

STRUCTURE PLAN

November 2015



— City of —
STONNINGTON



hansen



2015 GLENFERRIE ROAD HIGH STREET ACTIVITY CENTRE STRUCTURE PLAN

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Stonnington City Council Officers

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HIGH STREET



ARMADALE STATION



UNION STREET RESERVE



TRAM DEPOT



TOWN HALL

MALVERN CRICKET GROUND



working



dining

living



CABRINI HOSPITAL

shopping



MALVERN STATION

LINES

PAKENHAM
CRANBOURNE
FRANKSTON

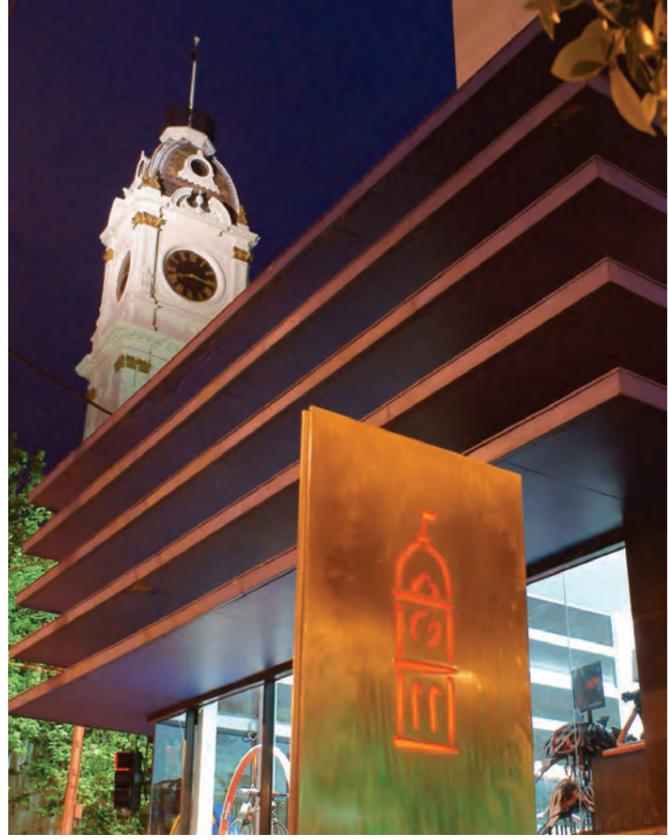


Figure 1 The study area: strategic illustration of key components

1 Introduction

What is a Structure Plan?

A Structure Plan is a strategic planning tool which sets out a shared long-term vision for the future growth and change of a defined area. The Structure Plan aims to give effect to the policies and objectives set out in the State Planning Policy Framework to provide effectively for community needs. The Structure Plan will guide the major change to land use, built form and public spaces that together aim to achieve economic, social and environmental objectives for the place.

A coordinated plan allows a wide range of stakeholders, service authorities, State Government departments and internal Stonnington Council departments to all 'read from the same page' to produce a coherent outcome and provide certainty for both residents and the Council.

A Structure Plan also provides the framework for statutory planning controls which are implemented through the Stonnington Planning Scheme. Structure Plans are informed by extensive public consultation and discussion. While everyone may not agree on every aspect of the plan, it should reflect broad community consensus.

How will the Structure Plan be used?

The aim of the Structure Plan for the Glenferrie Road / High Street Activity Centre is to guide future land use and development in a coordinated manner and provide greater certainty for all stakeholders to deliver a preferred future. A vision for the centre for a period of 10 - 25 years is established. Key policy directions, and important physical outcomes as they relate to the Glenferrie Road / High Street Activity Centre's built form and activity, landscape and environment and access and movement are identified. The Structure Plan will be used by:

Stonnington City Council:

- As a basis for introducing new planning policies, zones and overlay controls into its planning scheme (as appropriate);
- In assessing planning permit applications;
- In assessing requests to rezone land;
- In guiding non-statutory initiatives, arrangements or partnerships to assist in realising potential future opportunities within the centre;
- In preparing capital works budgets to implement public works; and
- In delivering community services.

The Community:

- To understand how the area, and specific precincts within the activity centre, are likely to change in the future;
- To assist community groups in prioritising future work and in seeking funding for projects; and
- To provide a framework for community groups to assist in making long term plans.

Existing Business Owners:

- To create greater certainty and appreciation regarding the future direction of the centre.

Developers:

- To understand the development opportunities that exist and the matters that will be taken into account in assessing development proposals.

Other government agencies:

- In coordinating infrastructure improvements with work undertaken by Stonnington City Council and other agencies including public transport, roads and water.

The Structure Plan must be regularly reviewed to ensure its directions and ambitions continue to be relevant to the centre as it changes over time.

Glenferrie Road/ High Street Activity Centre Structure Plan

This Structure Plan builds upon the earlier analysis and findings documented within the following documents:

- Glenferrie Road/ High Street Activity Centre Structure Plan Background Report: June 2014.
- Glenferrie Road/ High Street Activity Centre Structure Plan issues and opportunities paper: August, 2014.
- Economic Technical Report, Essential Economics, 2014.

This document does not seek to replicate the detailed content of above documents; rather they form necessary background information which is relevant to the detail contained within this Structure Plan.

The combined retail areas of Glenferrie Road and High Street form an important, and historical, retail precinct that is popular within the local catchment primarily for its convenience, as well as containing a range of retailing activity which has a wider regional catchment. As a combined centre the Glenferrie Road / High Street Activity Centre is an important hub in Melbourne's inner south east. It provides a range of retail and hospitality services and is well located within the Melbourne metropolitan context to capture future development, both residential and non-residential.

Stonnington City Council have engaged Hansen Partnership, a planning and design consultancy, to work with key stakeholders in preparing the Structure Plan for the Glenferrie Road / High Street Activity Centre. Hansen Partnership have been assisted with a supporting sub-consultant's team of O'Brien Traffic (transportation) and Essential Economics (economic analysis).

The Structure Plan constitutes a strategic document that sets out a long term planning and design vision for the evolution of the precinct and is intended to set a 10-25 year strategic framework to manage and guide the future development the centre.

Project Stages

This project is being undertaken in a four stage process. The first stage of the project involved the collation of range of background materials, whilst the second stage involved the preparation of an issues and opportunities paper including consultation with the local community. The current third stage involves the preparation the Structure Plan itself including further community consultation and, following on from this the final stage will involve the implementation of the Structure Plan.

2 Vision Statement

The Vision Statement which underpins the Glenferrie Road/ High Street Activity Centre Structure Plan is as follows:

In 2040 Glenferrie Road and High Street Activity Centre will be:

- *A thriving shopping centre, serving the day to day needs of the local community, coupled with a range of specialist retailers serving a wider regional area.*
- *A vibrant, inviting, attractive and clean centre with well-designed, 'green' streetscapes, pedestrian connections and community gathering nodes.*
- *A centre where new development respects and enhances the form and scale of heritage buildings, which retain prominence.*
- *A centre for localised employment and housing within the commercial spines whilst protecting the valued residential hinterland.*

Objectives

The primary objectives for the Structure Plan are:

- To protect and enhance the existing heritage and built form fabric.
- To provide clear urban and built form guidance for new infill development and additions and alterations of existing buildings.
- To strengthen the commercial and retail role of the centres.
- To improve pedestrian and cyclist mobility and infrastructure throughout the centre and minimise pedestrian and vehicular conflict zones.
- To improve the appearance of the public spaces within the centre, including primary streetscapes and secondary areas such as public car parks and laneways.
- Provide additional vegetated corridors and spaces in strategic locations to develop the urban forest and enhance the public realm.
- To provide for pause points at and around important junctions.
- To improve access and connection of Malvern Central with Glenferrie Road.
- To consolidate medical and health related facilities along Wattletree Road.
- To consolidate and strengthen the community node around Malvern Library.
- To establish clearly defined redevelopment sites into the centre.
- To provide healthy and diverse vegetated streets and public spaces.
- To provide clear guidance on the inclusion of vegetation into new infill development and additions and alterations of existing buildings



The study area comprises a diverse range of urban environments and experiences from hostile Dandenong Road, to the intimate leafy residential side streets off High Street

3 Study Area

The Glenferrie Road / High Street Activity Centre, as identified by Plan Melbourne (2014), is located within the City of Stonnington and is located approximately 7kms east of the Melbourne CBD, in the south of the Stonnington municipality.

The Glenferrie Road / High Street Study Area comprises an area of approximately 1.1km² and is a generally triangular area of land bound by the rail corridor to the south-west and the buildings on the northern side of High Street to the north. In 2013, the Centre was estimated to provide approximately 70,000m² of retail floor space, making it larger than the centres of St Kilda and Camberwell, yet smaller than Chapel Street and Chadstone Shopping Centre.

Defined by its two intersecting main streets, Glenferrie Road and High Street Activity Centre is an important hub in Melbourne's inner south east. It provides a range of retail and hospitality services to the immediate suburbs as well as speciality retailing with a more regional focus. Within the Melbourne metropolitan context and its inner suburban location the Glenferrie Road / High Street Activity Centre is poised to be the focus of future development, both residential and non-residential. Designated peripheral areas are located adjacent to the primary Activity Centre Boundary which will also evolve over time (refer Figure 3).

The designated Activity Centre Boundary (refer Figure 3) has been established to be reflective of the existing land use patterns and urban form, in addition to the potential to facilitate the future evolution of more intensive land use and built form elements.

The Structure Plan focuses on the commercial zones of the centre, broadly located along the main strip shopping centre frontages of Glenferrie Road and High Street. In addition, the Activity Centre Boundary includes Council buildings, car parks, community uses, child care centres, schools etc. and also the Cabrini Hospital and the 'commercial and medical related spine' located along Wattletree Road.

The Structure Plan is primarily characterised by the following attributes:

- The main street suburban strip shopping centre frontages located along Glenferrie Road and High Street. Both commercial spines Glenferrie Road and High Street have different functions and roles based on established commercial activity and road alignment (north/ south and east/ west).
- A community/ civic precinct centred around Malvern Town Hall located on the corner of Glenferrie Road/ High Streets and extending to the east to incorporate a number of schools and community based uses.
- A public transport node around Armadale Station to the west of the study area, which centres on the Kingsway Arcade heritage precinct.
- A public transport infrastructure node focused on the Malvern Tram Depot located behind Glenferrie Road to the west.
- A car parking precinct located behind Glenferrie Road to the east (extending from Winter Street to Wattletree Road), including a decked parking structure.
- A main commercial node focused on Malvern Central, located to the corner of Glenferrie Road and Wattletree Road.
- A secondary 'commercial and medical related spine' located along Wattletree Road extending from the railway line to the west to Cabrini Hospital to the east.
- A public transport node focused around Malvern Station to the south of the study area, which incorporates a small localised commercial precinct centred along Station Street and the intersection with Claremont Avenue.
- A service industrial/ commercial precinct focused on land between Dandenong Road and the railway alignment.
- The railway cutting which bisects the local neighbourhoods extending from Armadale to Malvern Station.

The Activity Centre Boundary has been determined by the extent of the commercial area, and explicitly excluded surrounding hinterland residential areas which have been recently designated for lower order change with the implementation of the New Residential Zones. This results in a relatively confined boundary which follows High Street, Glenferrie Road, the Town Hall, Armadale Station, Malvern Station and properties between Dandenong Road and the rail alignment. Peripheral areas located adjacent to the Activity Centre boundary includes significant facilities such as: the Civic Precinct, Cabrini Hospital / Wattletree Road, and Coldblo Tram Depot.

Based on the above-mentioned characteristics and the overall size of the Glenferrie Road and High Street Activity Centre, the Structure Plan is further broken down into framework plan areas, which will allow more specific detail for each of the precincts to be provided within the context of the Structure Plan. Individual framework plan locations are illustrated at Figures 9-16 in the locations shown in Figure 2 including:

1. Civic Centre
2. Armadale Station
3. Malvern Station
4. Malvern Central
5. Coldblo Tram Depot
6. Drysdale Street Carpark

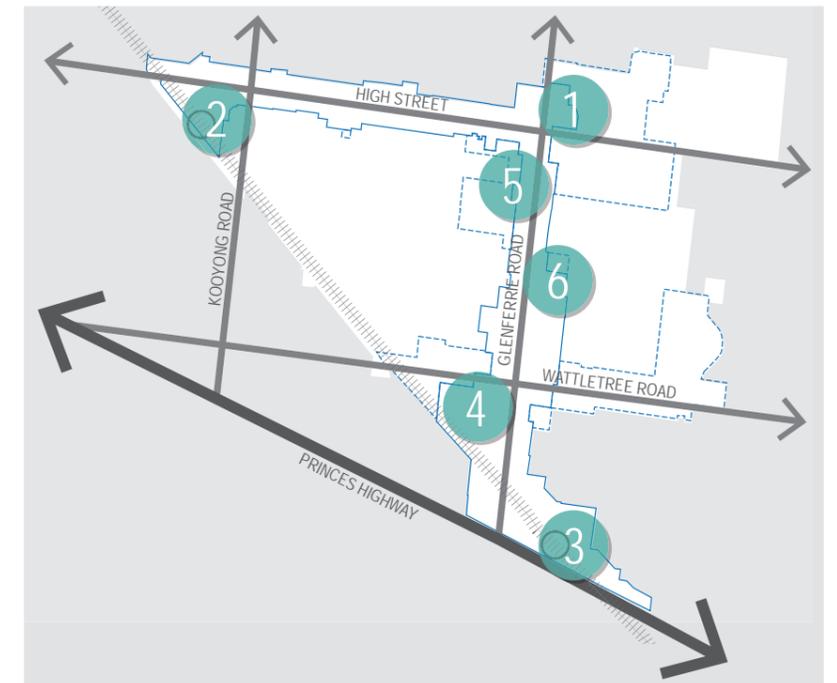


Figure 2 Precinct Framework Plan Locations



- legend
- study area boundary
 - activity centre boundary
 - peripheral boundary (areas of interest)
 - main road
 - railway line (pakenham, cranbourne, frankston lines)
 - railway station

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4 Background Analysis

In addition to the brief background analysis provided below, further detailed information is provided within the June 2014 Background Report.

History

- The Structure Plan area has a rich history; having originally been occupied by the Woi Worrung (Wurundjeri) and Boon Wurrong (Bunurong) people, forming part of the larger language group of the Kulin Nation, with a geographic range extending across the majority of metropolitan Melbourne down to Gippsland in the east.
- Post-settlement, the land was initially used for grazing and a number of stock routes which connected to Dandenong, Gippsland and Western Port Bay. Further subdivision and development of the land, in the form of a hotel, dwellings and shops commenced in the 1850s.
- The construction of the Oakleigh Rail Line in 1879 led to the development of Toorak, Hawksburn, Armadale and Malvern Stations. As is seen elsewhere in 19th century Melbourne, the suburban rail stations established the nodes from which growth radiated, and the clusters of commercial development at Malvern and Armadale Station particularly exemplify this.
- Other historical events of note within the Structure Plan area include:
 - The establishment of the Civic Precinct of Malvern Council in 1886.
 - The construction of the Coldblo Tram Depot 1910, leading to the creation of an L shaped tram route along this portion Glenferrie Road and High Street, stimulating the development of a combined Glenferrie Road and High Street commercial spine.
 - The development of the majority of the residential hinterland by the 1930s; resulting in a primarily Victorian through Edwardian to Interwar residential building stock characterising these residential areas.
 - The redefining of the role of the High Street commercial spine to specialise in high end retail, including wedding attire, serving a wider, regional, catchment in the 1980s.

The People

The demographic and socio-economic profile of residents can be summarised as follows:

- Income levels: are significantly higher than the Greater Melbourne average, with median individual incomes +50% above and median household incomes +35% above the Greater Melbourne median.
- Age Profile: is similar to the Greater Melbourne profile, with a median age of 37 years compared to a 36 years median for Melbourne. However, there is a higher proportion 'young professional' residents compared to Melbourne. Approximately 27% of residents are aged between 20 and 34 years compared to the Greater Melbourne average 23%.

