



Hawksburn Village Structure Plan

Final Discussion Paper

For Stonnington City Council

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Malvern Road, Toorak

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1.0 Introduction

David Lock Associates has prepared this Discussion Paper to present the economic, traffic and transport, and urban design analysis for Hawksburn Village. The purpose of the analysis is to explore the issues and opportunities to be addressed within the Hawksburn Village Structure Plan and to define the activity centre boundary.

1.1 Background

The Hawksburn Village Activity Centre has been identified as the next priority in Council's strategic planning program for its activity centres. This is in response to the centre's importance as a local centre. There are a range of development pressures and planning issues impacting upon Hawksburn Village at present. The purpose of preparing a Structure Plan is to undertake a comprehensive strategic plan in accordance with a shared vision for the centre.

It is intended that the structure plan will set out a strategic framework for the future development of the activity centre to 2040. The structure plan will guide the current and future form and function of Hawksburn Village in accordance with a shared vision. It will guide public and private sector actions related to land use, built form, movement networks, public realm and open spaces. It will also provide a framework for the integration of Council's services and programs, existing policies and strategies as well as identify the role of the activity centre within the broader context of the City of Stonnington and metropolitan Melbourne.

1.2 Regional Context

Located 4.5km away from the Melbourne CBD and abutting the South Yarra/Prahran Principal Activity Centre (PAC), Hawksburn Village is a desirable inner urban area with a local village feel. It has important physical, social and economic links to both these areas of activity.



Figure 1 Regional Context

1.3 Study Area

A study area for the structure plan has been defined by Council as shown in Figure 2. The study area encompasses the properties along both sides of Malvern Road, between Mathoura and Chatsworth Roads in the east through to York Street and Surrey Road in the west. It is bounded by the railway corridor to the north and by Murray Street and Wrights Terraces to the south.

To the general public the Hawksburn Activity Centre may be considered to be the commercial properties that front both sides of Malvern Road. This linear strip centre is approximately 650m in length and is divided into two parts.

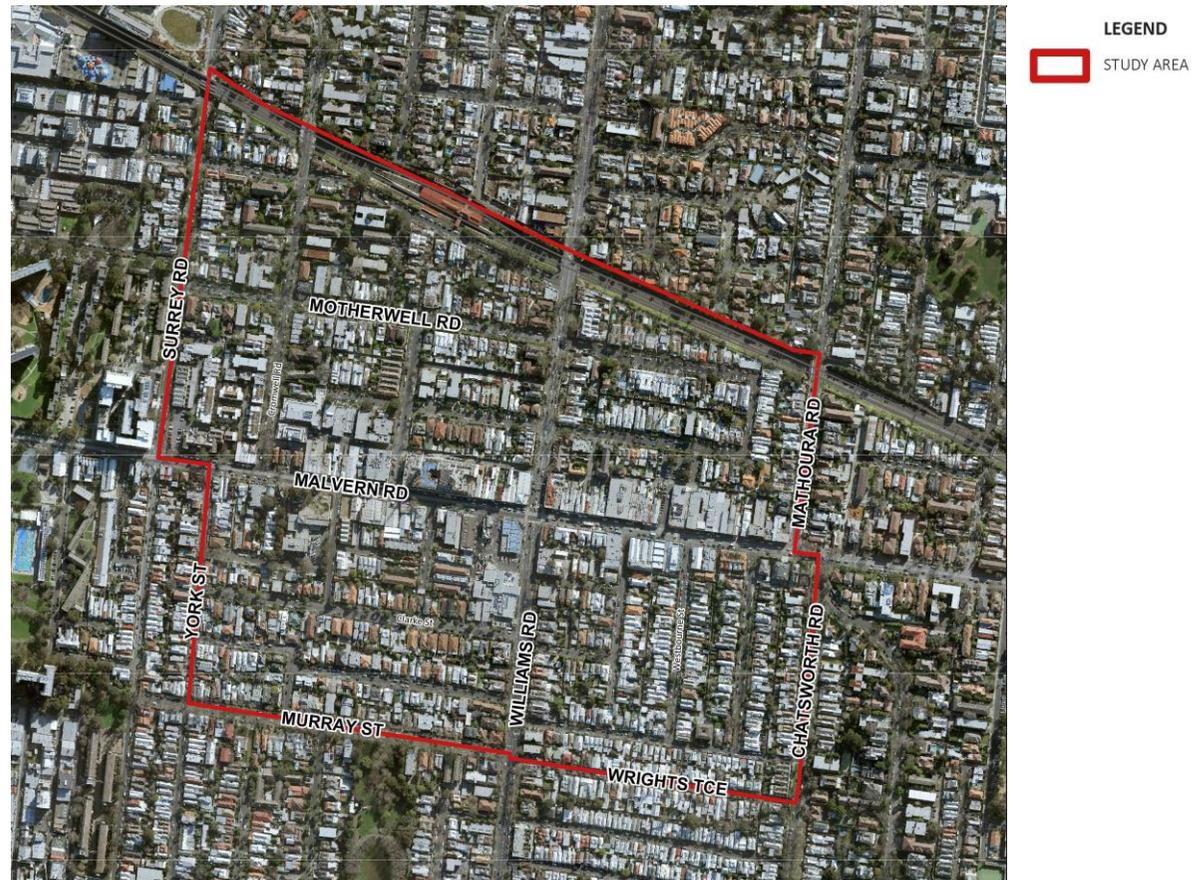


Figure 2 Study Area

1.4 Defining Precincts

The Hawksburn Activity Centre can be described as having two distinct precincts, with each precinct having its own well-defined and distinct character separated by Williams Road.

The eastern precinct (east of Williams Road) is predominantly defined by the Woolworths supermarket which acts as a major anchor and therefore supports a busy retail precinct.

The western precinct (west of Williams Road) is less intense and contains service industries, showrooms and other retailing, however it is currently evolving through the development of a large mixed use building which will contain 120 dwellings.

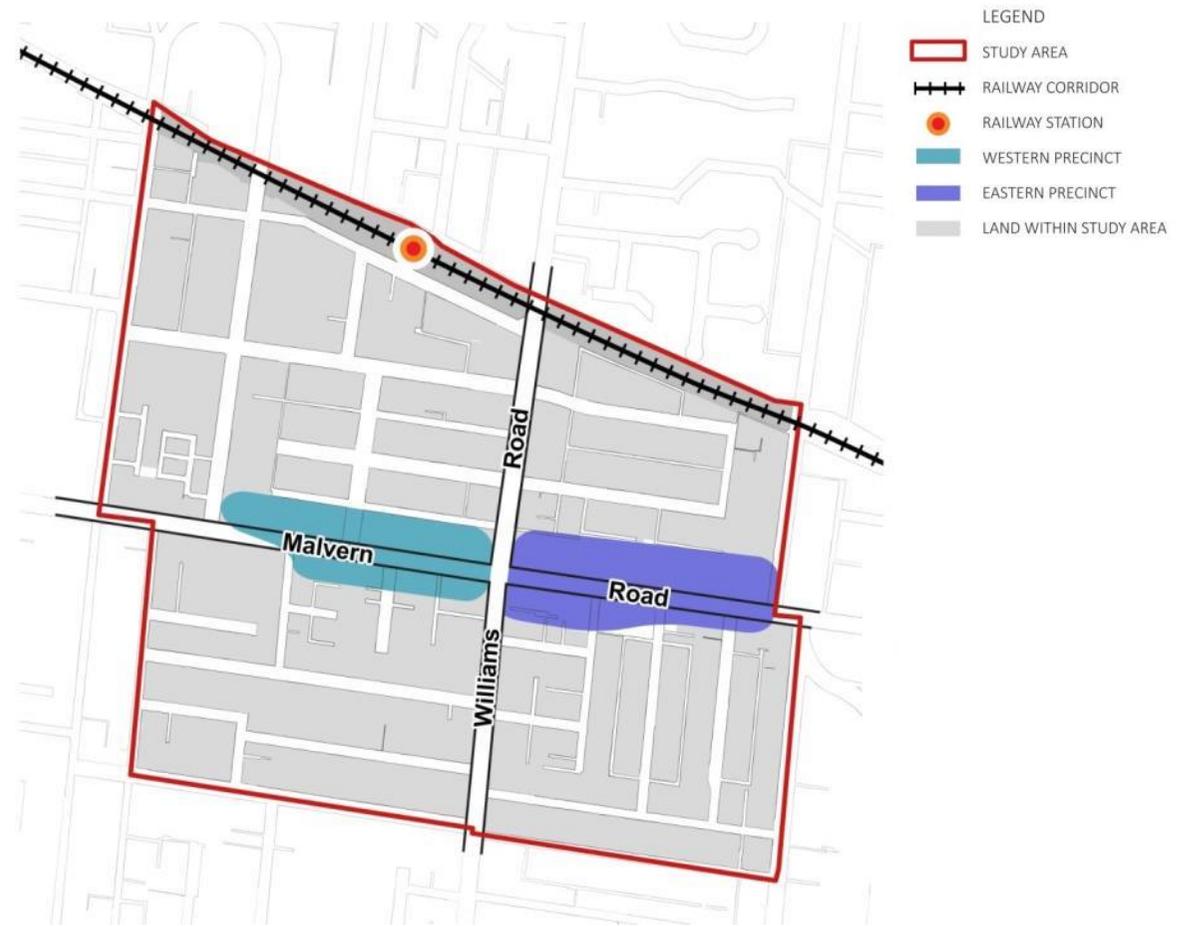


Figure 3 Hawksburn Village Precincts

2.0 Community and Stakeholder Consultation

This section outlines and summarises the Stage One of the community and stakeholder consultation undertaken to date (at the commencement of the Structure Planning process).

Gerard Coutts & Associates were engaged by the City of Stonnington to undertake Council officer/department consultation. The purpose of this discrete engagement was to inform key Council staff of the Hawksburn Village Structure Plan and seek their input at the beginning of the process.

