Stonnington. This must include:

- Ensuring new development contributions are captured from the private sector.
- Budgeting for local infrastructure.
- Budgeting for Federal and State infrastructure.

### 7.4 Action Plan

From the objectives and strategies identified within this Structure Plan, a series of actions have been identified. The following table lists the actions that must be undertaken to achieve the Vision, along with the lead department and State Agency that will need to respond to it.



Action Plan				
Theme	Action	Lead	Stakeholder	Priority
Land Use and Activities	<ul style="list-style-type: none"> <li>Via the Planning Scheme, or other tool, investigate a mechanism which creates an incentive to incorporate office space into mixed use development.</li> </ul>	City Strategy	DELWP	Short term
Built Form and Heritage	<ul style="list-style-type: none"> <li>Undertake a review of sites identified within this Structure Plan as having potential heritage value and that are worth retaining.</li> <li>Propose an appropriate planning mechanism to manage built form outcomes in the Neighbourhood Activity Centre.</li> <li>Investigate and consult on a new public toilet within the activity centre.</li> </ul>	City Strategy	DELWP	short term
		City Strategy	DELWP	short term
	<ul style="list-style-type: none"> <li>Propose an appropriate planning mechanism to apply to the Woolworths/ May Road car park opportunity site and investigated development opportunities for this site.</li> </ul>	City Strategy	DELWP	short term
Access and Movement	<ul style="list-style-type: none"> <li>Investigate removal of footpath clutter including superfluous signage and seating within the activity centre.</li> <li>Liaise with VicRoads to advocate for, and investigate, the opportunity to install peak hour bicycle lanes, with a traffic lane and tram/ vehicle lane, and potentially off-peak parking lane, bicycle lane, and tram/ vehicle lane.</li> <li>In the long term investigate options to widen footpaths in the western precinct as the transport mode splits alter in favour of walking / cycling/ public transport.</li> <li>Investigate the provision of a signalised pedestrian crossing facility in close proximity to the intersection of Hobson Street and Malvern Road.</li> <li>Investigate and establish clear funding mechanisms for the improvements to the footpath along Hobson Street and Barnsbury Road, including footpath widening, additional landscaping, and opportunity for a shared space in Barnsbury Road.</li> <li>Liaise with Bicycle Network, VicTrack, PTV, and Heritage Victoria to enable delivery of cycling facilities including secure bike parking at the Hawkesburn Train Station.</li> <li>Monitor PTV's implementation of DDA compliant tram stops in the activity centre and when appropriate advocate for quality, integrated, and accessible design in consideration of connectivity within the activity centre.</li> <li>Liaise with PTV and seek additional way-finding on the corner of Hobson Street and Malvern Road directing commuters to the train station.</li> </ul>	Public Spaces	Stonnington	medium term
		Public Spaces	Stonnington	medium term
		Transport and Parking	Stonnington	long term
		City Strategy, Strategic Planning and Economic Department and Cultural Development	Stonnington	long term
		Transport and Parking	Stonnington	medium term
		Transport and Parking		long term
		Transport and Parking	VicTrack, PTV and Heritage Victoria	long term
		Transport and Parking	VicRoads and Yarra Trams	long term
		Transport and Parking	PTV, VicRoads	long term
Public Realm and Open Space	<ul style="list-style-type: none"> <li>Investigate opportunities for public realm improvements and the incorporation of public open space in the Eastern Precinct of the activity centre.</li> <li>Investigate opportunities for public realm improvements and the incorporation of public open space in the Western Precinct of the activity centre.</li> <li>Implement landscaping improvements along Clarke Street, Spring Street, Wrights Terrace, Hobson Street, Barnsbury Road, and Luxton Road.</li> <li>Prepare design guidelines for the McKillop Street potential laneway precinct</li> </ul>	City Strategy, Public Spaces	DELWP	medium term
		Public Spaces	Stonnington	medium term
		Public Spaces, City Strategy	DELWP	medium term



Level 2/166 Albert Road  
South Melbourne 3205  
Victoria  
Australia  
t: +61 3 9682 8568

Studio 111, 50 Holt Street  
Surry Hills 2010  
NSW  
Australia  
t: +61 2 9699 2021

[www.dlaust.com](http://www.dlaust.com)

Twitter: @DLA\_Australia

Plantastic Blog: [dla-plantastic.blogspot.com.au/](http://dla-plantastic.blogspot.com.au/)

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