- Family Composition and Household Size: a higher proportion of families are couples without children, compared to Greater Melbourne (42% of 35%). There is a lower proportion of one parent families (11% of 15%). As such, the average household size of 2.3 persons per household is significantly lower than the average of 2.6 persons per household for Greater Melbourne.
- Cultural background: there is a relatively low degree of ethnic diversity compared to Greater Melbourne. 70% of residents were born in Australia, with approximately 80% speaking English only at home. These percentages compare to approximately 67% and 70% for Greater Melbourne respectively.
- Dwelling Type: Compared to Melbourne, the wider study area is characterised by a high number of apartments (39% of housing stock cf 15% of housing stock) and semi-detached dwellings and terraces (18% of housing stock of 12%).
- Tenure: the share of dwellings rented (37%) is significantly higher compared to Greater Melbourne (28%), while the share of homes that are owned with a mortgage is significantly lower (26%) compared to Greater Melbourne (38%).
- Housing Costs: housing costs in terms of both average rents (\$350/week) and mortgage repayments (\$2,510/month) are significantly higher than the Greater Melbourne averages (\$310/week) and (\$1,840/ month).
- Internet, Car Ownership: levels of car ownership are slightly lower compared to Greater Melbourne (89% of 91%), while a higher share of households in the MTA are connected to the internet (87% cf 82%).
- Employment: unemployment (4.3%) is lower than the Greater Melbourne average (5.5%), with a high labour force participation rate (69%) relative to Greater Melbourne (66%).
- Occupation: approximately 90% of the labour force are employed in white-collar occupations (Managers & Professionals, and Clerical & Sales workers), compared to Greater Melbourne (72%).

Population Growth

Analysis of ABS Census data shows recent changes in population and housing in the Glenferrie Road-High Street Activity Centre has occurred over the period 2006 to 2011. In 2011 the Centre contained an estimated 165 dwellings accommodating approximately 270 residents, representing an increase of +55 dwellings (+50%) and +85 residents (+46%) over the census period. Overall, the average household size in the Centre is 1.6 persons, which is in line with household sizes found in Activity Centres across Melbourne. In comparison, the broader study area has an average household size of 2.0 persons per household which is indicative of the larger detached dwellings and family homes in the centre's residential areas. Given the proximity of Glenferrie Road-High to central Melbourne and its desirable neighbourhood location, the centre can be expected to experience increasing pressure for additional residential development.

Metropolitan Context

A Structure Plan seeks to influence land use, transport networks and development and this is guided primarily by a State Planning Policy Framework and a Local Planning Policy Framework. The Local Planning Policy Framework must support the State Planning Policy framework. The Glenferrie Road, High Street Activity Centre Structure Plan Background Report: June 2014 provides a detailed review of applicable policy of the Stonnington Planning Scheme.

The Strategic Framework Plan at Clause 21.03 of the Stonnington Planning Scheme nominates the Glenferrie Road and High Street centre as a Major Activity Centre. Within the Stonnington Activity Centre Hierarchy, a Major Activity Centres is situated below the Principal Activity Centres of Chapel Street and Chadstone.

The Stonnington Planning Scheme provides policies, objectives and strategies that apply to development within the Structure Plan Area, however these are not specific to the Glenferrie Road/ High Street Activity Centre. This Structure Plan will form the detailed policy and strategy basis relating to the Glenferrie Road/ High Street Activity Centre.

In addition to the above, the current State Government's Metropolitan Planning Strategy *Plan Melbourne* was released in May 2014, which sets a new planning vision for the future development of Metropolitan Melbourne. Whilst Melbourne 2030 previously classified the Glenferrie Road/ High Street Activity Centre as a Major Activity Centre (MAC), it is noted that in accordance with Plan Melbourne, it is now referred to as simply an 'Activity Centre' which is equally applied to both Principal and Major Activity Centres.



Figure 4 Metropolitan Context

5 Community Views

Meaningful engagement with the community has been a key focus of this project. A series of events have been held to ensure that the community have had the opportunity to identify issues that currently affect the centre and to actively participate in the generation of ideas and concepts to guide the future direction for the centre.

Community consultation was undertaken in two stages, with Stage 1 occurring at the commencement of the Structure Plan process in March 2014 and Stage 2 in August to September 2014. The following is a summary of the main consultation events undertaken and the feedback received from the community.

Vision

The community raised the following comments in relation to the nominated Vision Statement for the Glenferrie Road / High Street Structure Plan:

- It is deemed beneficial to have a mixture of regional and local functions. There was a strong view that it would be beneficial for the centre to have a greater balance of uses to achieve its vibrancy and long term resilience by finding its niche and own unique offering.
- There was support for converting more areas to green spaces, retaining street trees and incorporating environmentally sustainable initiatives such as solar panels and green roofs. Amenity enhancements could include activating public spaces (car parks), improved lighting, public toilets, way-finding, landscaping and management of litter and graffiti.
- Heritage and its maintenance is deemed to be a very important element of the activity centre. There was a desire for a reasonable approach to be taken to development and protection that balances the amenity of heritage with functional needs.
- Support for mixture of residential and retail / commercial use while balancing the impact on existing residential amenity and not displacing commercial uses. The scale of development should be identified and strictly managed.

Significant sites

- A number of significant sites have been identified in the study area, where a summary of the main comments relating to the opportunities at these sites include:
- Creation of a 'strategic redevelopment site' at the Dandenong Road entry to the centre which protects existing heritage buildings and provides setbacks to allow for greenery on Glenferrie Road.
- An improved interface way-finding, amenity and vibrancy of Malvern Central at all access points, and improved links with the surrounding street network.
- A potential increase in the size of Malvern Central, including the provision of additional car parking and introduction of entertainment uses.
- The creation of a green corridor between the railway line and Malvern Central.
- The redevelopment of the Malvern Train Station which protects existing heritage elements, and increases accessibility through the station.

- Creating a more vibrant and activated Armadale Train Station through a higher quality public realm, the introduction of shared zones and the possible building over the railway corridor.
- Support for the presence of a health precinct along Wattletree Road associated with Cabrini hospital, provided it does not adversely impact on the adjacent residential and heritage areas. Car parking was identified as a key issue in this precinct.
- Redevelopment of the Tram Depot precinct which is sensitive to the existing heritage buildings.
- The activation of the Malvern Town Hall / civic precinct more as a civic space with increased way-finding pedestrian access to and through the site and improved landscaping.
- Potential reinterpretation of Malvern Square with increased landscaping and its use for markets and other activities, making the site more level so that it can be used as more of a congregation space, and increasing public seating.
- Retention and possible expansion of car parking areas.

Pedestrian, cycle, public transport friendly environment

- Strategies for creating a pedestrian, cycling and public transport friendly environment were sought from residents and traders, where the following suggestions were made: High Street and Glenferrie road act as community hubs where pedestrian access should be prioritised and the street spaces should service as community spaces.
- The potential reducing of speed limits, limiting turning options and creating one-way areas in and around side streets.
- Streetscape improvements (e.g. widening footpaths, kerb 'outstands' with shelters and seating at side street junctions) to enhance pedestrian amenity and connections.
- A better link the Kooyong Road bus route and increase in public transport services in the centre.
- Improved bicycle routes through the centre, including safe alternatives off Glenferrie Road and High Street, as well as improved way-finding and bicycle parking facilities.

Open / public space

- There was a view that there should be an increase in the amount of open space and green spaces within the centre.
- A general view was expressed for the need for general streetscape improvements.
- The vacant King David School site * was noted as an opportunity for creating additional public open space (* site noted to be located outside of the study area).

Built form

- The encouragement of shop top development (residential and office), and development of infill sites with 'delicate' multi-storey buildings including the protection of heritage buildings and amenity from the street face (ie. through the use of set-backs, no overlooking or overshadowing).
- Shop top development was generally supported at appropriate sites. It was recognised that the greatest opportunity for development in the activity centre is infill at individual sites – which should occur at a scale that reflects surrounding buildings.
- Better utilising second storeys of buildings and shop tops for residential / office spaces was suggested.

Land use

- A focus on the local catchment needs in terms of the retail sector.
- The consideration of alternative purposes for shops as a response to the changing retail sector.
- The need for development and land use to build a sense of community through street life and activity.
- Support to build night time activity in the centre, providing it is appropriately located and sensitive to surrounding residences.
- The need for additional public toilets throughout the activity centre.

A more detailed summary of the Community Consultation Stages 1 and 2 can be found at Appendix A.

Social Impact Assessment

A Social Impact Assessment (SIA) on the Structure Plan has been prepared examining the social impacts of the physical, demographic, environmental, visual and economic change that could result from the implementation of the Structure Plan. The SIA also considers the implications that this change will have on the access of local resident to services and facilities located in or near the Structure Plan area. Potential negative and positive social impacts have been identified as well as actions that could be taken to mitigate the negative impacts. Key strategies have been incorporated into the Structure Plan. The SIA is included in full at Appendix C: Social Impact Assessment.

6 Structure Plan Framework

As overarching strategic documents Structure Plans contain a range of strategies and objectives, some of which are more important to the broad aspirations of the Structure Plan, whilst others are more relevant to specific precincts and locations. During the background investigations and preparation of the Structure Plan a series of 'key directions' were identified. These 'key directions' represent the 'grand gestures' of the plan and should influence thinking on the range of smaller matters which will be needed to implement the preferred future for the Glenferrie Road / High Street Activity Centre. Furthermore there are more specific strategies and objectives relating to individually designated precincts which are addressed separately within the relevant neighbourhood framework plans.

Nominated key directions focus on five key areas including:

- Contribute to the local economy
- Facilitate place making
- Allow for mobility and safe access
- Celebrate heritage and embrace new development
- Green streets for everyone

Keeping the focus on these five key focal areas is intended to ensure that the key messages of the project are not lost within the detail. These key directions should be considered in all decision making within the centre.



Establish a clearly defined entry into the centre from the south

Key Directions

Contribute to the local economy

- Acknowledge High Street as a regional commercial and retail destination.
- Strengthen local and retail role of Glenferrie Road.
- Provide guidance to possible future expansion of Malvern Central.
- Consolidate medical and health facilities along Wattletree Road.
- Encourage entrepreneurship, creative industry and special focal points (i.e. market, events etc.) in the Centre.

Facilitate place making

- Strengthen and Establish clearly defined entries into the Centre.
- Establish diverse experiences along different streets within the Centre.
- Establish active and attractive street presentations along High Street.
- Establish clear urban form and design guidance throughout key strategic development areas.
- Consolidate and strengthen community node around Malvern Library.
- Establish clear urban form and design guidance throughout key strategic development areas.

Allow for mobility and safe access

- Establish clear streets hierarchy and users' priorities.
- Encourage and facilitate more sustainable modes of travel within and around the Centre.
- Establish clearly defined pedestrian linkages and manage road crossing points.

Celebrate heritage and embrace new development

- Protect and enhance significant heritage fabric along Glenferrie Road and High Street.
- Protect and enhance the laneway network within the Centre.
- Encourage development to provide a secondary address to rear lanes behind Glenferrie Road.
- Encourage development to provide a secondary address to the rear at Commercial interfaces.

Green streets for everyone

- Strengthen public meeting space at and around important junctions.
- Establish equitable access to all existing open spaces.
- Establish potential event spaces at key sites (market, special event etc).
- Establish additional vegetation in the public realm to contribute to the urban forest.

Figure 5 Framework Plan



Great Streets:

1. Contribute to Local Economy

-  Acknowledge High Street as a regional commercial and retail destination
-  Strengthen local and retail role of Glenferrie Road
-  Provide guidance to possible future expansion of Malvern Central
-  Consolidate medical and health facilities along Wattletree Road
-  Encourage entrepreneurship, creative industry & special focal points in the Centre

2. Facilitate Place Making

-  Clearly define strategic redevelopment sites into the Centre
-  Establish diverse experiences along different streets within the Centre
-  Establish active and attractive street presentations along Glenferrie Road and High Street
-  Establish clear urban form and design guidance throughout key strategic development areas
-  Consolidate and strengthen community node around Malvern Library

3. Allow for Mobility & Safe Access

-  Establish clear streets hierarchy and users' priorities
-  Encourage and facilitate more sustainable modes of travel within and around the Centre
-  Establish clearly defined pedestrian linkages and manage road crossing points

4. Celebrate its Heritage & Embrace the Future

-  Protect and enhance significant heritage fabric along Glenferrie Road and High Street
-  Protect and enhance the laneway network within the Centre
-  Encourage development to provide a secondary address to rear lanes behind Glenferrie Road.
-  Encourage development to provide a secondary address to the rear at commercial interfaces.

5. Green & For Everyone

-  Consolidate and strengthen the community node around civic space
-  Strengthen public meeting space and vegetation at and around important junctions
-  Establish equitable access to all existing open spaces
-  Establish potential event spaces at key sites (market, special event, etc.)
-  Establish additional vegetation in the public realm to contribute to the urban forest.

Land Use

This Structure Plan does not seek to implement significant and wholesale changes to the current land use patterns within the Centre. Instead, it seeks to enhance and improve the existing distribution of land uses, attract supporting services and retain the competitive position of the Centre.

As identified in the background economic analysis the High Street and Glenferrie Road Activity Centre *"manages to retain a sense of vibrancy and activity throughout virtually all of the centre, despite its relative size and extensive length" and has an "extremely diverse range of traders that is comparable to only a small number of locations in Melbourne"*.

While there are particular uses which would be welcomed by the local community (such as a cinema, bars, a bowling alley and music venues) the planning system has limited ability to guide the establishment of specific businesses. What it can do is continue to support the existing situation while making minor changes to the planning controls to ensure that, as development pressures increase, land uses reflect the best outcome for the Centre.

Given the projected continued successful functional commercial operation of the Centre, the Structure Plan focuses on, and seeks to encourage the expansion, consolidation and augmentation of key places, including Malvern Central, the medical and health facilities along Wattletree Road and the community node around Malvern Library. Aspirations for new localised hubs are encouraged through the Structure Plan (such as creative industry etc) and particularly including Malvern and Armadale Train Stations.

Also by moderately increasing the amount of people living within the Structure Plan area, as well as increasing populations in the surrounding residential areas, will be of benefit to the Activity Centre by increasing the hours of activity and offer further custom for local businesses. Given the constraints of the Heritage Overlay and Neighbourhood Residential Zone, which apply to much of the residential hinterland, it is anticipated that the majority of the increases in residential population are likely to be accommodated by suitably integrated 'above-shop' infill development, however noting that the such increased above street level activity may accommodate both residential and office uses.



An increased residential population within the Activity Centre will support existing and new businesses

Economic Activity

Glenferrie Road-High Street Today

- Glenferrie Road-High Street serves a wider role than is typical for similar-sized centres and provides a diverse range of fresh food, café/restaurant, apparel and comparison shopping, retail and professional services.
- A total 617 shopfront tenancies were identified in Glenferrie-High Street Activity Centre, comprising 489 retail tenancies, 76 other commercial tenancies and 52 vacancies.
- The 52 vacant shopfront tenancies account for 8.4% of total shopfront tenancies and is higher than the typical range of vacancies for a well-performing street-based centre of between 4% and 6%. A higher share of vacant tenancies are present in the High Street component of the centre.

Glenferrie Road-High Street Main Trade Area

- Glenferrie Road-High Street serves a main trade area (MTA) which reflects the geographic region within which residents are likely to regularly use the centre for day-to-day retail and other needs. The boundaries of the MTA are based on factors such as strength of retail offering in Glenferrie Road-High Street, the location and performance of competing centres, access via main road routes and via public transport, and existing travel to work patterns.
- The MTA is located entirely to the south of the Monash Freeway and is generally bounded by the East Malvern Train Station, Waverly Oval and Darling Road to the east, Dandenong Road and Balaclava Road to the south, and Orrong Road, Chomley Street and Toorak Village to the west.
- The MTA has a socio-economic profile which is significantly more affluent than the metropolitan Melbourne average. This aspect is reflected in the higher levels of retail spending by MTA residents compared with the metropolitan area. The population of the MTA served by the centre is estimated at 62,190 persons in 2014 and is expected to increase by approximately 6,000 persons, to 68,190 persons in 2031.

Floorspace Assessment

- It is forecast that the Glenferrie Road-High Street centre could accommodate additional retail floorspace of up to 11,500m² by 2031. Retail floorspace growth could be accommodated through initiatives such as expansion of the existing Malvern Central Shopping Centre, supermarket expansion on Glenferrie Road, and/or the redevelopment and intensification of the existing built form in the centre, particularly along High Street and around Malvern Station/Dandenong Road and the Armadale Station.
- An aspirational goal to encourage the development of up to 20,000m² of commercial floorspace, focused around the Dandenong Road entry. Such an outcome is likely to be achieved over the longer-term in view of current economic conditions which do not support strong levels of office development.

- It is expected that Glenferrie Road-High Street, as an activity centre, will accommodate in the order of 75%-85% of the projected dwelling demand for Armadale and Malvern South over the period to 2031.
- On this basis it is considered prudent to plan for dwelling growth in the activity centre in the order of +800 to +900 dwellings by 2031, at an average rate in the order of +50 dwellings pa.