2.1 Community Consultation

Council broadly consulted with the wider community using various communication methods. Two quarter page advertisements were placed in the Stonnington Leader on 11 and 18 November 2014, and a Facebook advertisement which reached 9,700 users. Further, a total of approximately 4000 postcard questionnaires were sent out to businesses and residents within the Study Area and surroundings. Council received valuable feedback from 174 individuals. A series of broad observations have been drawn from this engagement process to date, as follows:

- Respondents would like the Hawksburn Village in 2040 to be a welcoming, vibrant and trendy centre. Hawksburn Village is to be a connected, community focused environment which not only retains its charm and village feel but also offers a gourmet destination with quality and stylish shopping options. It is anticipated that the Hawksburn Village of 2040 will be a greener, less congested, safe, pedestrian and bike friendly environment. Respondents would like the Village not to be over developed but manage future growth pressures.
- Streetscape and open spaces emerged as a significant theme. Respondents expressed a desire for more trees, open green space and public meeting spaces or plazas. The establishment of a green break between the residential and commercial shopping strip was raised as highly desirable. The widening of the footpaths to provide increased space for vegetation, increased capacity for trading on the footpath and accessibility for pedestrians were also recognised as important issues for future planning. There is an opportunity for the improvement in the design and availability of street furniture and public toilet facilities.
- Improved traffic management, reduction in congestion on Malvern Road and a desire for an increase in parking options or facilities were viewed as significant issues within the Village precinct. Off-street parking is seen to be seriously limited. An underground car park, or an additional level at the back of the Woolworths supermarket, were suggested as possible solutions to address this concern. Further to this, addressing the entry and exit points of the supermarket, particularly in relation to delivery trucks, was also seen as an opportunity to assist with improved traffic movement.
- The walkability and accessibility of the Village by foot was greatly valued by respondents. Continued support for public transport usage and improvements in bicycle infrastructure were seen as positively contributing to the communities' future health and wellbeing.
- The preservation and protection of existing buildings and facades was seen as highly desirable. Their contribution to the distinct charm and character of the area as important heritage assets makes it desirable for considered planning controls. To maintain the integrity of the heritage assets, respondents wish for future developments in the Village and surroundings areas to be of small scale and low rise.
- The range and diversity of the shops, cafes and dining options provides a cosmopolitan atmosphere in the Village which respondents believe is not only unique but even attracts visitors from outside the area. There was a strong desire by respondents to ensure the maintenance of not only independent retailers but retailers which are an appropriate fit. Such retailers that mix with the Village atmosphere are actively encouraged.

2.2 Stakeholder Consultation

A consultation workshop was held in Council chambers on 2 December 2014 and a report documenting this workshop was prepared by Gerard Coutts & Associates.

Below is a summary of the staff workshop feedback:

Streetscape and open spaces

Opportunities to Consider:

- Protection of streetscapes.
- More trees, more green, more urban forest.
- Widen footpaths.
- Water sensitive urban design.
- Pause points for seating and shade options.

Heritage

Opportunities to Consider:

- Discover the things that make Hawksburn a distinctive place and protect and build on these.

Planning

Opportunities to Consider:

- Clear height controls and setbacks.

- Increased levels of car-free residential development around Hawksburn station.

Built Form

Opportunities to Consider:

- Protection of small scale residential housing.

Traffic movement, safety

Opportunities to Consider:

- Reduce car parking rate, but consider impact on local streets.

Shops and services

Opportunities to Consider:

- Strengthen economic viability of commercial precincts.

Well-being

Opportunities to Consider:

- Walkability.
- Connectivity.
- Keep people moving.

Other

Opportunities to Consider:

- Planning consultation in a range of languages.
- Balance.



Council officer workshop on 2 December 2014

2.3 Theme Identification

The Stage One: Information Gathering and Visioning community and stakeholder consultation undertaken by Gerard Coutts & Associates identified seven themes which emerged from the various engagement activities. These themes are:

- Shops and services.
- Community.
- Streetscape and open space.
- Heritage.
- Built form.
- Traffic movement and safety.
- Transport and Accessibility.

2.4 Implications for Structure Plan

However, in keeping with the Ruby Town Structure Plan template we will draft the Hawksburn Village Structure Plan under the following broad themes:

- Land Use and Activities.
- Built Form and Heritage.
- Public Realm and Open Space.
- Access and Movement.

- Sense of Place.

The seven themes which emerged from Stage One of the community and stakeholder consultation will be covered by the five themes of the Structure Plan as follows:

Land Use and Activities

- Shops and services.
- Community.

Built Form and Heritage

- Built Form.
- Heritage.

Public Realm and Open Space

- Streetscape and open space.

Access and Movement

- Traffic movement and safety.
- Transport and Accessibility.

Sense of Place

3.0 Economic Analysis Summary

Economic analysis of the Hawksburn Activity Centre was undertaken by Charter Keck Cramer (CKC). The following represents a summary of their key findings.

3.1 Context

Hawksburn's central location within Melbourne's relatively affluent inner south-eastern suburbs provides a strong basis for supporting specialty and niche retailing activities. This is reflected in the dominance of fashion retailing over local convenience retailing that would typically be found in neighbourhood centres of a similar size.

Given the smaller format Woolworths supermarket and a limited range of food specialties, the Hawksburn Activity Centre performs a secondary role to that of Chapel Street and Toorak Village in meeting local household's weekly shopping needs. The potential for an expansion of traditional food retailing within the centre is however limited by the size and characteristics of the existing Woolworths site as well as the absence of appropriately sized sites elsewhere within the centre.

Continued population growth within the surrounding area serviced by the centre will generate a growing requirement for a greater number and variety of retailers that may be easily accessed via sustainable transport modes including walking and cycling. Based on population projections prepared by Id Consultants for the suburbs of Prahran and Windsor, the combined population for these two suburbs is expected to increase by 17% over 2011-2016 and by a further 10% over

the period of 2016-2031. These numbers are based upon an assumed level of infill residential development.

The Indicative Trade Area represents the area from which the

majority of visitors to the centre may be expected to originate (Figure 4). Assuming that the population of the Indicative Trade Area increases at a similar rate, approximately 20,000



Figure 4 Indicative Trade Area (extracted from CKC report)

residents would be expected to live in the area by 2031. Hence, approximately 2,000 additional dwellings (based on an average household size of 1.75), are expected in the Indicative Trade area by 2031.

The centre will also need to service a broader range of income groups as new apartment developments attract a greater diversity of residents.

Recent State planning reforms provide greater flexibility for retailing activities across the centre, particularly within the western precinct which has traditionally comprised light industrial and peripheral sales retailing. While there is the potential for industrial style properties to be redeveloped to accommodate new activities, it is more likely that existing buildings will be adapted to accommodate new businesses. This reflects the likelihood that properties are held by passive investors as longer-term income generating assets.

Property market conditions in the Hawksburn activity centre suggest that it is in a stronger trading position than many other prime retail strips which is likely due to a strong connection with the local community. This provides some assurance that greater flexibility for retail uses following the introduction of State planning reforms will result in a more vibrant centre offering a wider mix of businesses.

3.2 Retailing

Future retailing opportunities within the Hawksburn activity centre are expected to relate to food retailing activities

reflecting the existing designation of the centre within the Stonnington retail hierarchy, as well as broader industry trends which favour activities less exposed to economic conditions and online retailing.

The nature of food retailing will however need to reflect the current provision of traditional supermarkets as well as the opportunities presented by a relatively affluent local population and the centre's accessibility from across a wider region. Similarly, there is the opportunity to complement key food retailing destinations such as Prahran Market through focusing upon niche segments such as gourmet and ethnic food retailing. At the same time there is the opportunity to better meet the needs of local residents through retailers operating longer hours than the Prahran Market.

Existing industrial buildings offer the opportunity to accommodate medium sized food retailers that are sufficiently unique to attract visitors from across a wider region, which in turn will provide exposure for more generic activities such as cafes and restaurants.

3.3 Hospitality

Cafes and restaurants were identified by the community as a potential opportunity within the centre given the relative under-provision that currently exist. This would be expected to be supported by continued population growth and the age and socio-economic profile of the local population.

Cafes and restaurants may establish within the western precinct given the likely synergies with the gourmet and ethnic food retailers identified above. The availability of the Victorian shop fronts on the northern side of Malvern Road would be expected to provide appropriately sized accommodation for these businesses.

3.4 Housing

The potential for apartment development within the Hawksburn activity centre has already been demonstrated with the Hawksburn Apartments currently under construction as well as more broadly within the surrounding inner south-east region.

It is however likely that there will be a limited number of sites that may potentially be redeveloped for apartment projects given the size and accessibility of sites, the value of existing capital improvements and likely ownership by passive investors. It is also expected that the opportunity for higher density housing within the centre will be determined by urban design considerations rather than property market conditions.

3.5 Commercial Offices

The potential for any significant level of office development within the Hawksburn activity centre would be expected to be limited by:

- Apartment projects representing a higher and better use for sites based upon current property market fundamentals and construction costs.

- Strong competition from recognised office precincts within Melbourne’s inner south and south-east regions including South Yarra, Richmond and St Kilda Road.
- There may however be a willingness from some developers to include commercial floorspace where they may wish to diversify their exposure away from the residential apartment market. Current market conditions would suggest that this may be limited to a small portion of any development and would most likely require car parking dispensation from Council. However, given the potential for a softening in the inner city apartment market at some stage in the future, combined with the lack of new office development since the onset of the Global Financial Crisis, there may be some shift in market conditions in favour of office suites. This may provide the opportunity to address the loss of employment identified by SGS Planning and Economics in their analysis undertaken for the Chapel reVision precinct.

3.6 Strategic Direction

The Hawksburn activity centre comprises two distinct precincts either side of Williams Road, each with very different opportunities for a change of land use reflecting their existing business mix and potential to accommodate new activities.