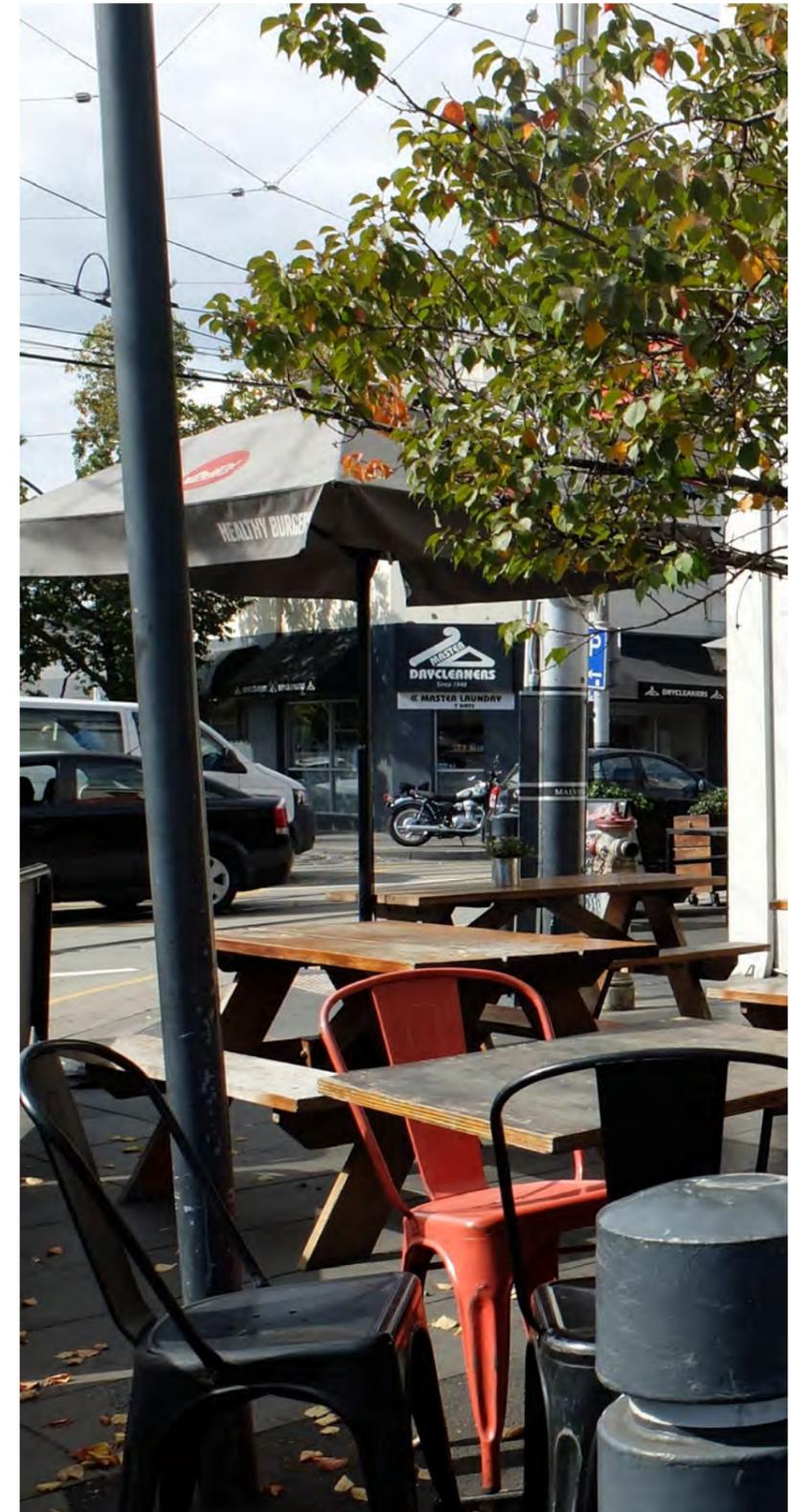
Relevant Key Directions

- Acknowledge High Street as a regional commercial and retail destination.
- Strengthen local and retail role of Glenferrie Road.
- Consolidate medical and health facilities along Wattletree Road.
- Encourage entrepreneurship, creative industry and special focal points (i.e. market, events etc.) in the Centre.

Objectives

Retail

- Build on the history of High Street and Glenferrie Road as a unique Melbourne shopping destination that does things differently and better than other shopping centres.
- Provide an integrated approach to the planning and development of Glenferrie Road-High Street, which reflects the various precincts that reflect how customers and visitors use different parts of the centre.
- Ensure that growth and change in the centre occurs in manner that doesn't negatively impact on the amenity of the centre to visitors and the local community.
- Increase market share in the face of continuing retail competition and generate new uses for existing and new retail floorspace. Retaining the popularity of Glenferrie Road-High Street with shoppers is essential in the face of increased competitive pressure from other centres.
- Accommodate additional retail floorspace through the expansion of the existing Malvern Central Shopping Centre, development of a future supermarket expansion on Glenferrie Road, and/or the redevelopment and intensification of retail activity in the balance of the centre.
- Find new niches in the retail sector and attract supporting activities, provide attractive amenities, and develop an enhanced sense of 'place' for the centre.
- Promote greater awareness of the diversity which exists in High Street-Glenferrie Road in order to generate higher levels of multi-purpose shopping trips.
- Better integration of key anchors the balance of the activity centre. For example, through better pedestrian linkages with Malvern Central, increased development of medical and para-medical uses adjacent to Cabrini Hospital, and higher levels of activity near the intersection of High Street and Glenferrie Road.



Ensure that growth and change doesn't negatively impact the amenity of the centre for residents and visitors

Office

- Ensure that the quality of office stock in the centre remains strong, particularly in the northern and southern parts of the centre where most of the dedicated office space exists.
- Seek more intensive office uses in Glenferrie Road-High Street through small-to-medium sized boutique developments that reflect the well-established built-form within the centre.

Residential

- Evidence across Melbourne shows that demand for higher-density residential development is extending outwards from the inner-city over time. In coming years, Glenferrie Road-High Street is expected to experience increasing pressure to accommodate higher-density residential development. This requires an appropriate response in the Structure Plan to ensure that market demand for additional housing is met without undermining the amenity of the centre for existing users.

Strategies

Important initiatives for future economic development, employment and investment in the Glenferrie Road-High Street activity centre include:

- Council to develop a strategy for the intersection of High Street and Glenferrie Road to become the symbolic 'heart' of the centre
- Support more-intensive retail activity by encouraging the redevelopment of the existing sites and tenancies in the centre
- Support comparison shopping in Glenferrie Road-High Street, with a particular emphasis on niche, independent and other non-chain stores (as appropriate, and not undermining the important role of major chain stores)
- Continue to work on improving the streetscape on Glenferrie Road-High Street to ensure that the centre continues to be seen as 'best practice' in Melbourne as a suburban street-based shopping and commercial precinct
- Allow flexibility for the provision of additional commercial floorspace should demand/interest eventuate.
- Work with property owners to unlock the potential for higher density-housing and other above-shop commercial development. This includes ensuring that the height limits applying in the centre are clearly understood by property owners and developers.
- Emphasise to developers the opportunity to incorporate residential and office components into mixed-use development schemes during pre-application discussions.

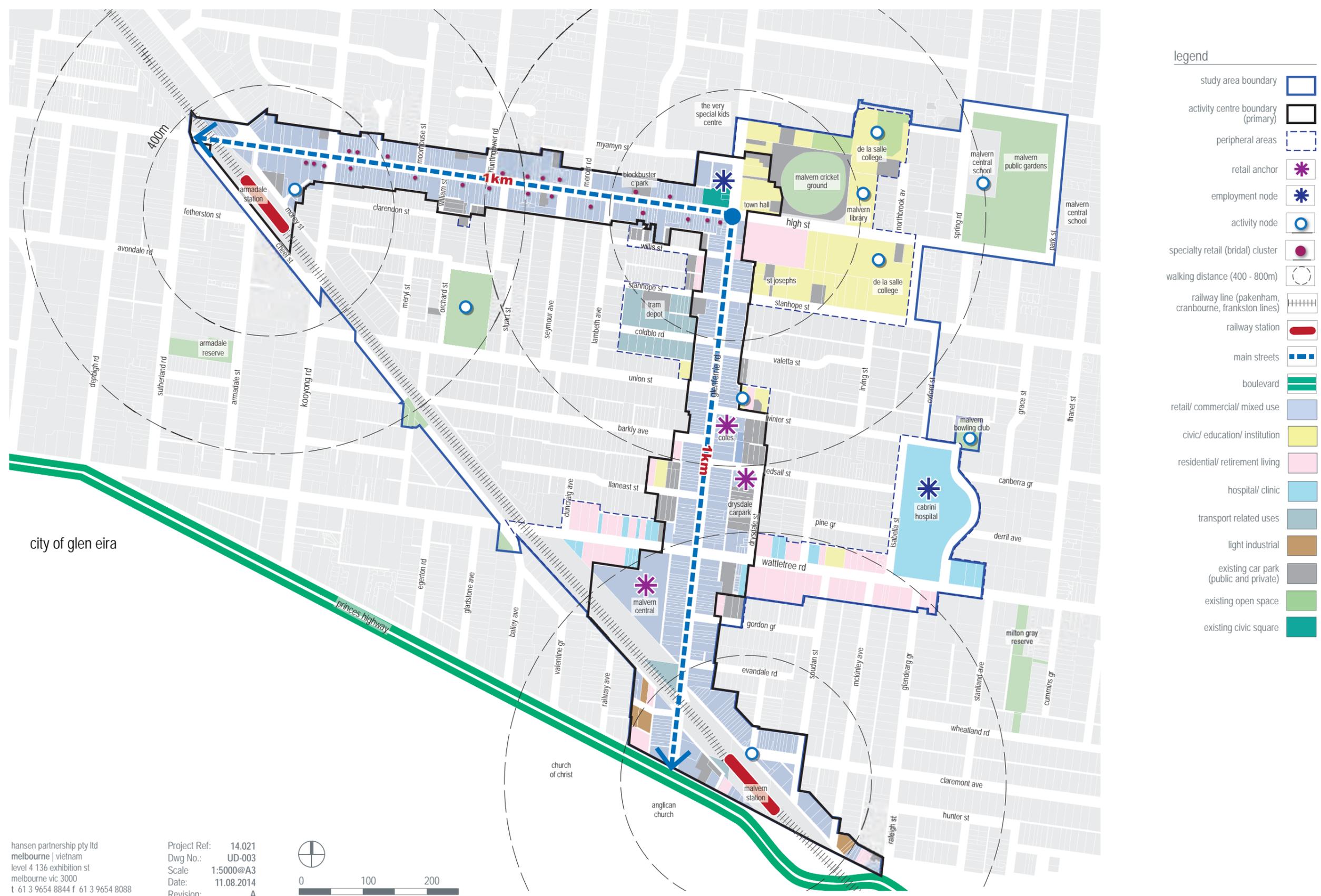
- Review, and if necessary upgrade, cycling access and infrastructure to reflect the growing popularity of this form of transport.
- Accommodate higher residential densities, particularly in the Malvern Station Precinct to the south of the rail line.
- Recognise the concentration of community and civic functions on the north-eastern fringe of the centre (without precluding retail and commercial development opportunities)
- Implement a set of performance benchmarks for marketing and promotional activities.

Additional detail on economic analysis and related recommendations is contained within the Economic Technical Report, which can be found at Appendix B.



Figure 6 Accommodate higher residential densities, particularly in the Malvern Station Precinct to the south of the rail line

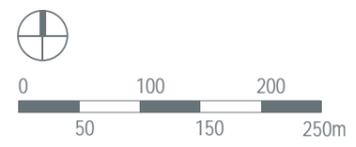
Figure 7 Land Use and Economic Activity



- legend
- study area boundary [solid blue line]
 - activity centre boundary (primary) [solid black line]
 - peripheral areas [dashed blue line]
 - retail anchor [purple asterisk]
 - employment node [blue asterisk]
 - activity node [blue circle]
 - specialty retail (bridal) cluster [red circle]
 - walking distance (400 - 800m) [dashed black circle]
 - railway line (pakenham, cranbourne, frankston lines) [grey line with cross-ticks]
 - railway station [red rectangle]
 - main streets [thick blue line]
 - boulevard [thick green line]
 - retail/ commercial/ mixed use [light blue square]
 - civic/ education/ institution [yellow square]
 - residential/ retirement living [pink square]
 - hospital/ clinic [light blue square]
 - transport related uses [grey square]
 - light industrial [brown square]
 - existing car park (public and private) [grey square]
 - existing open space [green square]
 - existing civic square [teal square]

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Access and Movement

The Glenferrie Road / High Street Activity Centre is anticipated to change over time, with an increase in the number of visitors and residents. This partly reflects the renewed desire of many people to live in close proximity to the range of retail, eating, services, and public transport. People living in the areas surrounding the Centre will also continue to be attracted to the evolving retail offering.

The changes envisaged are not a major readjustment to the nature of the area, however, they are likely to impact on how transport is used. Currently, travel by car to and from the Centre is the predominant mode choice, with congestion occurring at times. In the consultation process, provision of convenient car parking was one of the key issues raised. Whilst there is generally sufficient short-term parking for shoppers, at times parking opportunities become limited. Whilst many options for providing additional parking were put forward and explored as part of this process it was concluded that there is limited opportunity for the Council to significantly increase parking supply without the construction of costly decked or underground parking structures. There is also limited opportunity to provide for additional customer parking on most development sites (as they are not large enough to allow for convenient parking). Clearly, a new approach is necessary.

Council's Sustainable Transport Policy and supporting documents recognise that travel relates to the movements of people (and goods where appropriate) and not the movement of vehicles, thereby it prioritises transport modes in the following order:

- Walking
- Cycling
- Public Transport
- Commercial vehicles serving local businesses and institutions
- Multiple-occupancy vehicles
- Single-occupancy vehicles

This Structure Plan recognises that the continued dominance of the motor vehicle in the Centre would lead to a decline in its attractiveness as a place to live, shop and do business. By increasing the proportion of people using sustainable modes of travel the Centre will be more able to cater to any growth in the number of residents and visitors.

The Centre is well served by public transport with two rail stations (both on the Cranbourne, Frankston and Pakenham lines), and three tram routes providing easy access to a wide range of destinations. There are some good quality pedestrian facilities and urban realm treatments, and quiet local streets. However, a significant proportion of those living within close proximity still choose to drive. By enhancing existing pedestrian and cycling networks including the provision of 'Green Routes' around the Centre, it is these visitors and residents that the Structure Plan aims to attract away from the car.

Through these actions both road and parking capacity will also be freed up to allow those travelling from further afield, and those unable to use alternative modes to continue to drive to the Centre.

Among the outcomes pursued, a recasting of the area surrounding Armadale Station, investigate a 'shared space', where pedestrians, vehicles and cyclists can safely share the same road surface. These improvements will enhance the existing urban realm associated with Kingsway. Additional improvements for those walking in and through the area are also proposed, with measures to bring the walking environment on High Street more in line to that offered by Glenferrie Road. These are further supported by a network of 'Green Routes' linking routes along quiet streets to the centre where improved facilities are proposed.

The relative density of existing neighbourhoods in the vicinity of the Centre and the proximity of key attractions and services are ideally suited to travel by bicycle. One type of bicycle trip that is not currently catered for is utility trips. These trips are generally for specific purposes such as to visit shops and services, and are shorter (up to 2 km), often at slower speeds than longer commuter cycling trips. By providing cycling infrastructure aimed at this user type, demand could be reduced for other modes of transport. Therefore, cycling related improvements are proposed along the 'Green Routes' in addition to improved end of trip facilities (cycle parking) at key destinations.

A number of changes to the VicRoads controlled roads of High Street and Glenferrie Road are proposed, including a new signalised pedestrian crossing across Glenferrie Road in the northern end of the centre, to address the lack of pedestrian crossing opportunities in the area. Additionally, the establishment of bicycle lanes on Glenferrie Road is considered important to encourage users to cycle to the centre.

The Structure Plan will guide development within the Centre, and the transport aspects of development are no different with measures proposed to encourage low car use developments and to ensure that developments do not adversely impact their environs. An example of this is the strategy of preference of vehicle access by laneway (where appropriate) to ensure that main street frontages remain intact and that vehicle accesses are not scattered along the major pedestrian thoroughfares.

Relevant Key Directions

- Establish clear streets hierarchy and users' priorities
- Encourage and facilitate more sustainable modes of travel within and around the Centre
- Establish clearly defined pedestrian linkages and manage road crossing points
- Protect and enhance the laneway network within the Centre
- Encourage development to provide a secondary address to rear lanes behind Glenferrie Road.
- Encourage development to provide a secondary address to the rear at Commercial interfaces.



Despite the excellent level of public transport, cars will continue to have a significant ongoing role



Access to existing transport infrastructure increases the opportunity for sustainable transit uptake

Objectives

- To improve pedestrian amenity thereby encouraging visitors to walk to and around the centre.
- To provide improved connections to and through the centre and end of trip facilities for cyclists to encourage travel by bicycle.
- To maximise the use of existing car parking facilities given the limited opportunities to provide additional car parking.
- To improve pedestrian crossing opportunities across the arterial roads in the centre.
- To address areas of pedestrian/cyclist/tram/vehicular conflict.
- To reduce the dominance of motor vehicles in predominantly pedestrian areas (e.g. Kingsway, Armadale).
- To integrate and enhance public transport infrastructure appropriate to the Centre.
- To guide the design of new developments to support low car use.

Strategies

A range of transport related strategies are proposed, which individually addresses various modes of transport, including: walking, cycling, public transport, private vehicles and new development, each of which are addressed in turn below.

Walking

- Investigate the provision of a pedestrian crossing facility at a suitable location on Glenferrie Road between High Street and the signalised crossing south of Edsall Street to improve pedestrian crossing opportunities within this strip.
- Enhance the High Street walking environment through improved footpath surfacing and side road intersection upgrades (e.g. as per Glenferrie Road).
- Provide kerb buildouts to assist pedestrians (by reducing the width of road to be crossed) where formal pedestrian crossings facilities are not warranted.
- Provide pause points including the provision of seating at key locations.
- Enhance existing pedestrian and cyclist connections parallel to the rail line, including investigating a new link along the northern side of the railway reserve adjacent Malvern Central.
- Encourage safety and public realm improvements at the intersection of Glenferrie Road and Coldblo Road through discussions with tram operators.
- Introduce enhancements to the walking environment along designated "green routes" through ensuring high quality walking surfaces, provision of pedestrian refuges to assist in the crossing of roads, additional seating for pedestrians (particularly older pedestrians), and adequate lighting for security.

- Improve pedestrian wayfinding signage to key destinations within the Centre, including showing walking times on signs.
- Support programmes that encourage students of schools within the Study Area to walk to and from schools, such as those that support 'safe routes'.

Cycling

- Liaise with VicRoads to advocate for the establishment of bicycle lanes on Glenferrie Road and High Street (part-time only) given that this route is identified as a part of VicRoads Principal Bicycle Network.
- Encourage VicRoads for the provision of bicycle priority road marking at the signalised intersections within the centre to protect cyclists.
- Review the bicycle wayfinding strategy that guides cyclists around the Centre via safe and convenient routes.
- Support programmes that encourage school students within the area to participate in cycling, such as the Cycle with Confidence programme and Ride 2 School programme.
- Encourage Metro Trains to install secure long-stay bicycle parking facilities at Armadale and Malvern rail stations to encourage mixed mode trips.
- Provide additional bicycle parking facilities in areas of high demand, including at community facilities, parks, and in the vicinity of cafes that integrate into surrounds.
- Improve connections between Union Street Reserve and High Street through cyclist bypasses (Orchard Street and William Street), and contra-flow cycling lanes along one-way streets (Clarendon Street).
- Enhance the cycling environment along the designated "green routes" through additional cycle parking, converting the existing pedestrian operated traffic signals on Kooyong Road at the intersection of Morey Street to a pedestrian and cyclist (Toucan) crossing, and converting off-road pedestrian paths to paths (where appropriate).

Public transport

- In conjunction with Tram Operators and VicRoads investigate additional tram priority measures at signalised intersections and pedestrian operated signals.
- Investigate the benefits of an eastbound evening peak hour tram lane on High Street to coincide with the existing clearway hours of operation.
- Encourage PTV to provide pedestrian wayfinding signage to Armadale Station from High Street (via Morey Street).



Lobby for the establishment of bicycle lanes on the main road network



Provide for designated bicycle facilities near to public transport nodes

Access and Movement (continued)

Private vehicles

- Investigate the implementation of a 40 km/h speed limit through all streets within the Centre.
- Support a system of providing parking which maximises the use of existing car parking facilities, including making changes to parking restrictions to maximise the availability of car parking for visitors to the Centre (where justified).
- Investigate implementing a 'shared space' where all road users utilise the same area to reduce the dominance of motor vehicles in the predominantly pedestrian area of Morey Street and Kingsway in the vicinity of Armadale Station.
- Investigate implementing a shared space treatment along Station Street and Claremont Avenue;
- Consider altering vehicle priorities at the intersection of Station Street and Soudan Street to discourage through traffic of Station Street (should the shared space proposal on Station Street be pursued).
- Rationalise the vehicle movements at the intersection of Drysdale Street, Edsall Street, and the Winter Street car park through an intersection redesign with the aim of reducing user confusion and improving safety (potentially by relocating the access into and out of the car park).
- Redesign the intersection between Llaneast Street car park and Wattletree Road to address road safety issues (potentially by converting the Llaneast car park access to entry only and providing a footpath).
- Seek to slow vehicles using the laneway parallel to Glenferrie Road adjacent to the Drysdale Street car park through the introduction of traffic calming.
- Ensure that rear lane widths are sufficiently wide to accommodate building servicing and car park access, so as to preserve the primary active frontage to the main street (High Street, Glenferrie Road and Wattletree Road).
- Where a laneway width is insufficient for efficient vehicle movements require a minimum 1.5 metre setback.



The implementation of a 40km/h limit through the centre would increase safety for pedestrians and cyclists



Avoid crossovers within Kings Way, to maintain a pleasant, pedestrian focussed environment

New developments

- Encourage developments which support low car use through generous provision of cycle parking (above Planning Scheme Requirements) and those that facilitate car share schemes.
- Require developments to utilise laneways for vehicle access to retain active street frontages along main streets.
- Seek to upgrade the walking experience along laneways with a pedestrian function through the provision of low level lighting and through opportunities associated with new developments (increasing laneway widths).
- Encourage the design of developments to incorporate passive surveillance of rear laneways and pedestrian paths.
- Ensure that development of sites adjoining the intersection of laneways and streets address pedestrian visibility issues.
- Ensure developments fronting High Street and Glenferrie Road provide weather protection in the form of canopies to improve walkability during periods of inclement weather.
- Explore opportunities for new pedestrian connections between Malvern Central and Malvern Station via Glenferrie Road.
- Ensure that future developments at the Coldblo Tram Depot consider the access to and from the site by all modes.
- Avoid new crossovers to Kingsway, or manage these to ensure pedestrian prioritisation.

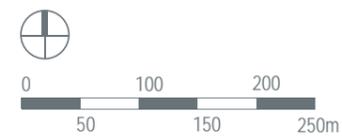
Additional detail on traffic and integrated transport and related recommendations are contained within the Background Transportation Report, which can be found at Appendix D.

Figure 8 Access and Movement



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city of glen eira

princes highway

Public Realm and Landscape

The urban structure of the study area provides an opportunity for street based connections between the main streets, including access to open space assets. However, these routes are often convoluted, and lack the legibility to function as important pedestrian links. There is an excellent opportunity to enhance these existing links through improvements to residential streets which connect between the Glenferrie Road, High Street and Wattletree Road spines. These works should be underpinned by a focus on green infrastructure, comprising new soft landscaping, Water Sensitive Urban Design (WSUD) treatments, public realm fixtures and high quality way-finding. It will be important that the footpath connections and street crossings provide for Disability Discrimination Act (DDA) compliant movement to ensure equitable access. Given new opportunities for green space are limited, the ability to better utilise and connect to existing open space such as Malvern Cricket Ground and Union Street Park will be imperative to the management of a high quality environment for a growing localised population of workers and residents.

The diversity of experience between Glenferrie Road and High Street is a positive attribute which should continue to be built upon, both in built form and public realm terms. Whilst Glenferrie Road has a high quality pedestrian experience, with slower vehicle movements, the hostility of vehicle movements along Wattletree Road and High Street however will warrant some further consideration of footpath layering devices such as planters or other public realm fixtures to improve the sense of refuge for pedestrians.

The Town Hall and Library are significant anchors within the centre, but lack connectivity to one another, with poorly considered 'in between' spaces around the edges of Malvern Cricket Ground. An excellent opportunity exists to rework existing accessways, parking areas and residual garden space to provide for informal recreation opportunities and enhance the role of Malvern Cricket Ground as a significant open space asset. This treatment should also seek to connect through to and complement Malvern Square and new Council Offices across Glenferrie Road to the west.

Existing kerb build-outs occur throughout the centre and in combination with priority surface treatments at crossings achieve a pleasant pedestrian environment. This strategy should be extended along High Street and Glenferrie Road and combined with new public realm fixtures integrated in an overall 'kit of parts' to create a unique sense of identity for the centre. These stopping spaces are an important part of the sequence of movement along the main streets, and provides a valuable opportunity for canopy tree planting.

A number of significant spaces are noted within the study area which represent an excellent potential opportunity for temporary or more permanent event spaces. Such temporary uses are a proven method of resolving conflicts with vehicular requirements, whilst also allowing the 'claiming' of additional space for community use. More specific opportunities exist to leverage off the distinct spaces at Claremont Avenue / Station Street (Malvern Station), Kingsway/ Arcade (Armada Station) and Coldblo Tram Depot, where street based festivals, community arts and cultural events or markets could be accommodated.

Permanent public realm improvements within these precincts should have regard to the potential function of these event spaces and allow for a suitable flexible condition, through the use of kerb-less streets, and limited or movable physical barriers between vehicle and pedestrian movement.

Relevant Key Directions

- Strengthen and Establish clearly defined strategic redevelopment sites into the Centre.
- Establish diverse experiences along different streets within the Centre
- Establish clearly defined pedestrian linkages and manage road crossing points
- Strengthen public meeting space at and around important junctions
- Establish equitable access to all existing open spaces
- Establish potential event spaces at key sites (market, special event etc.)
- Establish additional vegetation in the public realm to contribute to the urban forest.

Objectives

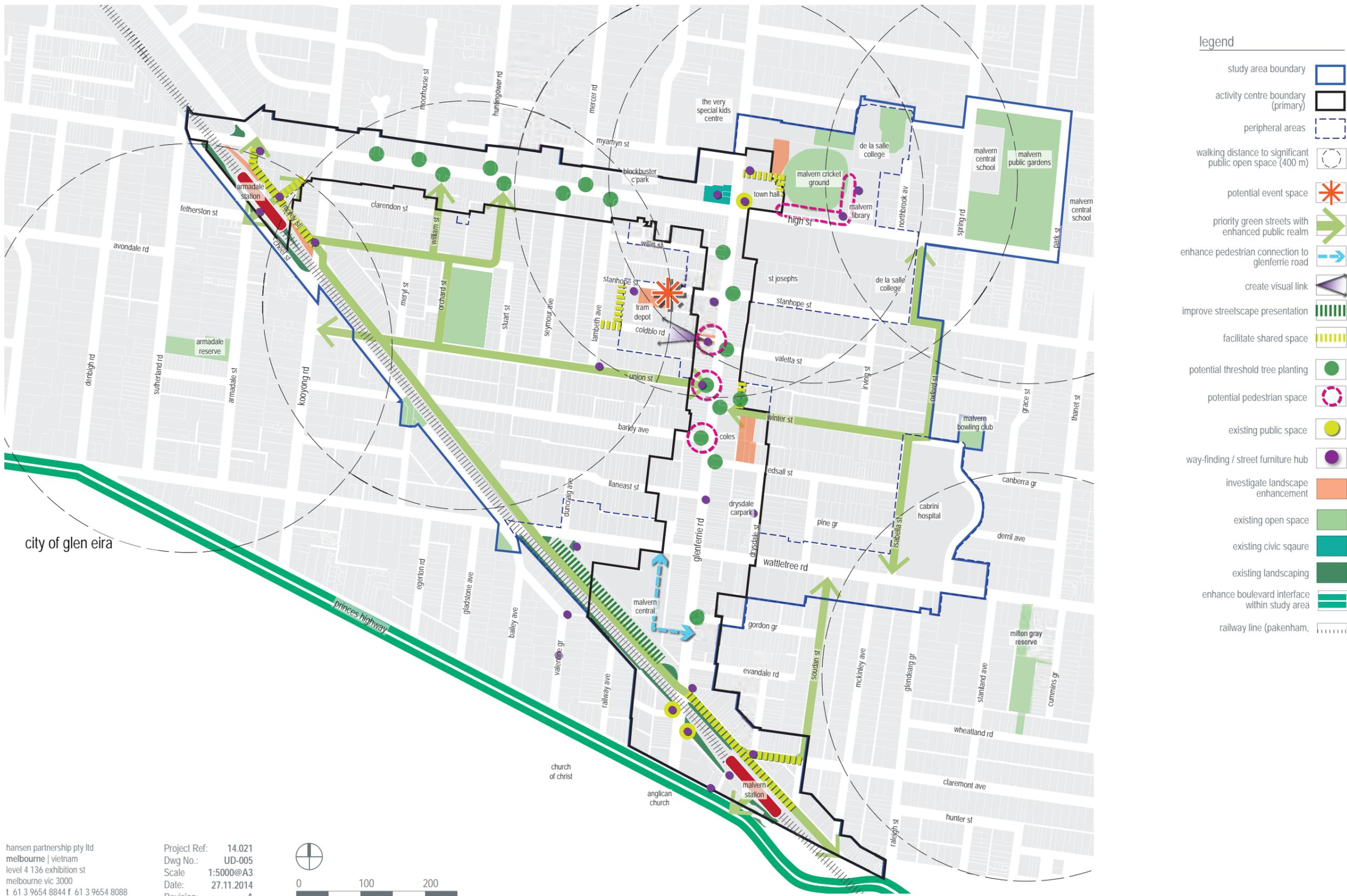
- Provide for a strong way-finding and branding strategy for the centre, to better unify disparate precincts within the broader study area.
- To provide public realm treatments at key entries into the study area which respond to both the pedestrian and vehicle scale of movement.
- Strengthen the legibility of existing street based connections between the main streets, public open spaces and local streets within the study area.
- Incorporate green infrastructure initiatives as an integral component of any new public realm works through management of stormwater runoff, canopy planting and soft landscaping works.
- Encourage the potential use of special places within the Activity Centre for temporary public use, including Coldblo Tram Depot, Claremont Avenue and Kingsway to increase public appreciation of existing 'gems' within the study area.
- Provide opportunity for residents and visitors to linger and meet without having to spend money within the public realm of Glenferrie Road, High Street and Wattletree Road.
- Improve the public realm experience within the Civic Precinct, and legibility of connections between Malvern Square, the Town Hall, Library, Police Station and Malvern Cricket Ground.
- Encourage greater use of Malvern Cricket Ground and its edges for informal public recreation use.
- Ensure that intensive redevelopment of the allotments fronting Dandenong Road to contribute to public realm enhancements along the rail alignment and adjacent to Malvern Station.

- Improve the legibility of the Malvern Station, Claremont Avenue and Station Street connections to Glenferrie Road.
- Enhance the existing qualities of Kingsway/ Arcade and Armada Station public realm, whilst increasing the emphasis on priority pedestrian movement.
- Encourage the potential provision of a pedestrian connection between Malvern Central and Glenferrie Road, so as to increase connectivity between Malvern Station and Malvern Central.
- Reconfigure back of house surface parking areas with increased emphasis on public realm treatment, and the creation of new safe and high quality pedestrian connections.
- Encourage the retention of arcades between Glenferrie Road and back of house parking with greater emphasis on the creation of safe, well lit and pleasant pedestrian environments with all-hours access.
- Encourage the retention of existing significant trees and street trees which contribute an important amenity to the public realm, whilst forming an important part of Stonnington's urban forest.

Strategies

- Develop a strong wayfinding and branding strategy for the centre, comprising a 'kit of parts' for public realm fixtures with a high level of design quality reflective of its time and place. This should be distinctive for the study area rather than one which applies throughout Stonnington.
- Prepare a landscape masterplan for the Civic Precinct encompassing new shared and pedestrian links, passive open space and informal gathering spaces.
- Prepare a landscape masterplan for the Drysdale Street car park to incorporate new public realm treatments and enhanced north-south pedestrian connections through the space. Ensure works complement future development opportunities and ambitions of adjacent key tenants.
- Provide for increased function for existing and future kerb build outs comprising seating, planting and other public realm fixtures to enhance their role as pause points, and to create subtle differences between each location.
- Develop a typical street section treatment for 'green streets' which connect the main roads through the residential hinterland with integral green infrastructure, way finding and public realm fixtures.
- Facilitate or engage with local community groups, within Council and private operators to explore opportunities for the establishment of temporary or more regular event spaces within Claremont Avenue, Station Street, Kingsway and Coldblo Tram Depot.
- Engage with the owners of Malvern Central to explore opportunities for a new pedestrian connection to Glenferrie Road.