Whereas the eastern precinct is expected to continue to perform its existing role as a neighbourhood centre servicing local residents, the western precinct will offer the opportunity for an expansion of retail and hospitality activities to better meet the needs of local residents and attract visitors from across a wider region.

Through focusing upon non-traditional food retailing activities the centre’s western precinct will complement existing retailers within Toorak Village, Chapel Street and the Prahran Market and provide local residents with a greater diversity of food retailing opportunities. Establishing the precinct as a regional food destination will also complement the existing fashion retailers within the centre’s eastern precinct while also providing exposure to potential customers for hospitality related activities.

The opportunities for apartment development within the centre will be largely guided by urban design considerations. Similarly, any office development within the centre is likely to be minimal and as part of a larger mixed-use development.

4.0 Traffic and Transport Analysis

In addition to an economic assessment, a traffic and transport analysis was also commissioned for the activity centre to inform the structure plan. The following represents a summary of the key findings of the Transport Assessment undertaken by One Mile Grid.

4.1 Traffic

Malvern Road and Williams Road provide obvious barriers, with traffic volumes in excess of 15,000 vehicles per day on Malvern Road, and in excess of 17,000 vehicles per day on Williams Road, splitting the study area into four quadrants.

Traffic congestion in the area is largely limited to the Malvern Road/Williams Road intersection, with some queuing of traffic observed at the signalised intersection of Malvern Road with Surrey Road, and at the sign controlled intersections of Malvern Road with Mathoura Road and Chatsworth Road.

Despite this, traffic speeds are well controlled in the area, particularly the western portion, with area speed limit signage controlling all local roads. It is understood that an expansion of the 40km/h area speed limit is being contemplated to the south eastern quadrant.

4.1.1 Malvern Road

Malvern Road is approximately 20.5m wide and is currently intended to accommodate trams, buses, bicycles, pedestrians and general traffic (under the Smart Road User Hierarchy). The Smart Road User Hierarchy places all of these users at the same priority. Considering the limited cross-section there is

an opportunity to modify the Road User Hierarchy to promote certain users as higher priority than others.



Figure 5 Road Hierarchy and Traffic Volumes (map extracted from One Mile Grid report)

4.2 Parking

Parking on Malvern Road and within the Woolworths car park was observed to be highly utilised during observations, which is supported by parking survey information, which suggests that both Malvern Road and the May Road car park are effectively at capacity between approximately 10:00am and 2:00pm on Weekdays and Saturdays.

The surrounding area is well provided for in terms of parking restrictions, with a good mix of unrestricted, permit restrictions and time-restricted parking.

Parking is largely unrestricted on Saturdays (or Sundays).

4.3 Public Transport

The centre is well served by public transport, with trams and buses running along Malvern Road and close to Hawksburn train station. Limited use of public transport was observed in the area outside of the commuter peak periods, although these observations were undertaken during Christmas and school holiday periods.

4.3.1 Trains

It is noted that a large number of train services are express through Hawksburn Station, particularly during peak periods. However, train frequencies remain high and are considered suitable.

4.3.2 Trams

Malvern Road is served with tram line No. 72, providing three non-DDA (Disability Discrimination Act) compliant tram stops located within the study area. Hence there is an opportunity to modify one of the existing tram stops within the study area to be DDA compliant to provide some form of level access.

It is noted that the IMAP (Inner Melbourne Action Plan) Potential Public Transport Network Improvements indicate the potential for a new tram super stop on Malvern Road, west of Surrey Road. However, installation of a tram stop would result in loss of on-street parking.

4.3.3 Buses

Buses run along Malvern Road and Williams Road with bus stops along Malvern Road typically located in proximity to the existing tram stops. Potential combined tram stops and bus stops would minimise the impact on on-street parking.

4.4 Pedestrians

Pedestrian activity in the area was observed to primarily occur along Malvern Road, and predominantly within the eastern section of the study area. In spite of high traffic volumes on Malvern Road and Williams Road and existing signalised crossings on Malvern Road, uncontrolled crossing movements occur. There is a potential to explore an additional formalised pedestrian crossing point within the study area, particularly in the vicinity of Francis Street and Hobson Street.

Along Malvern Road and William Road, some form of threshold treatment is provided to highlight pedestrian priority and slow traffic. However, these treatments do not provide sufficient level difference and are often in a state of disrepair. It is recommended to improve pedestrian safety on both roads by providing more level pedestrian crossings.

Some pedestrian activity outside of commuter peak periods was observed in the vicinity of Hawksburn Station, although activity in this area is concentrated to peak commute times.

Limited pedestrian activity was observed traveling between the station and Malvern Road, or through other areas of the precinct, though it is noted that observations occurred largely over the summer holiday period. It is also noted that limited pedestrian signage information is provided within the area, to direct pedestrians between Malvern Road, Hawksburn Station and other locations in the study area.

4.5 Bicycles

4.5.1 Bicycle Routes

Despite the fact that Malvern Road is nominated as a bicycle priority route, the cycling infrastructure along Malvern Road is limited to six (6) bicycle parking rails (see figure 6). Some bicycle usage was observed during inspections, though due to the time of year (being outside school periods), it was expected to be below normal.

A discontinuous shared path is provided along the railway reserve and by Luxton Road which provide an alternative east-west route. It is noted that the Travel Smart Map indicates Luxton Road and Wilson Street as part of an informal bicycle route.

4.5.2 Bicycle Parking

A number of bicycles were observed parked on Malvern Road, utilising either the bicycle parking rails provided (see Figure 6), or pedestrian fencing or sign poles. However, the footpath width along Malvern Road is limited and therefore bicycle parking needs to be considered carefully.

Hawksburn station has limited bicycle parking. A number of bicycles were observed to be parked near Hawksburn Station, utilising street furniture in the area for bicycle parking.



Figure 6 Existing Cycling Infrastructure (map extracted from One Mile Grid Report)

4.6 Loading

The vast majority of retail premises fronting Malvern Road have access to a rear laneway, although the laneway is typically narrow, with limited opportunity for turning or passing vehicles. Consequently, any loading operations via the rear laneway would typically be restricted to small vans and utility vehicles. Observations suggest that loading activity rarely occurs via the rear laneways, with the exception of Woolworths, which has a large formal loading area to the rear, with access assisted via the adjacent car parking area and laneway.

A number of on-street loading areas are provided within the area, predominantly along Malvern Road.

Given the size of the retail premises along Malvern Road, on-street loading and parking areas are expected to accommodate the majority of loading activity.

4.7 Crashes

A total of 31 crash locations (where each intersection, or each length of road between intersections is considered to be a location), were recorded in the study area, with crashes occurring within the most recent 5 year period.

Higher crash rates are typically expected on Arterial Roads, and at or near signalised intersections.

The crash data indicates that there have been 5 crashes at the intersection of Williams Road and May Road, however it appears that this figure is inflated. A review of the Crash data

identifies that four of the five crashes involved southbound vehicles, and rear-end, side-swipe or overtaking crashes. It is anticipated that these are likely due to traffic congestion from the Malvern Road intersection, and not specific to May Road.

No other trends are observable from the data.

5.0 Urban Design Analysis Summary

An urban design analysis has been undertaken to draw together the economic and transport findings, and consider how the existing environment of the study area can influence the direction of the structure plan.

The scope of the analysis was guided by Practice Note 58 to enable a thorough understanding of the existing and emerging structure and function of Hawksburn. The following pages provide an overview of an Urban Design Analysis prepared in the first stage of this process.

Key findings of the Urban Design Analysis are as follows:

5.1 Land Use and Activities

Activity is concentrated along Malvern Road, where there is a combination of mixed use and retail developments. There is a distinct transition across the intersection of Malvern Road and Williams Road, with retail focused to the east of Williams Road, and mixed use concentrated to the west. As part of the mix of uses to the west, there are some light industrial uses such as car repairs, tyre and furniture shops that contribute to the broader economy of the centre.

Aside from a small cluster of mixed use lots on Luxton Road opposite Hawksburn station, the remainder of the study area comprises general residential use.

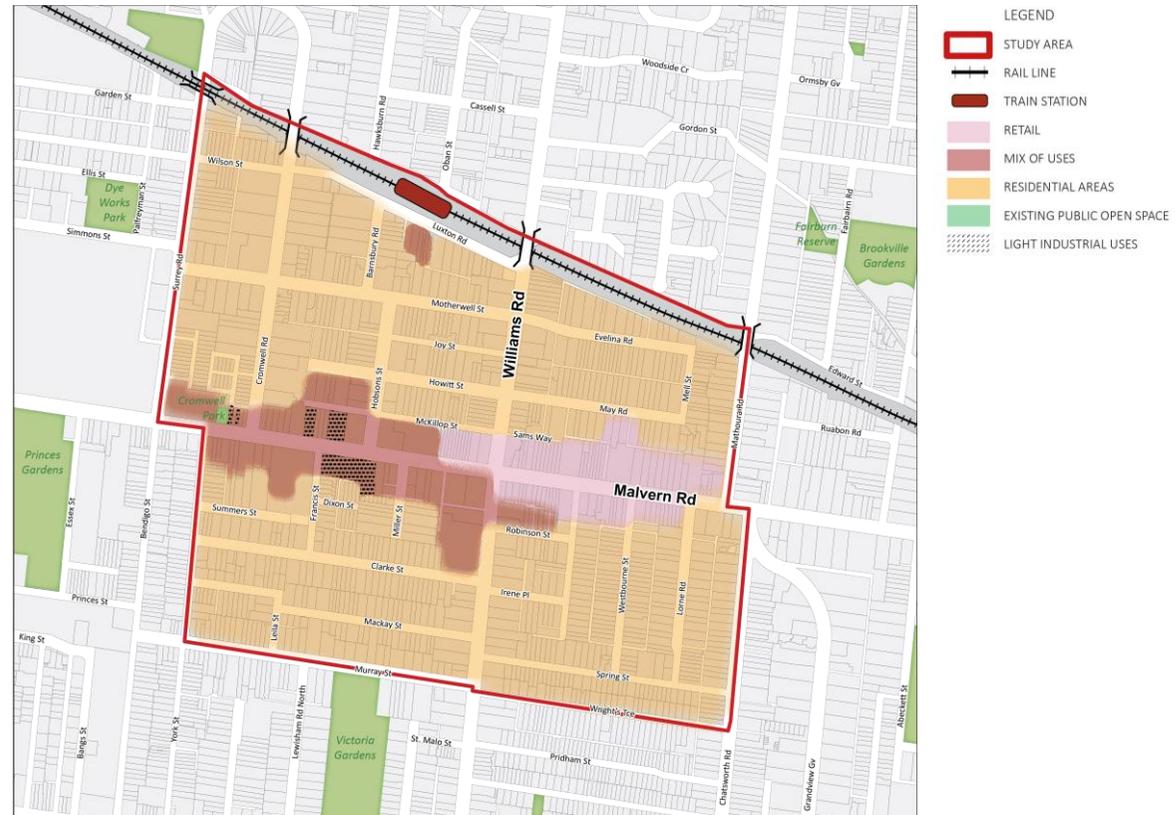


Figure 7 Land Use and Activities

5.2 Built Form and Heritage

Hawksburn generally comprises older Victorian era shops and houses with ornate facades and high ceilings.

The eastern precinct of Malvern Road has a 'traditional high street' streetscape character created by fine grain 1-2 storey Victorian and Edwardian retail and commercial buildings built to the street edge and side boundaries. The prevailing pattern of heights is approximately between 8 and 9 metres including parapets. This predominant street wall height is punctuated by some single storey properties and the Woolworths site. At ground level, Malvern Road has a strong level of activity due to the retail and mixed use developments present.

The western precinct of Malvern Road consists of a mix of medium grain light industrial, commercial and mixed use developments. In contrast to the eastern precinct, it has a less dominant street wall character. The 5 storey mixed use development at 441-473 Malvern Road continues the street wall height of adjacent Victorian era shops, comprising a 3 storey street wall, with recessed upper levels. The length of its frontage along Malvern Road will create a new built form character which at present will be unique to the study area. Further to this, on the southern side of the western precinct, 426 Malvern Road is proposed to be developed as a 5-storey mixed use development.

To the north of Malvern Road, properties fronting Howitt Street are 3-4 storeys (as identified in Figure 8). Howitt Street (east of Hobson Street) comprises residential development. Howitt Street (west of Hobson Street) comprises a mix of residential and office uses. The location of sensitive residential interfaces abutting the Malvern Road lots is illustrated in Figure 8. The predominant residential interface is to the rear of commercial and retail lots, separated by a laneway. Very few lots along Malvern Road have direct abuttal with side boundaries of residential lots.

A large proportion of the residential areas abutting commercial and retail areas are under heritage overlays, and as such, minimum change is anticipated. The interface between these lots and areas of potential redevelopment in adjoining commercial/retail areas are considered highly sensitive.

5.2.1 Capacity Calculation

A key component of structure planning is determining what the future Hawksburn Village will look like, its potential development capacity and how it responds to the growth predicted within the Municipality.

As stated in Section 3.0, the economic analysis completed suggested there are approximately 2,000 additional dwellings (based on an average household size of 1.75) expected in the Indicative Trade Area (ITA) by 2031. The ITA includes the study area but extends south to Dandenong Road, west to the edge of Chapel Street, east to Orrong Road and north to the rail

way line in the north. Based on the land take up (i.e. the proportion of lots that can be developed, in this case for residential purposes) associated with the study area versus the ITA, an assumption is made that Hawksburn Activity Centre should aim to accommodate one third (approx. 600) of the additional dwellings.

The following sections provide an analysis of the potential developable area in Hawksburn and yield based on some assumptions regarding achievable intensification levels and height increases.

Assumptions

To help determine the redevelopment potential of Hawksburn Village, a series of assumptions were built. These factors as they relate to Hawksburn include:

- **Lot size and shape** - Small, narrow and/or awkwardly-shaped lots are more difficult to develop, unless neighbouring properties are in the same ownership.
- **Non-adjacent lots** - Properties in the same ownership may be unlikely to amalgamate due to physical separations; for example, by a lane-way or possibly by a property under separate ownership.
- **Strata titles** - Properties in multiple ownership arrangements are less likely to be developed.

- **Heritage** - Properties under a heritage overlay are less likely to have development potential.
- **Building age** - Newer buildings are less likely to be redeveloped.
- **Building height** - Taller buildings that are already close to the future height envisaged for the area are unlikely to be redeveloped.
- **Significant slope** - Properties on a significant slope may be more difficult to develop
- **Sensitive interfaces** - Sensitive neighbouring uses, such as housing may limit redevelopment potential.

Based on the influencing factors outlined above, across the Activity Centre, the take up rate of redevelopment was set at 25%. In order to come to a development capacity for the activity centre, the following assumptions were developed.

Development Scenarios

Typical lot widths vary throughout the Activity Centre. Malvern Road, east of Williams Road, typically has lots approximately 5m wide. West of Williams Road, lots are typically either 6 to 10m wide. As the majority of the properties are narrow, it is assumed that consolidation of lots will be required in order to create viable redevelopment opportunities.

Based on the ability to accommodate car parking and retail on the ground floor with apartments at upper levels, it is

anticipated that 2 or 3 lot amalgamations would be required. Offices on the upper levels are not part of the proposed scenarios as the parking rate required is approximately twice that of apartments.

Based on the typical lot widths, a 3-lot amalgamation assumes a width of 15 to 18m. Lots of 18m widths would meet the minimum requirement to accommodate a double aisle car park accessed via the rear lane-ways. Lot depths average around 40m.

Design and Development

For the purpose of capacity testing, we have assumed a maximum height limit of 4 storeys in the eastern precinct and 5 storeys in the western precinct. The height limits applied are due to existing heritage constraints, lot sizes, as well as market and community acceptance.

It is assumed that new buildings will have zero side setbacks, with all accommodation facing the street or the rear of the property. This optimises use of the land on equitable development principles, ensures good activation and passive surveillance of the public realm, ensures good internal amenity, and is consistent with an 'urban character'. Further, it is assumed that ResCode Standard B17 will provide the basis for setbacks from rear boundaries adjacent to residential properties.

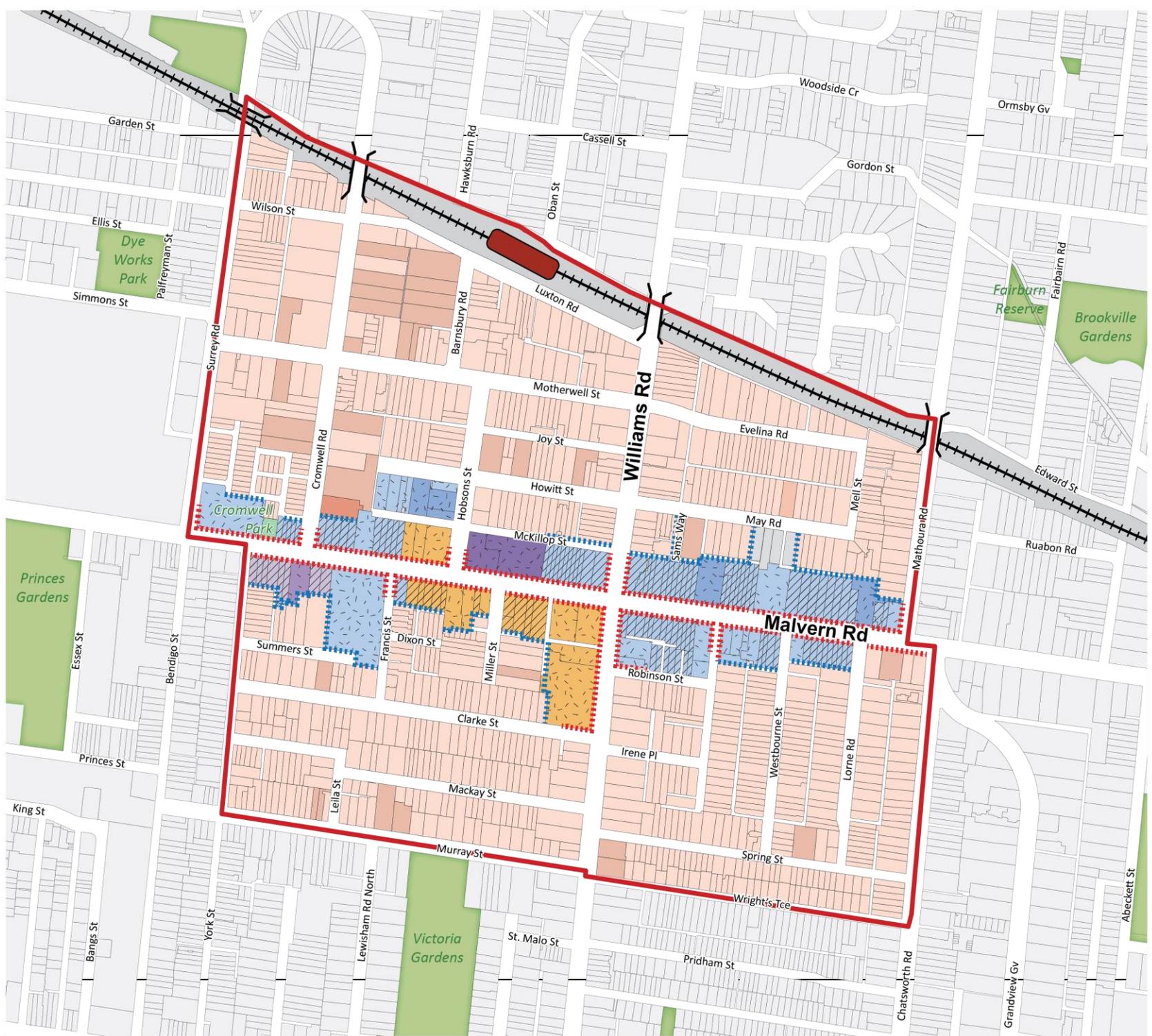
Given the activity centre location, it is assumed that commercial space will be provided at the ground level of all properties. The depth of the ground floor space is determined

based on the car park layout, ensuring functional commercial space and enabling a central location for the core. It is assumed that vehicle access will be from rear lane-ways where they exist.