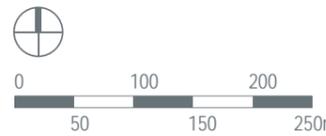
Figure 9 Public Realm and Landscape



- legend**
- study area boundary [blue solid line]
 - activity centre boundary (primary) [black solid line]
 - peripheral areas [blue dashed line]
 - walking distance to significant public open space (400 m) [dashed circle]
 - potential event space [orange starburst]
 - priority green streets with enhanced public realm [green arrow]
 - enhance pedestrian connection to glenferrie road [blue arrow]
 - create visual link [purple triangle]
 - improve streetscape presentation [green vertical bars]
 - facilitate shared space [yellow vertical bars]
 - potential threshold tree planting [green circle]
 - potential pedestrian space [pink dashed circle]
 - existing public space [yellow circle]
 - way-finding / street furniture hub [purple circle]
 - investigate landscape enhancement [orange square]
 - existing open space [light green square]
 - existing civic square [teal square]
 - existing landscaping [dark green square]
 - enhance boulevard interface within study area [green double line]
 - railway line (pakenham) [dotted line]

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Built Form and Heritage

Given the relatively modest level of growth anticipated both in residential and commercial development within the Study Area, the area as a whole is not anticipated to change substantially from its current image when compared to the substantial growth anticipated for other Activity Centres such as Chapel Street Activity Centre. Accordingly, the proposed built form and heritage guidance seeks to reinforce the existing qualities and difference between sub precincts within the Study Area, whilst ensuring that new insertions or precincts of greater change are carefully managed carefully within the overall framework.

Based on current growth trajectories, it is estimated that 80% of the houses that exist today will still be standing in 50 years (Trina Day, 2012)¹. Further to this, additional housing growth within the Neighbourhood Residential Zone areas and Heritage Overlay precincts proximate to the Activity Centre Boundary will be minimal, emphasising the need for growth to be contained within the Activity Centre Boundary. This suggests that the existing, positive abrupt transition between commercial and low rise residential building stock in the hinterland will be maintained and emphasised in some instances. In this sense, rather than promoting a typical 'wedding cake' or terracing of building format, that it may be acceptable to have a more shear profile of 3-4 storeys, with a single setback above a single storey form. Through a uniform requirement for a 4.5m laneway offset, privacy mitigation can be more readily dealt with by measures other than screening, whilst rear facades at might take on equivalent significance through exposure to the street frontage and improved building composition.

From yield and typology testing, the built form scale proposed by the Structure Plan has been devised to comfortably enable the housing growth projections anticipated by the Economic Technical Report. This is to be realised through a combination of selective infill, as well the concentration of higher order growth to large lots within the Dandenong Road Precinct, as well as along High Street and Wattletree Road. The general scale of 4 storeys with some potential for a 5th level in designated areas on larger than typical allotments is sufficient to encourage investment whilst not representing a significant departure from the existing scale of built form. Conversely, the heights proposed for Dandenong Road and Malvern Central represent a concerted ambition to transform and strengthen the image of these precincts, based on their ability to accommodate more robust built form without negative impacts on surrounds.

The method of built form and heritage management is broken into a series of elements, comprising 'generic' streetscape conditions and 'framework plan' areas. The generic streetscape condition principle assumes that there are a series of common allotment types within High Street, Glenferrie Road and Wattletree Road, within which a range of small, medium, large and corner allotments will occur, for which built form modelling has been prepared.

With respect to areas of specific interest, the approach slightly differs, with an 'acupuncture' approach to new built form initiatives, to strengthen interfaces, preserve heritage values and enhance the intensity and activity of the space. These framework plans have been prepared for 5 specific precincts including: Armadale Station, the Civic Precinct, Drysdale Street Carpark, Coldblo Tram Depot and Malvern Station. Additional attention is given to Dandenong Road, where the area suffers from a poor built form image, and naturally invites a higher order of infill development, in order to position a greater population directly adjacent to Malvern Station.

A common challenge for higher intensity infill development within period tram-street shopping centres is the desire to preserve the heritage built form which makes the places unique and valued by the community. The desire in this instance is to ensure that this valuing of heritage extends beyond the immediate façade to some greater portion of the fabric of the building (including the retention of original roof forms where possible). Whilst the preferred setback of upper level form is 3m for non-contributory built form, the response to heritage conditions will defer to the requirements of the Heritage Overlay and existing policies at Clause 22.04, as distinct from the generic outcomes sought in the built form precinct diagrams.

The retention of heritage commercial forms, will also warrant some greater consideration of rear interfaces, which will have an increased role as a secondary address as well as the primary location for servicing and vehicular access. These rear areas will require further consideration of lighting, tactile materiality and wayfinding signage. Widening of laneways will be necessary to ensure their ongoing function, in order to maintain a primary active commercial frontage to High Street, Glenferrie Road and Wattletree Road.



Existing mixed use development within Study Area

Relevant Key Directions

- Provide guidance to possible future expansion of Malvern Central.
- Strengthen and Establish clearly defined strategic redevelopment sites into the Centre.
- Establish clear urban form and design guidance throughout key strategic development areas.
- Protect and enhance significant heritage fabric along Glenferrie Road and High Street.
- Encourage development to provide a secondary address to rear lanes behind Glenferrie Road.
- Encourage development to provide a secondary address to the rear at Commercial interfaces.

Objectives

- To ensure that individually significant heritage forms within the centre are given adequate regard in terms of retention of substantial fabric in any infill development scenario.
- To maintain a modest scale of infill development within High Street and Glenferrie Road, which complements the existing heritage fabric.
- To ensure that new insertions along High Street are clearly distinguished as high quality contemporary elements adding a layer of diversity to the streetwall.
- Ensure that new buildings along High Street enhances the image of the street as a boutique retail destination through high quality contemporary architecture, integrating branding and signage where appropriate.
- To ensure that new insertions within Glenferrie Road are more recessive and neutral in profile and presentation to draw attention to the primary order of existing heritage built form.
- To facilitate and focus use and development of medical aligned activities along Wattletree Road to better stitch Medical uses back to the Glenferrie Road spine.
- To establish substantial growth opportunity south of the railway alignment within the Dandenong Road precinct, to enhance the image and sense of arrival to the Activity Centre at the entrance to Glenferrie Road
- Carefully manage change within significant character precincts of Claremont Avenue and Kingsway with a greater emphasis on heritage over new development opportunities.
- Ensure that development of Malvern Central is carefully managed within a vertical footprint with improved public realm interfaces.

- Should Malvern Central seek to expand over the rail alignment, ensure that this provides improved interface and connection conditions to Glenferrie Road and positive presentation from the rail corridor.
- Should the Coldblo Tram Depot become redundant for public transport requirements in future, seek to ensure that the significant cultural and architectural value is preserved, integration with the surrounding street network is improved and interfaces with lower scale residential neighbours is carefully managed.
- Ensure that new built form oriented onto rear carparks provides high levels of surveillance from upper level uses, and an improved presentation.

Strategies

- Draft a local planning policy and Design and Development Overlay for land within the Activity Centre Boundary and implement through a formal Planning Scheme Amendment, so as to give effect to the findings and recommendations of the Structure Plan.

Additional Interface Design Guidance
Highway/ Railway Reserve Interface

Proposed typical building profile for the 'highway/ railway reserve' interface is applicable for designated sites, away from the traditional Glenferrie Road and High Street spines. The recommended building profile acknowledges the precinct's renewal potential. For properties without an immediate sensitive residential abuttal, lots with primary frontage to Dandenong Road and the railway reserve have the capacity to establish a more robust presentation of up to 4 storey streetwall.

Local street interface

Side street interface is applicable for designated corner sites located throughout the Structure Plan area . The anticipated building profile acknowledges the streetwall requirements stipulated within the Glenferrie Road/ High Street interface conditions and the visual exposure along the side streets. Active frontages should turn the corner with a sense of address to the side streets. In absence of immediate sensitive residential abuttal, there is capacity for upper levels to be built to boundaries where it demonstrates no adverse amenity impact onto the public realm.

Laneway/ Rear Interface

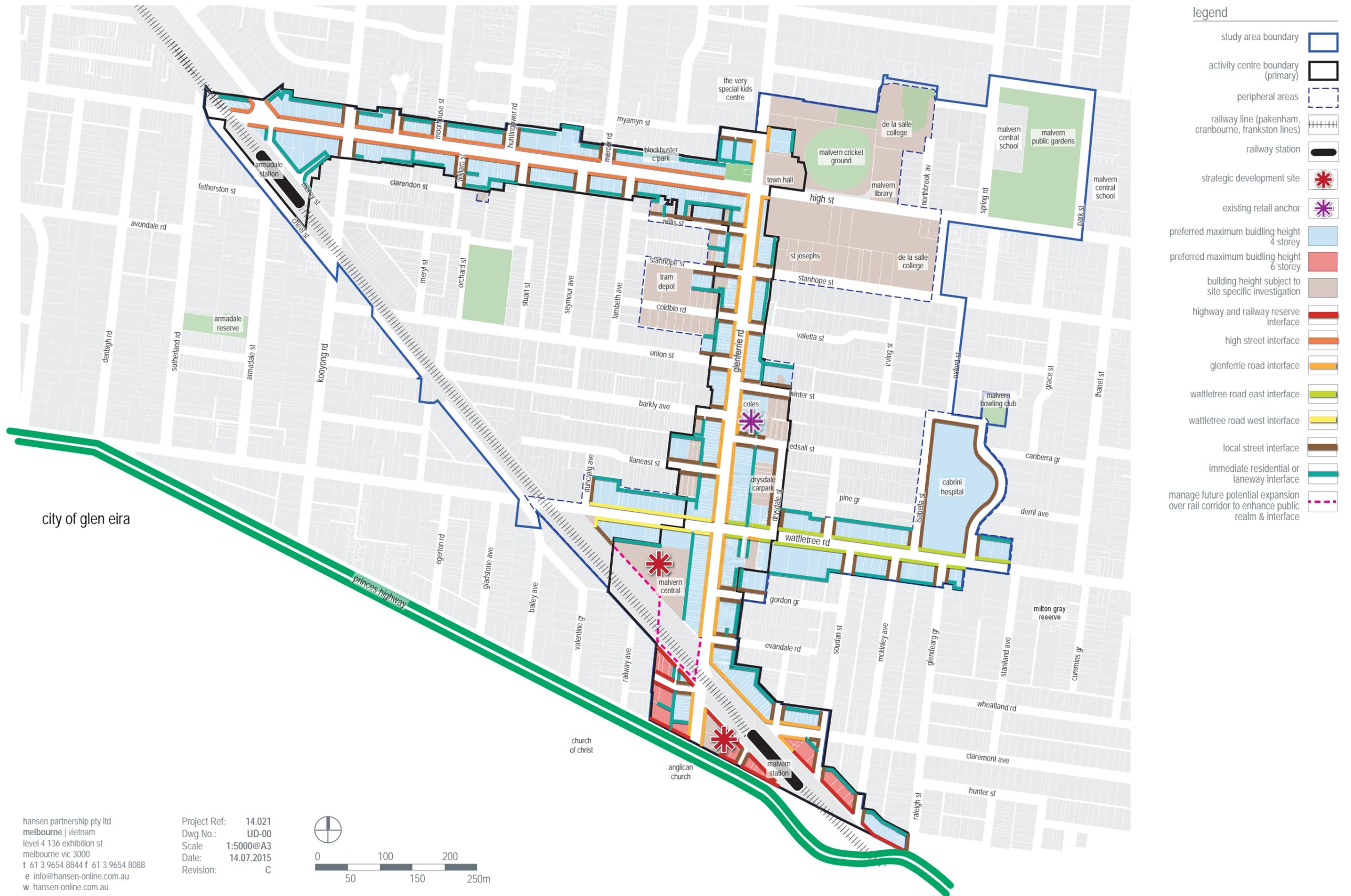
For 'rear interface' to laneway (residential/ sensitive interface), the recommended building profile incorporates a mid-block setback at the top level to allow greater separation and mitigate overwhelming sense of visual bulk. This mid-block setback does not apply to the 'rear interface' to laneway (non-residential).

Where future private open space (balconies) have a direct interface to the rear, it must not encroach beyond the recommended building envelope/ profile to maintain the integrity of building separation and minimise potential overlooking.



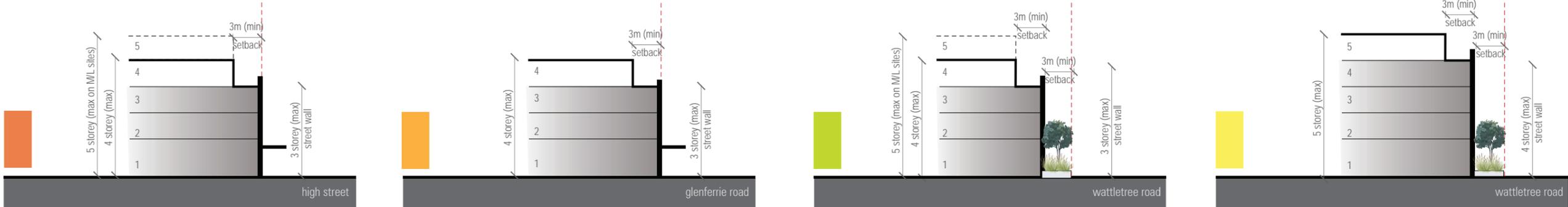
Existing residential development within Study Area

Figure 10 Built Form and Heritage

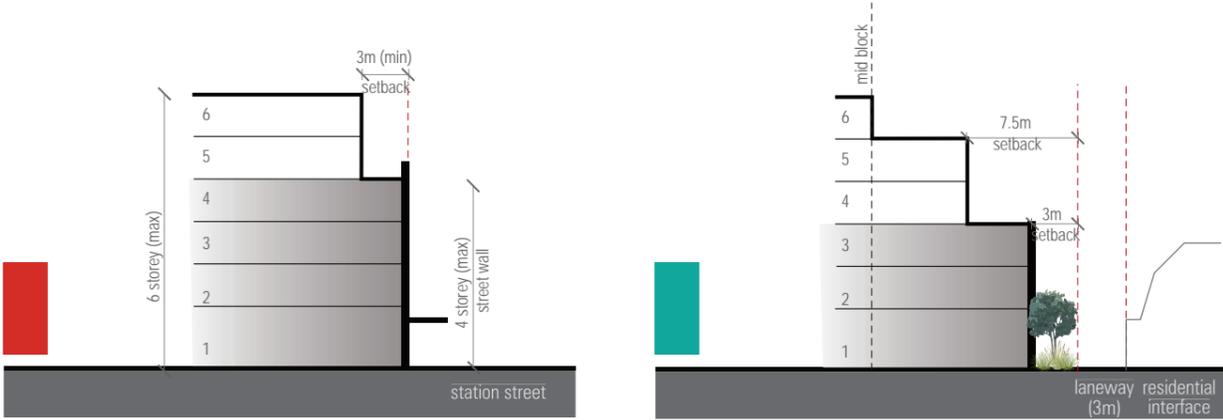


Typical Edge Condition

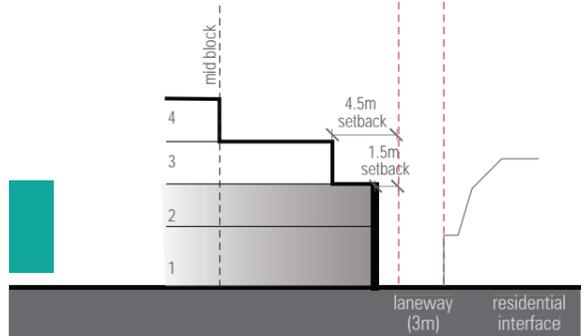
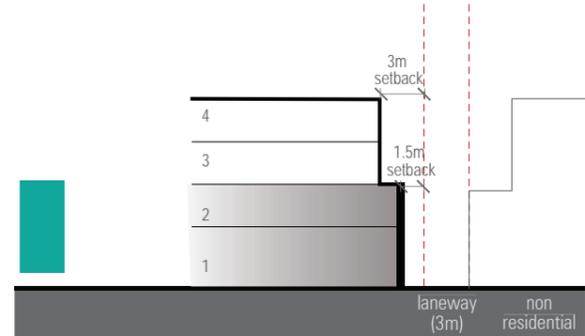
Main Spine Interfaces



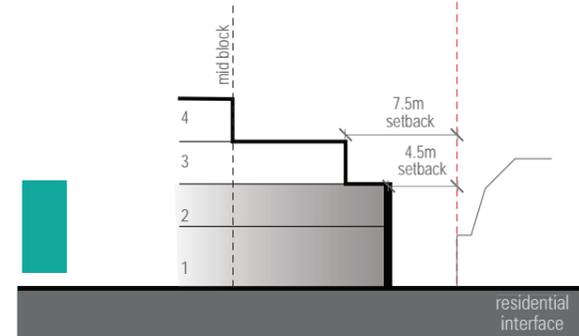
Highway / Rail reserve interface



Rear/ Laneway Interface



Rear Interface (no laneway)



Note:
Sensitive Residential Interface: sites with an immediate abuttal to a residential property (or adjacent along the rear laneway interface).
Non-Sensitive Interface: sites with an immediate abuttal non-residential property (or adjacent along the rear laneway interface).