Results

The analysis confirmed that, with allowable height increases of 4-5 storeys within the Activity Centre Boundary along with a projected 25% take up, the Activity Centre can accommodate approximately 500 new dwellings. The addition of approximately 500 dwellings will generally enable the housing growth projections anticipated by the Economic Analysis and also provide an adequate balance between change, growth accommodation and the retention of Hawksburn's unique village quality.

FIGURE 8 BUILT FORM AND CHARACTER



- LEGEND**
- STUDY AREA
 - RAIL LINE
 - TRAIN STATION
 - PUBLIC OPEN SPACE
 - ACTIVE STREET FRONTAGE AT GROUND FLOOR
 - SENSITIVE RESIDENTIAL INTERFACE
 - 1-2 STOREY RESIDENTIAL
 - 3-4 STOREY RESIDENTIAL
 - 1-2 STOREY MIXED-USE
 - 3-4 STOREY MIXED USE
 - 5+ STOREY MIXED USE
 - 1-2 STOREY RETAIL/COMMERCIAL
 - 3-4 STOREY RETAIL/COMMERCIAL
 - 1-2 STOREY LIGHT INDUSTRIAL
 - SMALL LOT (4-8m WIDTH)
 - MEDIUM-LARGE LOT (8+m WIDTH)
- NOTE: No distinction has been made with residential land uses as stripe relates to lot size



5.3 Access and Movement

The study area is well served by public transport with numerous options including train, tram and buses. Williams Road and Malvern Road are the two main arterial roads that run through the study area.

Malvern Road is the main channel for public transport and pedestrian and cycle movement within the study area, with both trams and buses operating along it. This both promotes, and is aided by, the existing retail uses along Malvern Road. While Malvern Road is considered a bicycle priority route, it has a distinct lack of cycling infrastructure.

As shown in Figure 9, currently there is both an off-road and on-road shared path that extends along the northern side of the train corridor which connects to a link that crosses the railway line ending at Luxton Road. The link however, does not extend through to Malvern Road, therefore limiting pedestrian and cycling access in the western precinct.

As indicated in Figure 9, the study area has a significant amount of laneways. Their narrow and intimate nature and bluestone features help to define the local character and sense of place within the study area. The majority of retail properties along Malvern Road are accessed by laneway. Similarly, many residential properties are accessed by laneways.

The Hawksburn train station is relatively isolated from the other forms of public transport. There is no direct access from Malvern Road, where the majority of pedestrian activity is.

On-street parking is provided within the area with the majority being restricted spaces. There is only one surface car park in the study area, behind Woolworths with access from Malvern and May Roads.

**FIGURE 9
ACCESS AND
MOVEMENT**



- LEGEND**
- STUDY AREA
 - RAIL LINE
 - TRAIN STATION
 - TRAM LINE
 - TRAM STOPS
 - BUS ROUTES
 - BUS STOPS
 - PRIMARY ARTERIAL ROAD
 - ON-ROAD BICYCLE PATH
 - OFF-ROAD BICYCLE PATH
 - KEY PEDESTRIAN MOVEMENTS
 - SIGNALISED PEDESTRIAN CROSSING
 - COMMERCIAL ACCESS LANEWAY (WITH LOADING ACTIVITIES)
 - RESIDENTIAL ACCESS LANEWAY (WITH GARAGE ACCESS)
 - CAR PARK
 - DEAD END ROAD
 - BRIDGE



5.4 Public Realm and Open Space

The amenity of the public realm varies within the study area, from a combination of leafy streets in residential areas to the highly utilised public realm along Malvern Road.

Both Malvern and Williams Road have a lack of public realm amenity, with limited signalised pedestrian crossing points, signage, cycle lanes and cycle parking hoops. Hobson Street forms an important pedestrian link from the train station. Its intersect with Malvern Road does not include a signalised crossing point for pedestrians heading south or signage directing pedestrians towards the train station at this point.

Several residential streets have poor landscape quality as compared to the other streets. These include Luxton Road, Clarke Street, Errol Street, Spring Street and Wrights Terrace.

The only public open space within the study area is Cromwell Park on Malvern Road. However, the study area is close to several public open spaces. An analysis of properties within 400m walking distance of these open spaces shows that the majority of the study area to the west of Williams Road is within the 400m walking distance. However, more than 70% of the properties to the east of Williams Road are outside the 400m walking distance. Hence, there is a need to identify opportunities for new public open spaces in the study area.

The pedestrian amenity and public realm around Hawksburn Station at Luxton Road creates a generally underwhelming arrival experience. Hobson Street and Barnsbury Road are

identified as an important pedestrian link between Hawksburn Station and Malvern Road. Similarly, Luxton Road provides access to Hawksburn station. However, these streets display a relatively poor pedestrian environment. This is primarily as a result of the associated land uses along them, the general lack of activation and interaction at street level and relatively poor streetscape conditions.

**FIGURE 10
PUBLIC REALM AND
OPEN SPACE**



- LEGEND**
- STUDY AREA
 - RAIL LINE
 - TRAIN STATION
 - EXISTING PUBLIC OPEN SPACE
 - 400M WALKING DISTANCE FROM EXISTING PUBLIC OPEN SPACE
 - POOR LANDSCAPE QUALITY
 - ↔ IMPORTANT PEDESTRIAN LINK
 - ↔ POOR PEDESTRIAN AND CYCLIST AMENITY
 - POOR PEDESTRIAN CROSSING INFRASTRUCTURE



5.5 Sense of Place

A location's sense of place can be determined by a multitude of factors that contribute to its authenticity. Hawksburn has many areas that contribute to its unique character.

The largely fine grain subdivision and building form in the eastern precinct are generally attractive places for a pedestrian environment and contribute to its sense of activity. Currently Malvern Road presents a contested space that is shared by various road users. The eastern precinct has a dominant retail and commercial character. The character of the western precinct is industrial. The varying composition of footpaths also creates a sense of incoherence and unattractiveness.

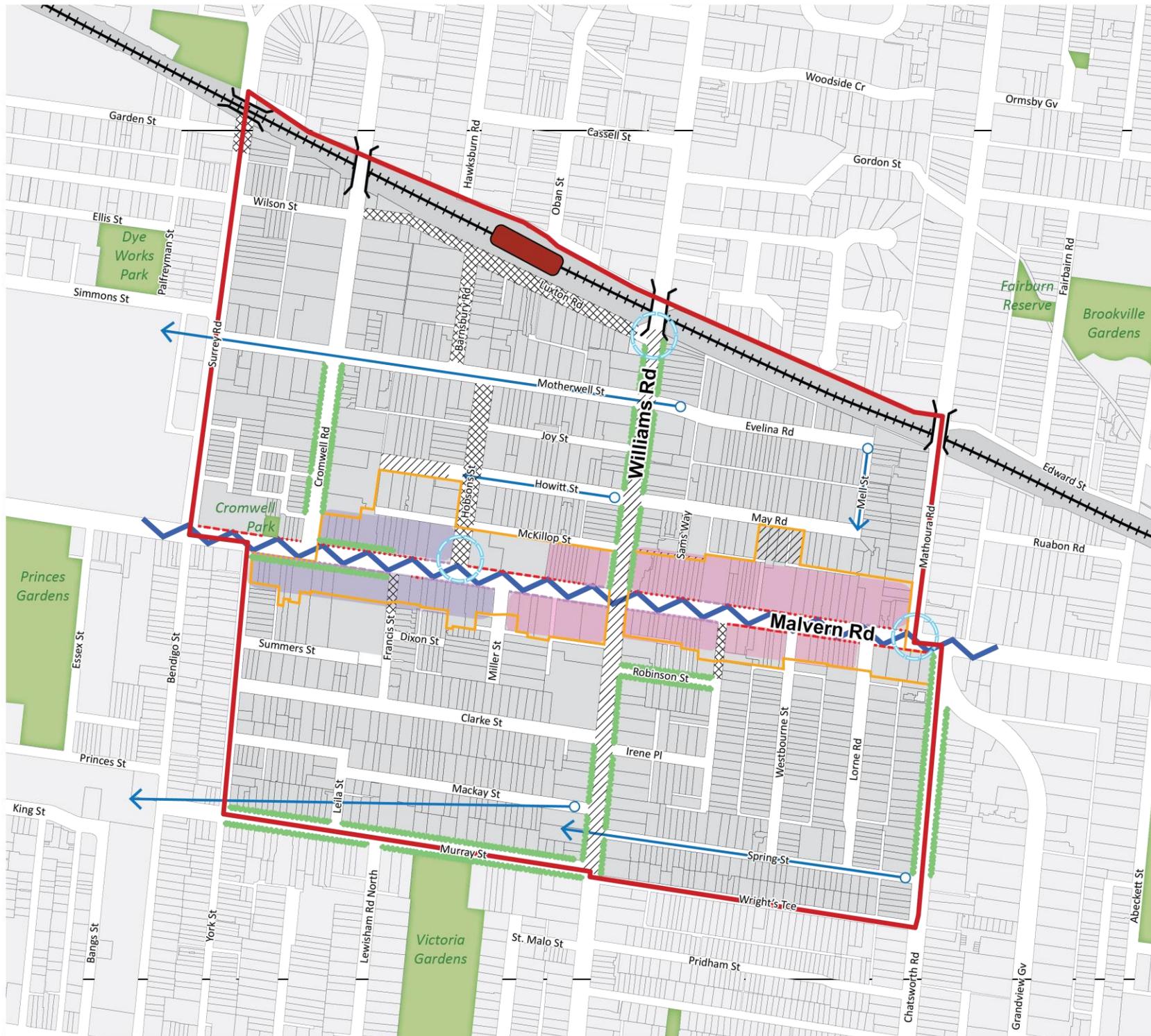
The intersection of Mathoura Road and Malvern Road presents an entryway to road users arriving at the centre from the east. Similarly, the intersection of Hobson Street and Malvern Road presents an entry way for pedestrians arriving from Hawksburn station to the main commercial core. Hence these two intersections have been identified as 'Entryway' in Figure 11.

Williams Road combines heavy vehicular traffic with a generally medium grain subdivision pattern. This creates a perception of car dominance, which contrasts with the pedestrian-friendly canopy trees located along the majority of the footpath.

Hobson Street and Barnsbury Road are important pedestrian links from Malvern Road to Hawksburn station. Similarly, Luxton Road provides the main access to the station. Despite their significance, these streets have a poor pedestrian environment presented by lack of activation and relatively poor streetscape conditions.

Several of the east-west streets have long views towards the taller development near Chapel Street to the west. This contributes to a lower feeling of intimacy compared to adjacent streets with shorter view lines, or more enclosed tree canopies.

FIGURE 11
SENSE OF PLACE



- LEGEND**
- STUDY AREA
 - RAIL LINE
 - TRAIN STATION
 - ACTIVITY AREA - Denotes areas perceived to have high activity. May not correlate with actual Activity Centre boundary.
 - INDUSTRIAL CHARACTER QUALITY
 - RETAIL AND COMMERCIAL CHARACTER QUALITY
 - ← SIGNIFICANT VIEWS - Views towards prominent buildings and landscape elements.
 - CANOPY TREES - Significant streetscape planting creating a perception of enclosure.
 - BUILDING MASSING - Perception of the pedestrian environment with regards to development grain. Colour denotes appearance from the opposite side of the street (red is more attractive, purple is less attractive). Dash size indicates building massing (smaller dashes are finer grain, larger dashes are coarser).
 - CAR DOMINANCE - Areas perceived to be car-centric and uninviting to pedestrians.
 - POOR PEDESTRIAN ENVIRONMENT - Areas perceived to be unattractive to pedestrians and lack activation.
 - ~ CONTESTED SPACE - shared by trams, cyclist, pedestrians and cars (traffic lane and on-street parking)
 - ENTRYWAY - Points that denote a significant change of character and/or channel pedestrian/vehicular traffic towards a point of interest.



For Stonnington City Council



Retail and Commercial Character Quality along east side of Malvern Road

Industrial Character Quality along west side of Malvern Road

6.0 Opportunities and Constraints

This section summarises the opportunities and constraints within the study area as they relate to the themes that will inform the framework of the Structure Plan. The themes include Land Use Activities, Built Form and Heritage, Public Realm and Open Space, Access and Movement and Sense of Place.

6.1 Constraints

6.1.1 Land Use Activities

The existing land use activities within the study area are limited to residential, mixed use, retail, commercial, light industrial and periphery sales.

The established residential areas of Hawksburn typically consist of a low-rise, fine grain, human scale, Victorian-era built form. As such, 60% is encumbered by heritage overlays (HO) and proposed Neighbourhood Character Overlays (NCO). Further, there are a considerable number of residential properties that are subject to Strata Titles. There are also a number of Church properties within the study area that are unlikely to be redeveloped in the foreseeable future.

With regards to the retail and commercial uses within the study area, the eastern character precinct and its fine grain built form constrains its land use role to mainly retail provision.

The existing larger format industrial buildings within the western precinct offer more opportunities to accommodate medium sized retailers, in particular food retailers.

As a whole, both the western and eastern character precincts are limited by their location and position in the existing retail hierarchy. As mentioned in the economic analysis, Hawksburn Village will only play a secondary role to that of Chapel Street and Toorak Village, meeting local household's weekly shopping needs.

The sensitive interfaces between land use activities can also be a constraint to any future development or change of use. A large proportion of the residential areas abutting the commercial and retail uses are affected by heritage overlays. Therefore, more consideration needs to be given to the nature of change and development typology proposed along the interface between these 'sensitive' lots and areas of potential redevelopment in adjoining commercial and retail areas.

6.1.2 Built Form and Heritage

Retention of neighbourhood character and residential amenity are particular constraints to the type and extent of any future development within the study area.

New built form must consider the sensitive interfaces to the existing residential character areas, respecting the existing scale, form and setbacks. They must also be cognisant of maintaining the fine grain built form at the street level.

With regards to housing growth predictions across the ITA, this analysis has found that increases of height across the Activity Centre to 4-5 storeys should provide approximately 500 dwellings, responding adequately to the predicted demand.

In relation to the commercial character precincts identified within the study area, the eastern precinct and its fine grain, active frontages and ornate facades, will be more constrained than the western precinct and its larger format industrial typology.

6.1.3 Access and Movement

Malvern Road has a limited cross-section and a multitude of competing demands, being nominated as a Bicycle Priority Route, Tram and Bus Priority Route, Pedestrian Priority Area and Traffic Route. Delivering the desired car movement functionality of Malvern Road (including car parking) may therefore limit opportunities for public realm improvements including footpath widening, on road cycle lanes and DDA compliant trams stops. The limited footpath widths existing may also inhibit the amount of additional bicycle parking infrastructure that can be introduced.

With regards to connectivity, pedestrians are highly constrained from crossing both Williams Road and Malvern Road, due to the lack of signalised crossing points. This is exacerbated by the high traffic volumes that both Williams Road and Malvern Road experience, making uncontrolled crossing movements difficult.

6.1.4 Public Realm and Open Space

The public realm along Malvern Road currently acts as a contested space, where both the required flow of traffic and on-street parking for traders constrains further public realm improvement opportunities. Public realm improvements along

Malvern Road, including street tree planting, are also constrained by the limited cross section and built form limitations.

The study area currently includes one open space reserve. The lack of vacant land within the study area inhibits and constrains the provision of additional open space.

6.1.5 Sense of Place

Through the analysis undertaken within this paper, a distinct transition between the retail focus to the east of Williams Road and the mixed use and industrial component to west of Williams has been identified. Each of which has a particular impact on Hawksburn's identity and sense of place.

From a character perspective, the built form to the east of Williams Road, which includes a fine grain subdivision with intricate and detailed facades, is a strong contributor to Hawksburn's sense of place and therefore less likely to be developed. The larger, light industrial lots to the west of Williams road have a less distinctive character and therefore more likely to become redevelopment opportunities.

6.2 Opportunities

6.2.1 Land Use Activities

There are several opportunities for land use intensification within the study area, particularly within the identified strategic development sites. The Woolworths supermarket, its car parking and abuttal to Council owned land give significant

opportunities for intensification and adaptation of site for a combination of retail, residential and additional open space.

As discussed in Section 5.3, the study area includes a variety of laneways that deliver a unique urban character. As an extension of the retail activities along Malvern Road, McKillop Street should be considered as an opportunity for a laneway precinct.

6.2.2 Built Form and Heritage

The variation in built form within the study area offers particular development opportunities within two distinct character precincts which lie east and west of Williams Road.

The urban design analysis has identified that there is an area with redevelopment potential within the western character precinct, predominantly along the southern side of Malvern Road. This opportunity exists due to the dominant presence of medium grain properties, which are generally occupied by light industrial and commercial uses, such as showrooms, offices and service industries in predominantly older and low scale buildings. Further, the new 5 storey mixed use development opposite creates a new character and marks a positive change in both the identity and image for this section of Malvern Road. This also contributes to increasing the local activity within the Activity Centre due to the additional resident population.

The eastern character precinct supports a fine urban grain which delivers a DNA of urban form that fosters diversity. As a juxtaposition to the new character and sense of place that will

evolve west of Williams Road, this precinct and its character should be retained and respected with built form interventions that maintain this existing diversity.

Notwithstanding this, the eastern character precinct does contain smaller redevelopment opportunities on larger parcels of land, being either in contiguous ownership or abnormally larger than prevailing and surrounding properties. More specifically, 537-541 Malvern Road presents itself as a redevelopment opportunity due to its larger lot size. Any redevelopment of this site, however, would be limited due to both economic conditions and urban design considerations pertaining to the eastern precinct. Investigations should be undertaken to explore opportunities to apply the most appropriate form of development or activity to the site.

Throughout the study area, three key strategic sites have been identified. These are:

- 559-565 Malvern Road and 32 May Road, which contain the Woolworths supermarket and some of the associated car parking.
- 145 Williams Road, which contains the BMW Dealership and Service Centre.
- 396-400 Malvern Road, which contains the Prahran Police Station.

The Woolworths property has been identified because of its significant size when compared with other commercial

properties along Malvern Road, as well as its connection through to May Road. It's core anchor retail function, and it's abuttal to a number of Council owned properties (which accommodate further surface car parking to the rear of the supermarket) also present opportunities for redevelopment.

The BMW Centre and Police Station have been identified because of their significant size, prominent corner locations and rear service laneway access. Further, we understand that a new BMW showroom is to open shortly in the ground floor of the new mixed use development at the corner of Hobson Street and Malvern Road. Therefore, it has been assumed that some or all of BMW's current activities could transfer to the newer facility and that some or all of the existing property may be in surplus.

Another development opportunity that presents itself is around the Hawksburn Train Station. Though fully developed and at times constrained by heritage, it is believed that the built form surrounding the train station should be densified, truly reflecting the importance of the train station as a sustainable mode of transport. This would further support the small shopping precinct adjacent to the train station.

6.2.3 Access and Movement

Malvern Road is the main east-west movement corridor. It includes a tram route and forms the main activity spine with concentrated retail and business uses where pedestrians, shoppers and visitors congregate. This will become the focus

for transit oriented development opportunities as compared to the residential hinterland within the study area.

For Malvern Road to perform as the main activity spine, village and east-west corridor, consideration will need to be given to changing the road user hierarchy, placing more emphasis on pedestrians, cyclists and public transport users, rather than cars.