Strategic development Site: Malvern Central

The preferred building form on the Malvern Central Strategic Development Site includes the following elements:

1. Maximum scale of 10 to 12 storeys at the north eastern corner, stepping down to 8 storeys to the south and west (when measured from the Wattletree Road frontage).
2. Separated slender tower forms with a maximum width of 25 metres.
3. Provide separation greater than 10 metres between potential tower forms on or off site.
4. A 2/3 storey street wall/ podium (equivalent to 3/4 storey residential) along Wattletree Road to maintain a consistent parapet line.
5. Preferred setback of 12 metres from the Wattletree Road parapet line to maintain Wattletree Road streetscape presentation.
6. Preferred setback to other external boundaries of 5 metres (minimum).
7. Any development of VicTrack land to match the future parapet line of Malvern Central's rear elevation.
8. No overshadowing of residential allotments to the south side of the railway line, or the footpath on the eastern side of Glenferrie Road should be caused at the equinox (22 September) between 9am and 3pm.

Building scale is required to be confirmed and determined through further site specific investigations and its positive net contribution to its context. Site specific investigations will require the preparation of a detailed urban form and public realm/ urban landscape analysis to determine an appropriate built form scale and public contribution.

Recommendations regarding built form scale for Malvern Central should clearly demonstrate how proposed built form is moderated by and respects the existing and proposed built form within the surrounding locality. Site specific investigations to inform built form scale must consider:

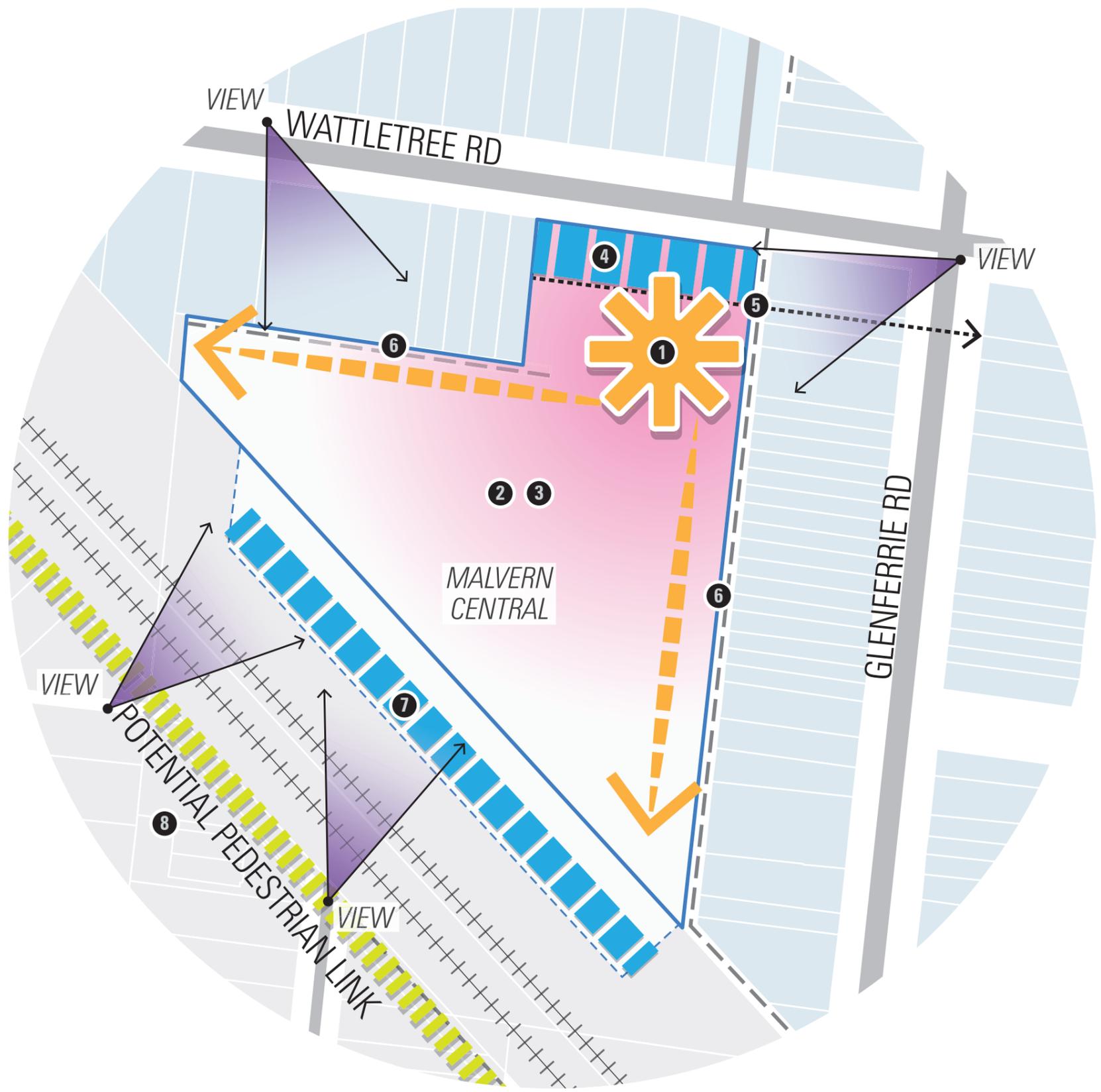
- Current built form (typology, height datum, grain and aspects) within 500 metres of the site.
- Built form recommendations as outlined within this Structure Plan, with particular reference to the 4 storey scale along Glenferrie Road and Wattletree Road.
- Built form scale associated with the preferred number of storeys to be guided by 4.0 m at ground level and 3.5 m for upper levels.

- The siting and orientation of taller built form to be centralised within the site to maintain prominence of heritage built form, particularly when viewed from Glenferrie Road.
- Immediate and longer range views of the site from public vantage points, including: Glenferrie Road, Wattletree Road and the rail corridor.
- Mitigation of potential visual bulk through separated towers with varied building heights, rather than horizontal setbacks alone.
- Opportunities to improve the southern elevation of Malvern Central and ensuring façade treatments address frontage interfaces and views to the site.
- Opportunities to improve pedestrian connectivity to Glenferrie Rd and between Malvern Central and Malvern Station.
- The utilisation of roof decks of lower forms to provide communal gathering space, urban landscape and a high quality outlook from upper level commercial or residential uses.
- Initiatives and objectives associated the Malvern Central Precinct Framework Plan (Figure 21).
- Development equity for abutting/ adjacent sites to the north and east through sufficient building separation (i.e. to maximise daylight access to lower floors, managing outlooks and external aspects, and privacy matter).
- An exemplary standard of internal amenity and avoiding reliance on privacy screening, as strategic development sites should accommodate amenity matters within its own site.
- Promotion of sustainability measures including: sustainable design, sustainable transport, minimising heat island effect etc.



Malvern Central from Glenferrie Road

Malvern Central built form diagram



Strategic Development Site: Dandenong Road Gateway

The preferred building form on the Dandenong Road Gateway Strategic Development Site includes the following elements:

1. The retention of the corner heritage form (Angel Tavern) in situ, without any upper level additions.
2. Preferred maximum building height of 8 storeys (when measured from the Dandenong Road frontage) including with appropriate design and management of built form at the interface the retained corner heritage form (Angel Tavern).
3. Ability to increase building height to 10 storeys with the provision of a 3 metres ground level setback of new buildings along north and south boundaries (so as to facilitate more generous ground level pedestrian space). 9th and 10th storey required to be setback to minimise overshadowing of Dandenong Road.
4. Preferred setback for upper levels above the podium is 5 metres (minimum).
5. Provide separation greater than 10 metres between potential tower forms on or off site.

Building scale is required to be confirmed and determined through further site specific investigations and its positive net contribution to its context. Site specific investigations will require the preparation of a detailed urban form and public realm/ urban landscape analysis to determine an appropriate built form scale and public contribution.

Recommendations regarding built form scale for Dandenong Road Gateway site should clearly demonstrate how proposed built form is moderated by and respects the existing and proposed built form within the surrounding locality. Site specific investigations to inform built form scale must consider:

- Current built form (typology, height datum, grain and aspects) within 500 metres of the site.
- Built form recommendations as outlined within this Structure Plan, with particular reference to the 4 storey scale along Glenferrie Road.
- Built form scale associated with the preferred number of storeys to be guided by 4.0 m at ground level and 3.5 m for upper levels.
- The siting and orientation of a slender vertical projection to be located away from the Dandenong Road and frontage to maintain prominence of the former Railway Hotel (Heritage Overlay Schedule 403).
- Potential taller form/s on this site should contribute positively to the skyline interest.
- A potential plot-ratio arrangement which balances additional floor space with ground level publicly accessible open space.
- Immediate and longer range views of the site from public vantage points, including: Dandenong Road, Glenferrie Road and Station Street.

- Ensuring potential façade treatments address frontage interfaces and views of the site, including opportunities to activate the northern façade along the pedestrian walkway to improve passive surveillance of the station environs and pedestrian connectivity to Malvern Station.
- Initiatives and objectives associated the Malvern Station Precinct Framework Plan (Figure 18).
- Development equity for abutting sites through sufficient building separation (i.e. to maximise daylight access to lower floors, managing outlooks and external aspects, and privacy matter).
- An exemplary standard of internal amenity and avoiding reliance on privacy screening, as strategic development sites should accommodate amenity matters within its own site.
- Promotion of sustainability measures including: sustainable design, sustainable transport, minimising heat island effect etc.

Strategic Development Site: Glenferrie Road Gateway

The preferred building form on the Glenferrie Road Gateway Strategic Development Site includes the following elements:

6. A podium form with a 3-4 storey base and preferred maximum building height of 8 storeys (when measured from the Glenferrie Road frontage).
7. Setback rising form behind the podium parapet line by at least 3 metres to the north and west, and at least by 5 metres to the south and east.
8. Position tallest form centrally within the site away from Glenferrie Road Railway Reserve junction and the adjacent heritage form (Angel Tavern).
9. Provide separation greater than 10 metres between potential tower forms on or off site.

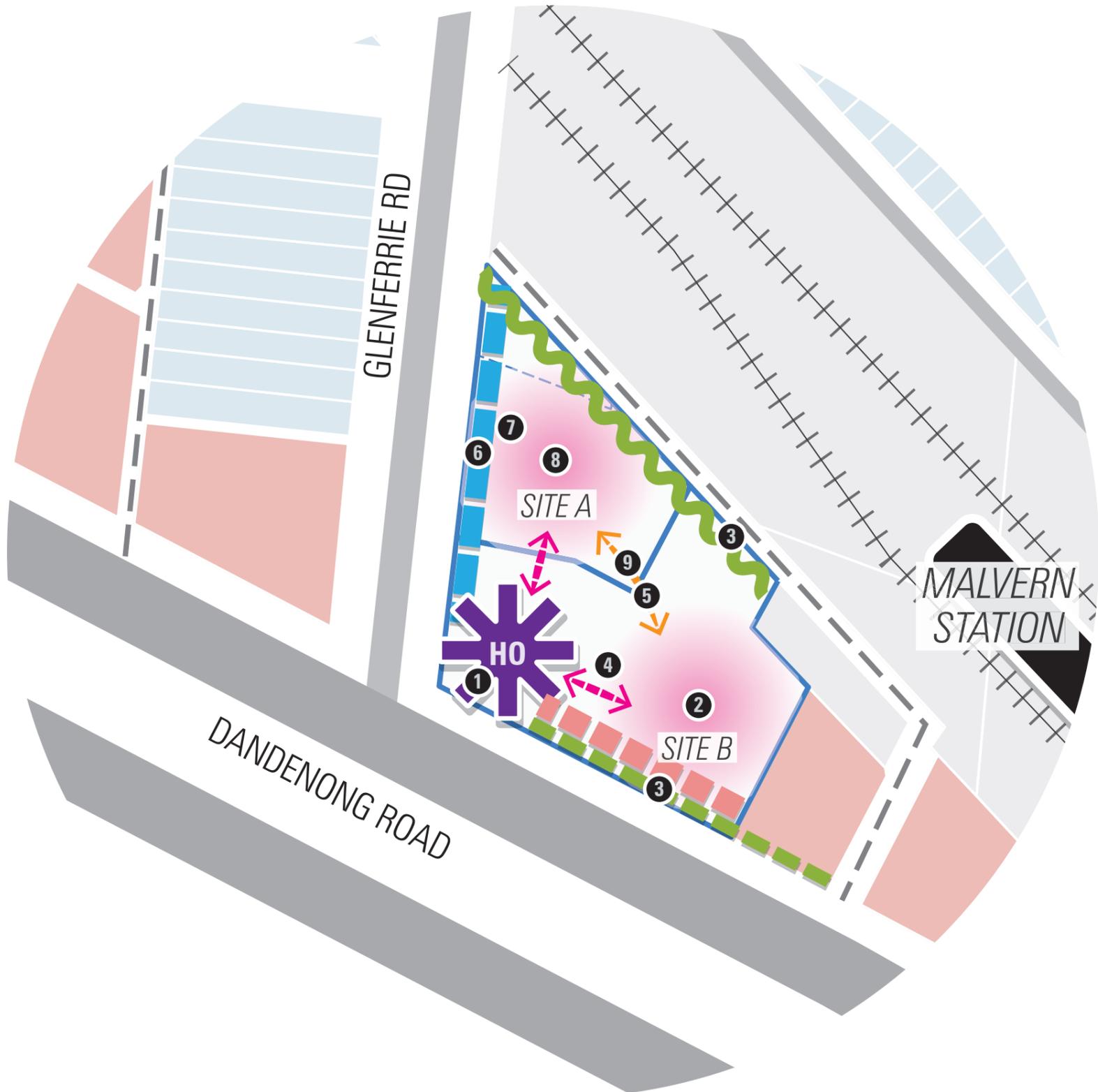
Building scale is required to be confirmed and determined through further site specific investigations and its positive net contribution to its context. Site specific investigations will require the preparation of a detailed urban form and public realm/ urban landscape analysis to determine an appropriate built form scale and public contribution.

Recommendations regarding built form scale for Glenferrie Road Gateway site should clearly demonstrate how proposed built form is moderated by and respects the existing and proposed built form within the surrounding locality. Site specific investigations to inform built form scale must consider:

- Current built form (typology, height datum, grain and aspects) within 500 metres of the site.
- Built form recommendations as outlined within this Structure Plan, with particular reference to the 4 storey scale along Glenferrie Road.

- Built form scale associated with the preferred number of storeys to be guided by 4.0 m at ground level and 3.5 m for upper levels.
- The siting and orientation of a slender vertical projection to be located centrally on the site to maintain prominence of the former Railway Hotel (Heritage Overlay Schedule 403).
- Potential taller form/s on this site should contribute positively to the skyline interest.
- A potential plot-ratio arrangement which balances additional floor space with ground level publicly accessible open space.
- Immediate and longer range views of the site from public vantage points, including: Dandenong Road, Glenferrie Road and Station Street.
- Ensuring potential façade treatments address frontage interfaces and views of the site, including opportunities to activate the northern façade along the pedestrian walkway to improve passive surveillance of the station environs and pedestrian connectivity to Malvern Station.
- Initiatives and objectives associated the Malvern Station Precinct Framework Plan (Figure 18).
- Development equity for abutting sites through sufficient building separation (i.e. to maximise daylight access to lower floors, managing outlooks and external aspects, and privacy matter).
- An exemplary standard of internal amenity and avoiding reliance on privacy screening, as strategic development sites should accommodate amenity matters within its own site.
- Promotion of sustainability measures including: sustainable design, sustainable transport, minimising heat island effect etc.

Dandenong Road and Glenferrie Road Gateway built form diagram



GLENFERRIE ROAD



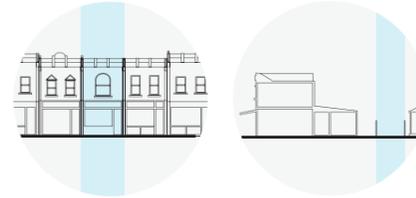
ATTRIBUTES

(s) small sites are sites which have 6m street frontage (width), or less.