If consideration was given to the potential removal of a traffic lane each way mid-block (between signalised intersections), opportunities would arise for footpath widening and additional cycling infrastructure (including bike parking and on-road cycle lanes), all of which improve accessibility to the area.

This would also allow for the redesign of the existing tram stops within the study area to be DDA compliant. To reduce the loss of on-street car parking associated with the addition of DDA compliant tram stops, OMG suggests there is an opportunity to rationalise the stops through the study area, providing two mid-block tram stops instead of three (see Figure 11). This, along with the rationalisation of bus stops with tram stops, would minimise the impact on parking along Malvern Road. However, as suggested by OMG, this opportunity is based on an assumption that a new tram super stop platform will be investigated to the west of Surrey Road.

With regards to north-south movements within the study area, there are opportunities to strengthen the link between Malvern Road and the train station via Hobsons Street and

Barnsbury Road. This link can be strengthened through the provision of additional signage along with a signalised pedestrian crossing where Hobson Street intersects with Malvern Road. This would support connecting rail users in a north-south direction through the study area.

6.2.4 Public Realm and Open Space

Public open space serves an important social function and adds to the character and sense of place of a study area. As the study area is currently lacking in public open space, two sites have been identified as potential areas for new public open spaces. These are the Woolworth's car park, and the area at the rear of the Police Station between Summers Street and Francis Street. Both sites hold significant opportunities to deliver both a higher density built form outcome that aligns with the existing character, whilst also providing opportunity for more pocket parks and open space to serve needs of the community. These locations will contribute to increased accessibility to public open space for residents outside of the 400m walking distance.

Malvern Road presents active ground floor frontages predominantly in the eastern precinct, which contribute to the appeal of the public realm for pedestrians, and thus the success of the activity centre. The current condition of Malvern Road presents an opportunity to improve the amenity of its streetscape environment. This could include additional bike parking predominantly focussed on food outlets. Bike rails could be provided in the vicinity of the Woolworths supermarket, potentially within the May Road car

parking area, and also at Hawksburn Station where currently street furniture is utilised for bicycle parking.

Given the redevelopment potential identified along Malvern Road to the west of Williams Road, as mentioned in Section 6.2.3, there is an opportunity to better link the train station to Malvern Road. Hobson Street has a broad road reserve, which could easily accommodate wider footpaths and increased amenity, plus signage to both indicate and encourage a stronger pedestrian connection between the station and Malvern Road.

Barnsbury Road is a relatively narrow road reserve. However, broader and high quality footpath could be provided along its eastern side to accommodate and encourage increased pedestrian movements.

Barnsbury Road and Luxton Road have potential to improve the streetscape environment by providing more landscaping, lighting and improved footpath material and signage opportunities to increase safety and legibility. Opportunities also exist to improve the activation and streetscape infrastructure along Hobson St, utilising the higher amounts of foot traffic as an economic driver for an increase in retail based activities.

Other residential streets like Clarke Street, Spring Street, Wrights Terrace and Errol Street present landscape improvement opportunities.

6.2.5 Sense of Place

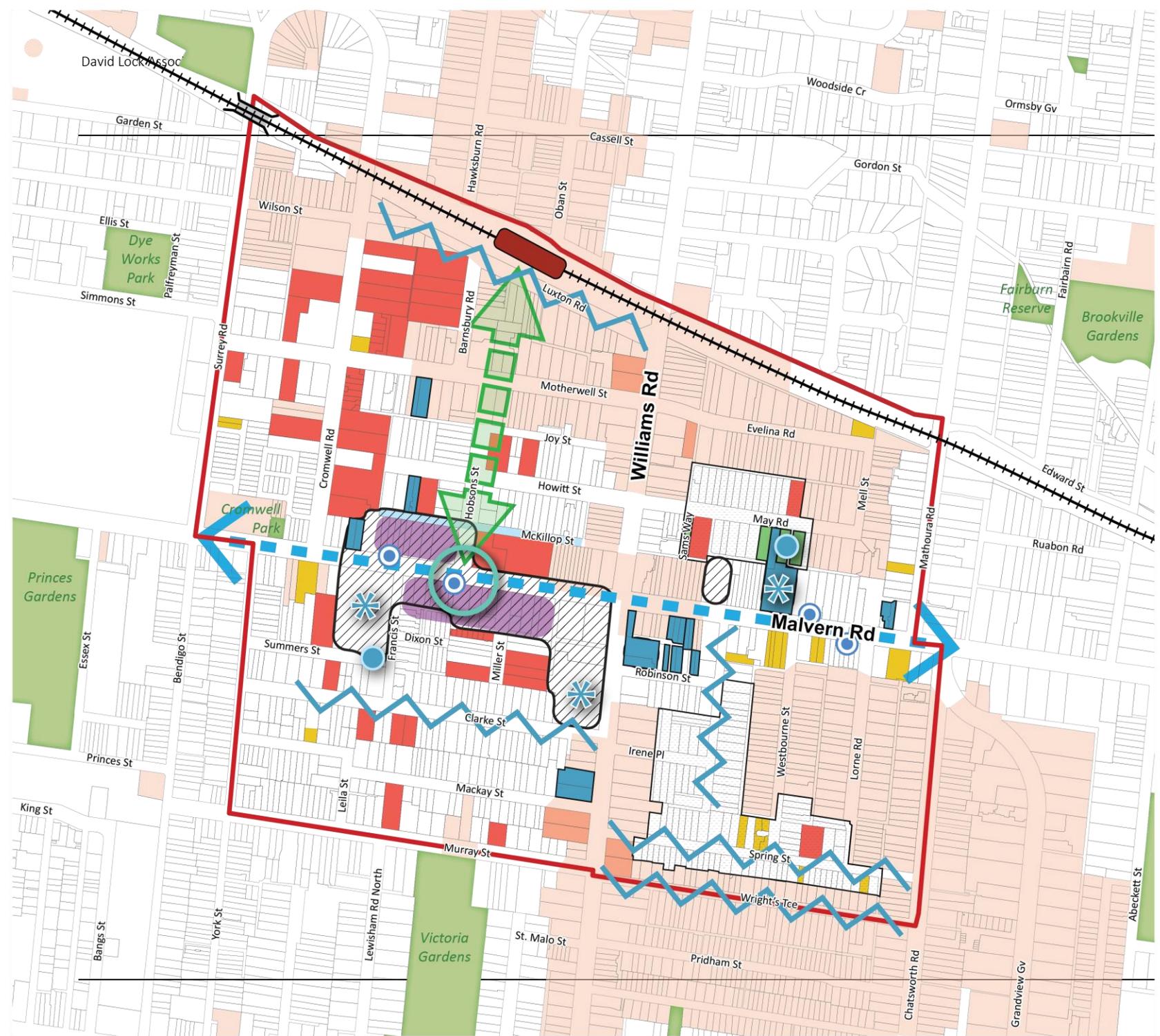
As mentioned in Section 6.2.2, the study area has two distinct character precincts which allow for different levels of development intervention. To retain the prevailing character and sense of place achieved in the eastern precinct, development opportunities must be of a smaller scale, adapting existing spaces and retaining the fine grain nature and diversity in built form.

The western precinct, and in particular, the light industrial uses along Malvern Road, hold opportunities for larger development interventions and the ability to create a new sense of place.

Parts of the study area are distinguished by their leafy green public realm which contributes to sense of place. It is therefore of particular importance that these features are protected and enhanced moving forward.

As mentioned in section 6.2.4, there are opportunities for landscaping improvement across the precinct, but in particular, along Barnsbury Road, Luxton Road, Clarke Street, Spring Street, Wrights Terrace and Errol Street.

**FIGURE 11
OPPORTUNITIES
AND CONSTRAINTS**



- LEGEND**
- STUDY AREA
 - RAIL LINE
 - TRAIN STATION
 - PUBLIC OPEN SPACE
- CONSTRAINTS**
- STRATA TITLES
 - RECENT DEVELOPMENT LAST 5-10 YEARS
 - HERITAGE OVERLAY
 - HERITAGE OVERLAY AND STRATA TITLE
 - PROPOSED NEIGHBOURHOOD CHARACTER OVERLAY
- OPPORTUNITIES**
- STRATEGIC SITES
 - CONTIGUOUS OWNERSHIP
 - COUNCIL OWNED LAND
 - RETAIN EXISTING DISTINCTIVE INDUSTRIAL CHARACTER
 - REDEVELOPMENT OPPORTUNITIES (INCLUDING DIVERSITY OF USES)
 - POTENTIAL LANEWAY PRECINCT
 - STREETSCAPE ENHANCEMENT
 - POTENTIAL STREETSCAPE 'GREENING'
 - POTENTIAL PUBLIC OPEN SPACE
 - IMPROVE CONNECTION
 - SIGNALISED PEDESTRIAN CROSSING
 - POTENTIAL DDA COMPLIANT TRAM STOP AND COMBINED BUS STOP



7.0 Definition of Activity Centre Boundary

A study area has been defined by Council for the purpose of undertaking a structure planning process.

One of the initial aims of the Structure Plan is to define an activity centre boundary for the Neighbourhood Activity Centre of Hawksburn Village.

Practise Note 58 – Structure Planning for Activity Centres (April 2010) provides guidance and criteria to define the extent and boundary of an activity centre.

7.1 Desktop analysis

To inform the preparation of this document, an ‘initial’ activity centre boundary was determined through a desktop analysis process applying the Practice Note 58 activity centre boundary criteria. This analysis process started with the existing extent of the commercial and mixed use zoned land and considered whether or not abutting residential zoned land would be suitable or is currently used for commercial or higher density residential purposes. Then any abutting residential land which is already covered by a Heritage Overlay was excluded within the initial activity centre boundary as such properties have limited development potential due to the heritage constraint. This decision making process is illustrated in Figure 14.

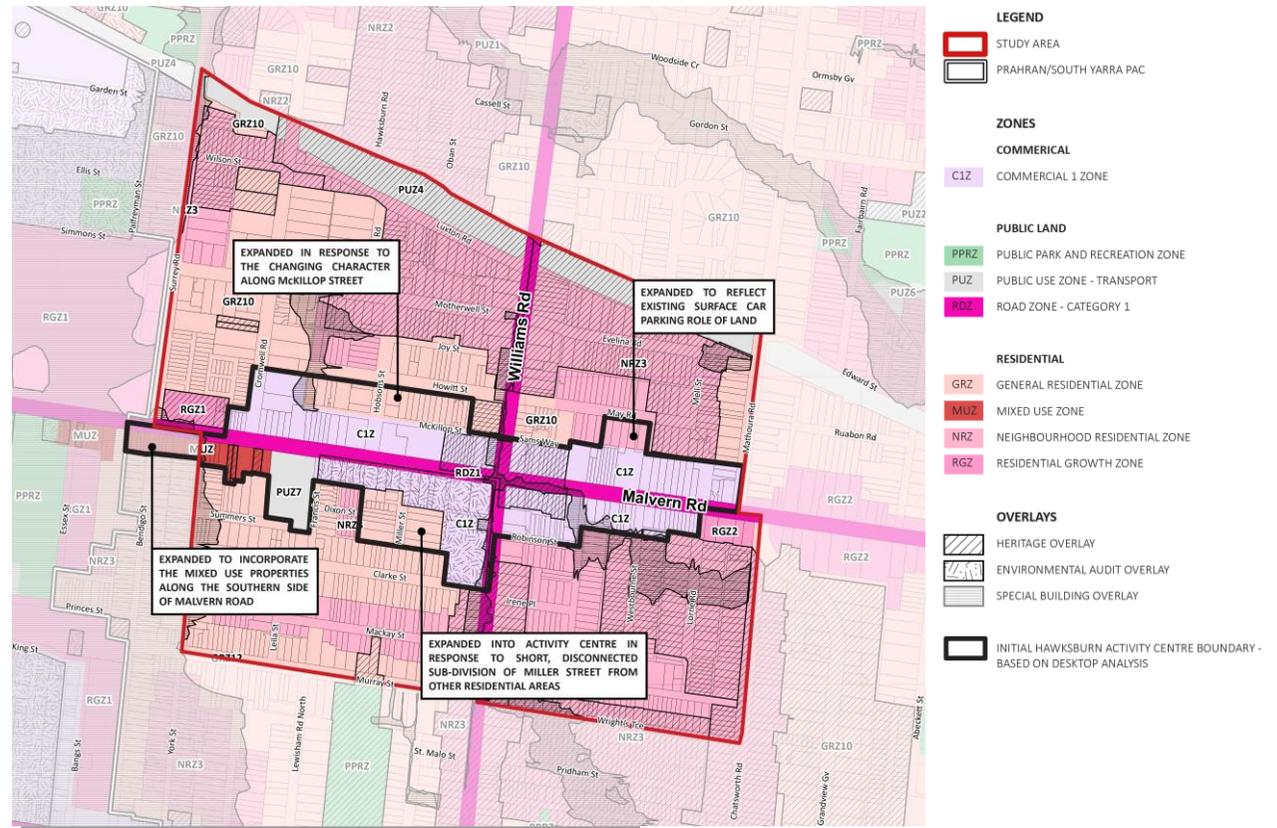


Figure 8 Initial Activity Centre Boundary

7.2 On-site refinement

Once the ‘initial’ activity centre boundary was defined, a detailed on-site inspection was undertaken to view and test this ‘initial’ activity centre boundary, and to refine it where necessary. Based on the outcome of this targeted site inspection a number of amendments were made to the initial boundary, to create the proposed activity centre boundary for the reasons stated on the plan. The rationale behind these decisions is elaborated on in Figure 16.

Activity centre boundary criteria

a) Consider the following issues in determining the potential location of an activity centre boundary:

- the location of existing commercial areas and land uses
- the location of existing government and institutional areas and land uses
- the location of existing areas of public open space
- commercial and residential needs
- environmental and flooding constraints
- heritage constraints
- availability of strategic redevelopment sites, both existing and potential
- the location of residential areas, including whether they provide significant redevelopment opportunities or constraints for the centre
- consideration of physical barriers and opportunities for their improvement
- proximity to public transport, especially fixed rail (train or tram)
- the location of existing and potential transport infrastructure including fixed rail, buses, bicycle paths, car parking areas and modal interchanges
- walkability – opportunities to provide for and improve walkability within 400 to 800 metres from the core of the centre (depending on topography and connectivity)
- consistency with State policy
- consistency with local policy and Municipal Strategic Statement (MSS)
- impacts of the boundary on other activity centre boundaries.

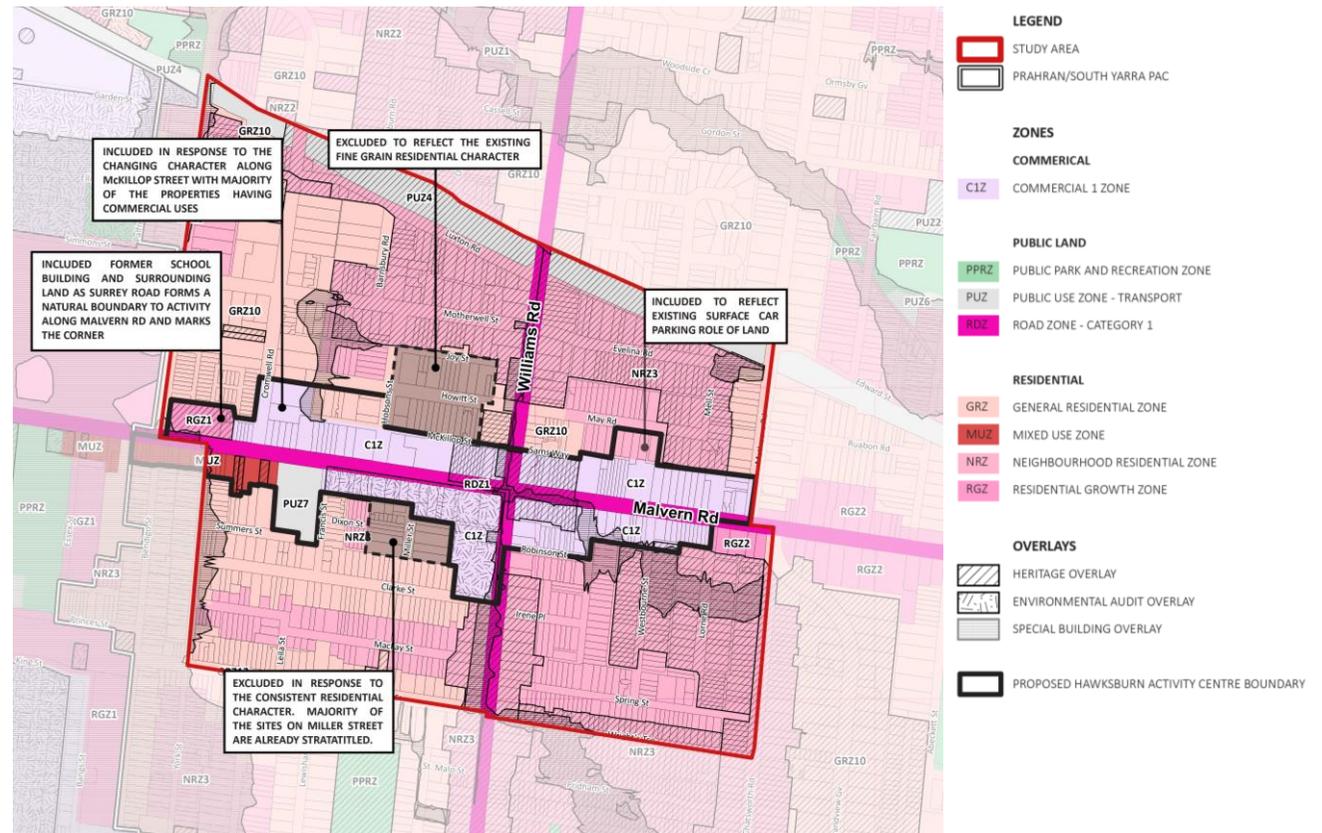


Figure 15 Excerpt from Practise Note 58 – Structure Planning for Activity Centres

Figure 16 Proposed Activity Centre Boundary

8.0 Preliminary Vision

The Stonnington Municipal Strategy Statement (MSS) promotes a role for a larger neighbourhood activity centres to cater for everyday needs and wider speciality retail, office and service markets. It encourages Hawksburn Village to provide predominantly retail uses, with a food shopping character, plus a wider mix of office and service uses at the western end.

Based on the background analysis, and feedback received during the community and stakeholder consultation, this Discussion Paper proposes the following vision and supporting objectives to guide the preparation of the Structure Plan.

8.1 Preliminary Vision

“Hawksburn Village will continue to thrive as a welcoming, vibrant and diverse centre, and be known for its distinctive charm and village feel, and safe, green, and walkable street network.”

9.0 Next Steps

This Discussion Paper presents an analysis of the economic, traffic and transport and urban design conditions of Hawksburn, informing the identification of constraints and opportunities that will need to be addressed within the Hawksburn Village Structure Plan. Its contents also inform the next phase of the structure planning process, which involves the drafting of built form and development options. It also helps to form the framework of the structure plan and will inform the community consultation to be held, that will ask the community questions directly relating to the opportunities and constraints as identified within this paper.

The next steps from here are as follows:

- Community consultation on the opportunities and constraints as identified within this paper;
- The drafting of different options that map different extents for built form, public realm and development opportunities;
- In consultation with the Working Group and Steering Committee, the identification of preferred options to inform the preparation of the Draft Structure Plan; and
- Preparation of the Draft and Final Structure Plan for implementation into the Stonnington Planning Scheme.