Glenferrie Road exhibits the highest level of consistency of small, fine-grained allotments due to its continued economic success since settlement. The spine between the rail cutting to the south and the Civic Precinct is perceived as a continuous set of 2 storey forms with pronounced parapets. As opposed to High Street where a greater degree of infill is anticipated, Glenferrie Road will emphasise the existing streetwall condition with any new built form set behind the parapet. New built form should seek to provide surveillance of rear laneways and incorporate softening of the laneway with tactile materiality and landscape elements where possible. Larger non-contributory forms within the street are to be carefully managed to complement existing built form in scale and vertical rhythm whilst avoiding heritage mimicry.



minimum typical site dimensions for a SMALL allotment on Glenferrie Road



GUIDELINES

- 1 maximum built form height of 4 storeys with 3 storey streetwall based on an appreciation of the established and predominantly street wall/heritage fabric, key views from the public realm within Glenferrie Road and side streets.
- 2 the preferred streetwall height of 3 storey (built to boundary) with top level setback minimum 3 m to ensure reduced visual prominence, whilst accepting a degree of visibility. For an existing building of heritage significance, refer to Clause 22.04 Heritage Policy of the Stonnington Planning Scheme (and associated reference documents) for further guidance.
- 3 primary active frontages be maintained at the ground level of Glenferrie Road with attractive frontages to the rear lane incorporating tactile materiality, signage, lighting and low level softening vegetation in planter box format to provide a secondary address where possible.
- 4 building setbacks should be simplified to provide for good building composition and avoid a 'wedding cake' profile, whilst party walling should be maintained to at least 3 levels to encourage a strong street edge profile.
- 5 a 1.5 m minimum rear lane offset is required at the ground level and first level to facilitate the ongoing function of the laneway with all vehicle access from the rear lane.
- 6 rear building profile should incorporate sufficient setback to achieve the required building separation between building lines and primary outlooks.
- 7 rear elevations should step down mid-block from 4 to 3 storeys where a sensitive interface occurs beyond the ROW (this does not apply to rear non-residential interfaces).
- 8 above the first level, rear elevations should be setback a minimum of 3m from the rear boundary for non-residential interfaces and 4.5 m from the rear boundary for residential interfaces. This is to manage building bulk, avoid a 'wedding cake' profile, and provide sufficient spatial separation to ensure good management of privacy outcomes. Balconies must be contained within their respective setbacks.

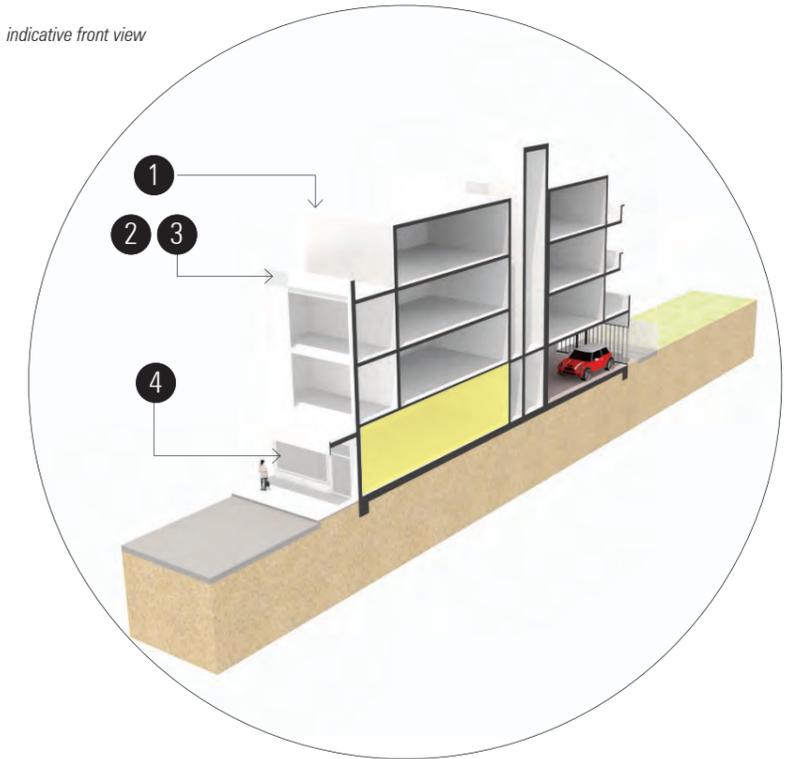


attractive rear interfaces with adequate design effects

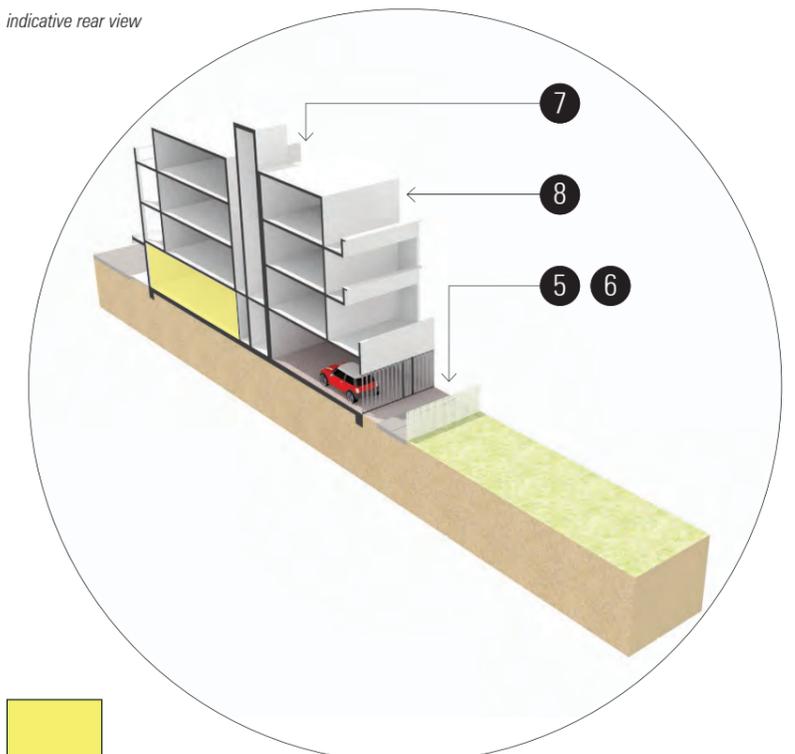


new infill building should reflect the existing parapet line

indicative front view



indicative rear view



encourage active uses at the ground level



GLENFERRIE ROAD

M/L

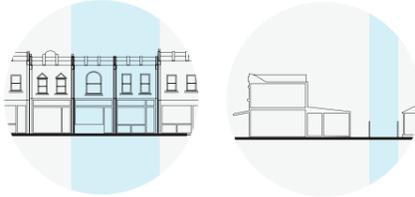
ATTRIBUTES

(m) medium sites are sites which have 6m-18m street frontage (width).
 (l) large sites are sites which have street frontage (width) greater than 18m.

Whilst Glenferrie Road is dominated by fine-grained allotments a handful of heritage replica forms and non-contributory forms are noted which constitute medium or large allotments. These allotments include supermarkets, and mini-malls with arcades, and require positive management in terms of infill opportunity. As opposed to High Street where a departure in character on these larger sites is anticipated, new infill of larger allotments along Glenferrie Road will continue to emphasise the existing streetwall condition with any new built form set behind the parapet. New built form on larger sites should seek to provide a secondary address, with surveillance of rear laneways and incorporate softening of the laneway with tactile materiality and landscape elements where possible. Larger allotments should complement or instate an appropriate sense of vertical rhythm in ground and upper level treatments, whilst heritage mimicry should be avoided.



minimum typical site dimensions for a MEDIUM to LARGE allotment on Glenferrie Road



GUIDELINES

- 1 maximum built form height of 4 storeys with 3 storey streetwall based on an appreciation of the established and predominantly street wall/heritage fabric, key views from the public realm within Glenferrie Road and side streets.
- 2 the preferred streetwall height of 3 storey (built to boundary) with top level setback minimum 3m to ensure reduced visual prominence, whilst accepting a degree of visibility. For an existing building of heritage significance, refer to Clause 22.04 Heritage Policy of the Stonnington Planning Scheme (and associated reference documents) for further guidance.
- 3 primary active frontages be maintained at the ground level of Glenferrie Road with attractive frontages to the rear lane incorporating tactile materiality, signage, lighting and low level softening vegetation in planter box format to provide a secondary address where possible.
- 4 building setbacks should be simplified to provide for good building composition and avoid a 'wedding cake' profile, whilst party walling should be maintained to at least 3 levels to encourage a strong street edge profile.
- 5 a 1.5m minimum rear lane offset is required at the ground level and first level to facilitate the ongoing function of the laneway with all vehicle access from the rear lane.
- 6 rear building profile should incorporate sufficient setback to achieve the required building separation between building lines and primary outlooks.
- 7 rear elevations should step down mid-block from 4 to 3 storeys where a sensitive interface occurs beyond the ROW (this does not apply to rear non-residential interfaces).
- 8 above the first level, rear elevations should be setback a minimum of 3m from the rear boundary for non-residential interfaces and 4.5 m from the rear boundary for residential interfaces. This is to manage building bulk, avoid a 'wedding cake' profile, and provide sufficient spatial separation to ensure good management of privacy outcomes. Balconies must be contained within their respective setbacks.

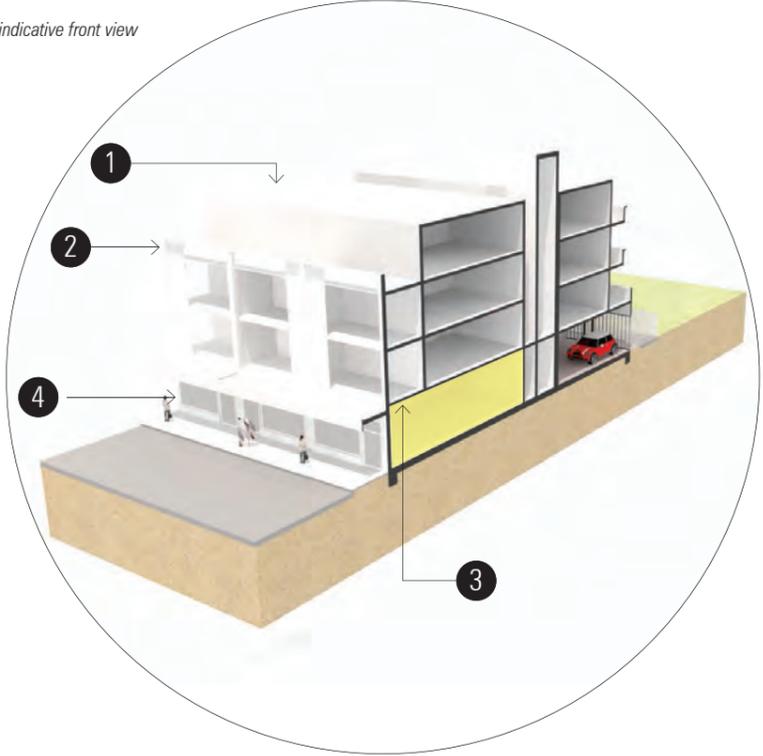


setback new built form beyond heritage fabric

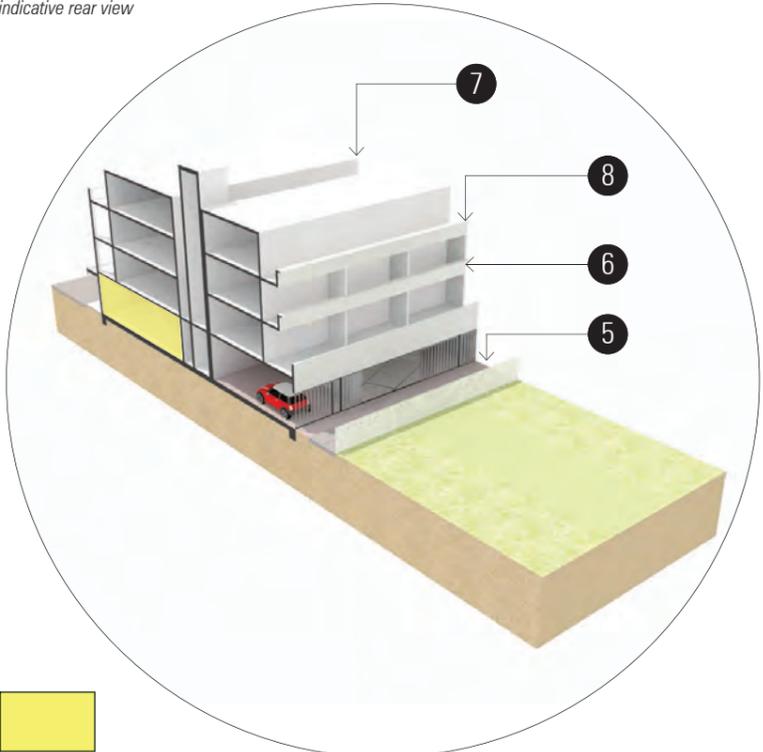


contemporary infill should achieve high standards of design

indicative front view



indicative rear view



encourage active uses at the ground level

HIGH ST



ATTRIBUTES

(s) small sites are sites which have 6m street frontage (width), or less.

High Street has a more varied character, influenced by a gently sloping street profile, varied parapet height and non-contributory buildings interspersed with valued period stock. New buildings should maintain and enhance streetscape diversity, with a combination of retained heritage fabric, and marginally taller contemporary streetwalls. Narrow and moderate sites can accommodate 4 levels. Where a sensitive residential interface exists to the rear, the building should step down in height by 1 level toward the rear boundary. New buildings should maintain the sense of openness to the sky through lightweight or retractable canopy treatments. All new apartments should be oriented with adequate spatial separation from adjoining development sites in accordance with equitable development principles. Apartments oriented with primary aspect to side boundaries should be discouraged.



minimum typical site dimensions for a SMALL allotment on High Street



GUIDELINES

- 1 maximum built form height of 4 storeys with 3 storey streetwall based on an appreciation of the established and predominantly street wall/ heritage fabric, key views from the public realm within High Street and side streets.
- 2 the preferred setback above the streetwall is 3 m to ensure reduced visual prominence, whilst accepting a degree of visibility. For an existing building of heritage significance, refer to Clause 22.04 Heritage Policy of the Stonnington Planning Scheme (and associated reference documents) for further guidance.
- 3 building setbacks should be simplified to provide for good building composition and avoid a 'wedding cake' profile, whilst party walling should be maintained to at least 3 levels to encourage a strong street edge profile.
- 4 primary active frontages be maintained at the ground level of High Street with attractive frontages to the rear lane incorporating tactile materiality, signage, lighting and low level softening vegetation in planter box format to provide a secondary address.
- 5 a 1.5 m minimum rear lane offset is required at the ground level and first level to facilitate the ongoing function of the laneway with all vehicle access from the rear lane.
- 6 rear building profile should incorporate sufficient setback to achieve the required building separation between building lines and primary outlooks.
- 7 rear elevations should step down mid-block from 4 to 3 storeys where a sensitive interface occurs beyond the ROW (this does not apply to rear non-residential interfaces).
- 8 above the first level, rear elevations should be setback a minimum of 3.0 m from the rear boundary for non-residential interfaces and 4.5 m from the rear boundary for residential interfaces. This is to manage building bulk, avoid a 'wedding cake' profile, and provide sufficient spatial separation to ensure good management of privacy outcomes. Balconies must be contained within their respective setbacks.

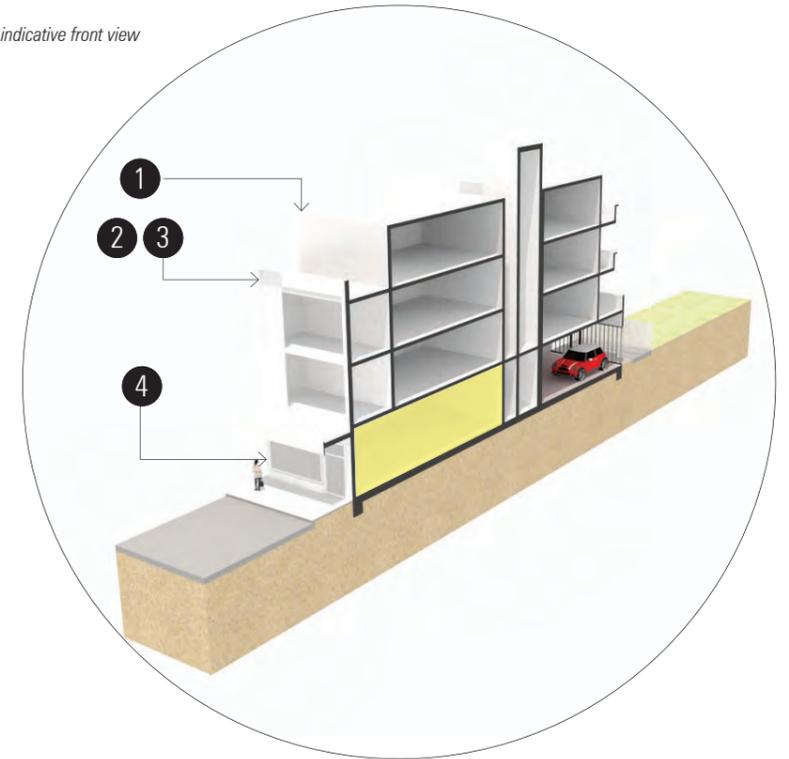


high quality architecture is permitted to project above the parapet line by 1 level in height

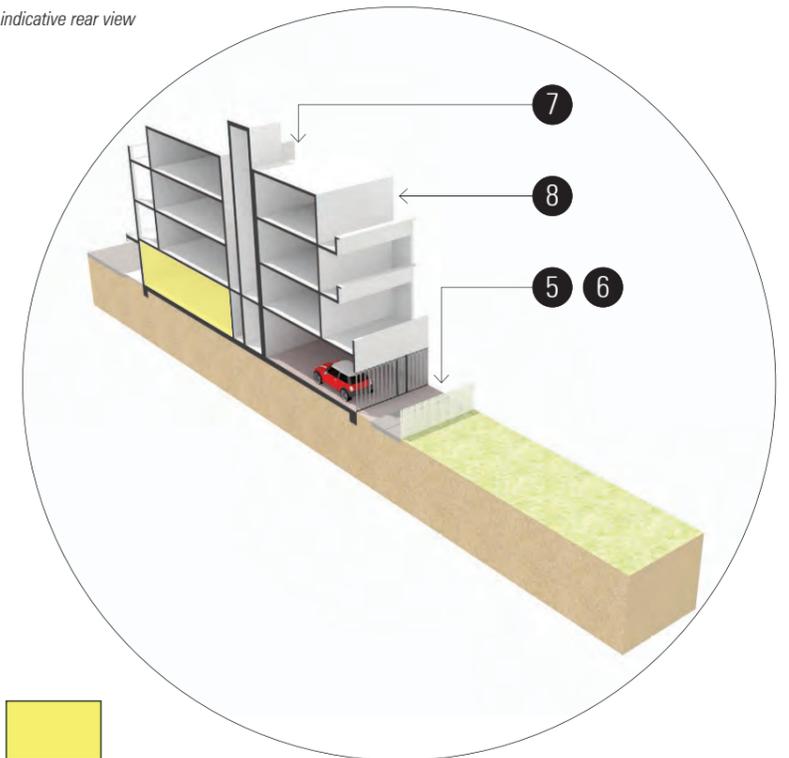


encourage party-walls with apartments oriented front and back wherever possible

indicative front view



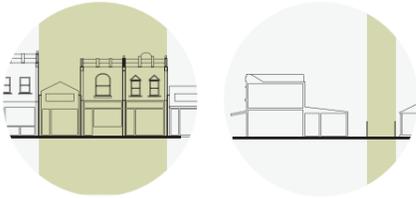
indicative rear view



encourage active uses at the ground level

HIGH ST

M/L



ATTRIBUTES

(m) medium sites are sites which have 6m-18m street frontage (width).
 (l) large sites are sites which have street frontage (width) greater than 18m.

High Street has a more varied character, influenced by a gently sloping street profile, varied parapet height and non-contributory buildings intermingled with valued period stock. New built form on larger allotments should maintain and enhance the streetscape diversity, with a combination of retained heritage fabric, and marginally taller contemporary streetwalls. Larger allotments can accommodate a smaller 5th level envelope subject to the achievement of a high design quality. Where a sensitive residential interface exists to the rear, the building should step down in height by 1 level toward the rear boundary. New buildings should maintain the sense of openness to the sky through lightweight or retractable canopy treatments. All new apartments should be oriented with adequate spatial separation from adjoining development sites in accordance with equitable development principles. Apartments with primary aspect to side boundaries may be possible on larger allotments, so long as equitable development principles are adhered to.



minimum typical site dimensions for a MEDIUM to LARGE allotment on High Street

GUIDELINES

- 1 new building forms to incorporate existing heritage fabric where possible, with maximum built form height of up to 5 storey with 3 storey streetwall to maintain key views from the public realm within High Street and side streets. A 5th level element may be warranted if a high quality design and internal amenity standard is achieved on medium to large allotments.
- 2 preferred setback above the streetwall of 3m to ensure reduced visual prominence, whilst accepting a degree of visibility. For an existing building of heritage significance, refer to Clause 22.04 Heritage Policy of the Stonnington Planning Scheme (and associated referenced documents) for further guidance.
- 3 building setbacks should be simplified to provide for good building composition and avoid a 'wedding cake' profile, whilst party walling should be maintained to at least 3 levels to encourage a strong street edge profile.
- 4 allow variation in the streetwall and make provision for a parapet height discrepancy of 1 level to achieve positive diversity, except where neighbouring heritage built form necessitates a site specific design response (refer to Clause 22.04 Heritage Policy of the Stonnington Planning Scheme (and associated reference documents) for further guidance.)
- 5 primary active frontages be maintained at the ground level of High Street with attractive frontages to the rear lane incorporating tactile materiality, signage, lighting and low level softening vegetation in planter box format to provide a secondary address.
- 6 a 1.5m minimum rear lane offset is required at the ground level and first level to facilitate the ongoing function of the laneway with all vehicle access from the rear lane.
- 7 rear building profile should incorporate sufficient setback to achieve the required building separation between building lines and primary outlooks.
- 8 rear elevations should step down mid-block 1 level in height where a sensitive interface occurs beyond the ROW (this does not apply to rear non-residential interfaces).
- 9 above the first level, rear elevations should be setback a minimum of 3.0 m from the rear boundary for non-residential interfaces and 4.5 m from the rear boundary for residential interfaces. This is to manage building bulk, avoid a 'wedding cake' profile, and provide sufficient spatial separation to ensure good management of privacy outcomes. Balconies must be contained within their respective setbacks.

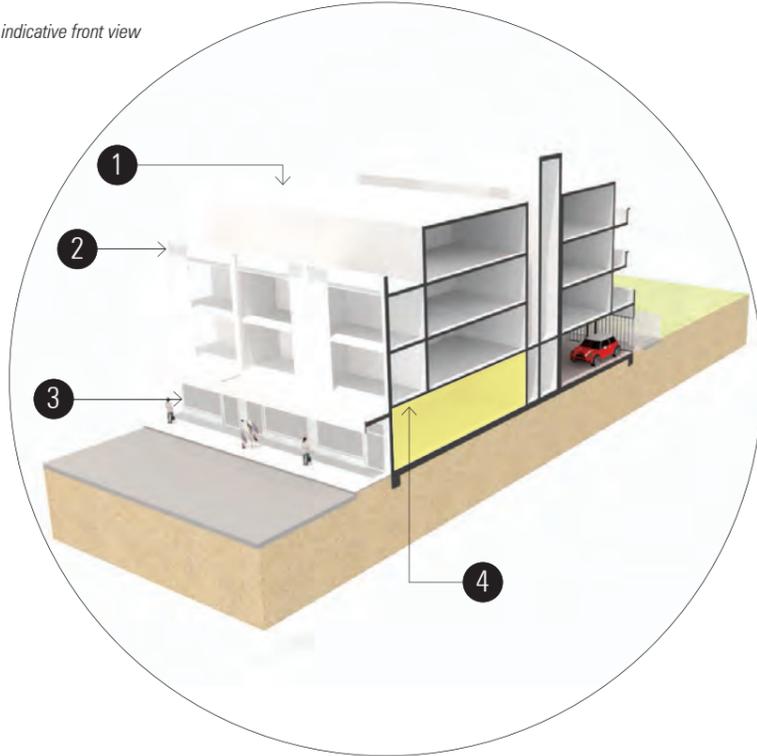


step the parapet form in height to add diversity

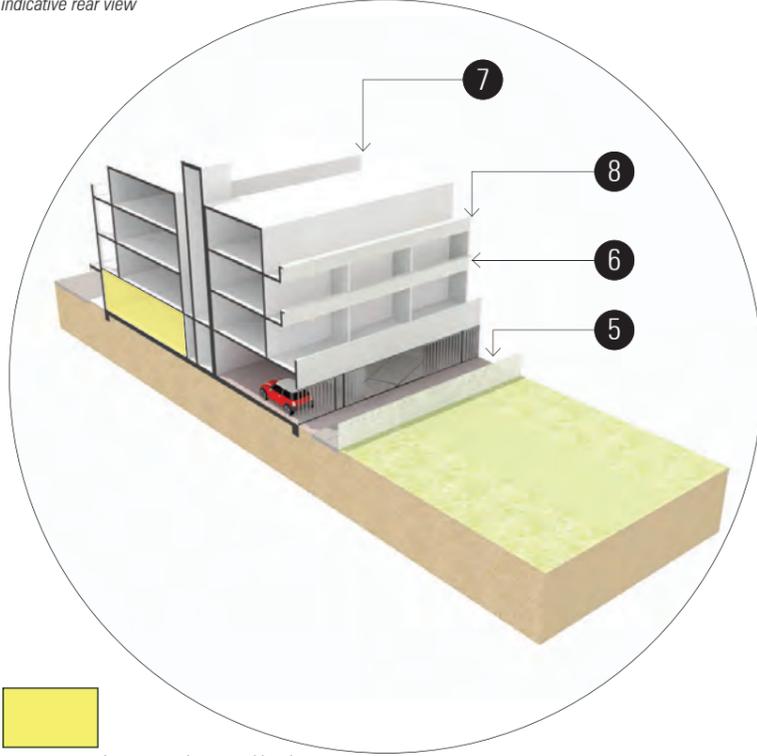


provide surveillance and a sense of address to laneways

indicative front view



indicative rear view



encourage active uses at the ground level

WATTLETREE WEST M/L

ATTRIBUTES

Applicable on all allotments situated along Wattletree Road, on the west side of Glenferrie Road.

An eclectic precinct dominated by the Art Deco Clocktower (on Wattletree Road) and the only street frontage of Malvern Central, this precinct comprises a mix of period dwellings, contemporary infill and commercial built form consistently serviced by a ROW to the rear. Notably the majority of dwellings remaining in the precinct are in use as offices or medical facilities, some in association with Cabrini. New built form in the precinct should seek to strengthen the street edge in a distinctive way given the lack of existing parapet datum, with taller streetwalls and party-wall construction, with buildings accommodating medical and office facilities in purpose built spaces, whilst maintaining adequate streetscape activation with foyer or common areas. New buildings of an urban profile should consider their temporary exposure over lower scale built form with adequate treatment of exposed walling.



minimum typical site dimensions for a MEDIUM to LARGE allotment on Wattletree Road West



GUIDELINES

- 1 building forms to adopt a 4 storey infill streetwall to promote surveillance as well as a 3m minimum setback to accommodate shrubs and small canopy trees within the frontage to contribute to the urban forest. A 5th level possible on larger sites subject to the achievement of high design and internal amenity standards.
- 2 facilitate a combination of landscaping and active frontage, including weather protection at entrance points along Wattletree Road.
- 3 all vehicle access or secure surface parking to be maintained off rear lane where possible, except on larger allotments where parking entry from Wattletree Road is necessitated for legibility purposes for patients or visitors to medical premises.
- 4 a 1.5 m minimum rear lane offset is required at the ground level and first level to facilitate the ongoing function of the laneway with all vehicle access from the rear lane.
- 5 above the first level, rear elevations should be setback a minimum of 3.0 m from the rear boundary for non-residential interfaces and 4.5 m from the rear boundary for residential interfaces. This is to manage building bulk, avoid a 'wedding cake' profile, and provide sufficient spatial separation to ensure good management of privacy outcomes. Balconies must be contained within their respective setbacks.
- 6 ensure that high levels of surveillance are achieved from the design of upper level apartments / tenancies to enhance the Wattletree Road street environment.

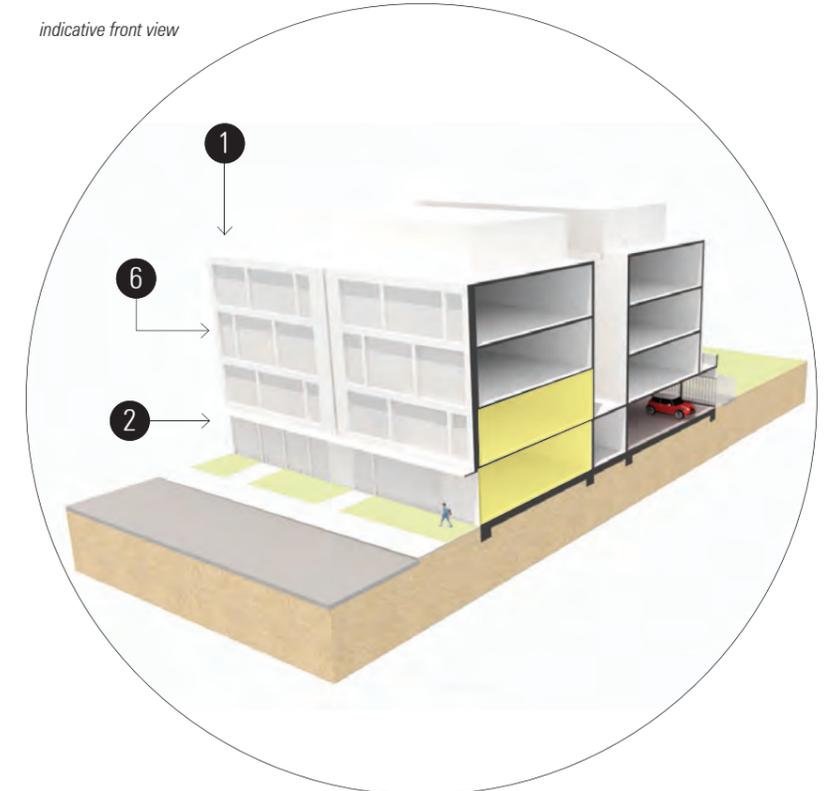


promote a taller streetwall to maximise surveillance

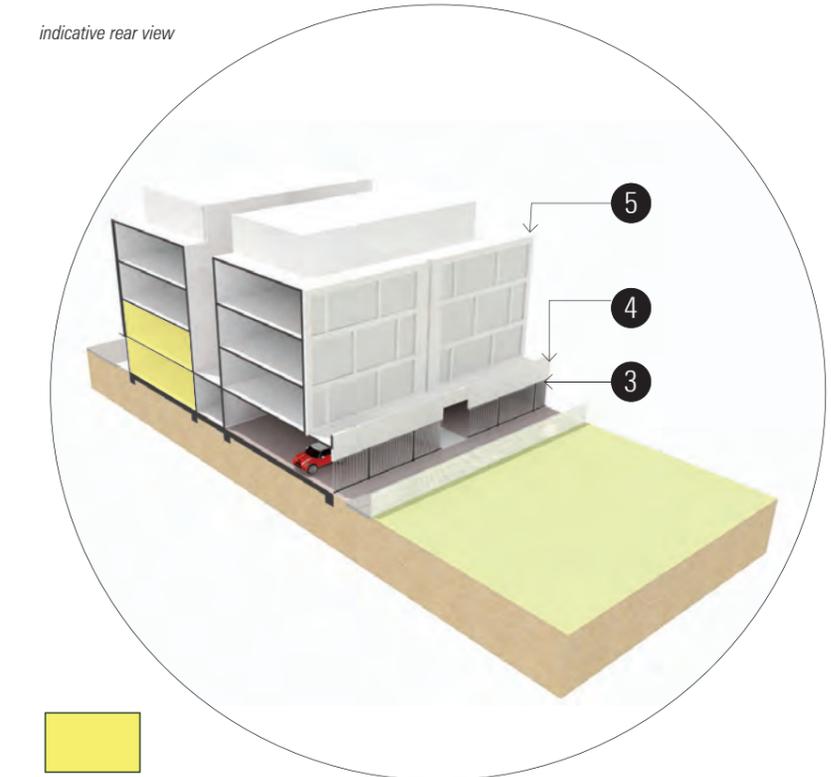


achieve a strong urban profile in new buildings

indicative front view



indicative rear view



encourage flexible street-oriented tenancies to enable a range of uses

WATTLETREE EAST M/L

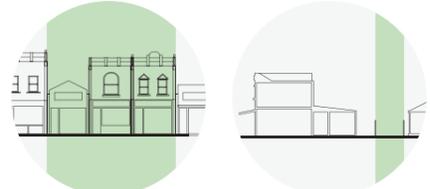
ATTRIBUTES

Applicable on all allotments situated along Wattletree Road, on the east side of Glenferrie Road.

Wattletree East is distinctive from Wattletree West due to the dominant residential character and landscape quality within the Residential Growth Zone. Whilst some infill medical built form is noted, the primary stock is a combination of semi-detached dwellings in a garden and some multi-unit development. New built form in the precinct should seek to maintain the garden setting with more intensive built form sited off at least one side boundary with a garden lane effect, whilst small front and rear setbacks are to be maintained to accommodate small tree planting. Buildings will provide for adaptable ground levels which can accommodate either medical or office facilities, or home occupation components of maisonette dwellings.



minimum typical site dimensions for a MEDIUM to LARGE allotment on Wattletree Road East



GUIDELINES

- 1 building forms to adopt a 3 storey street elevation profile positioned with a 3m minimum setback to accommodate shrubs and small canopy trees within the frontage to contribute to the urban forest.
- 2 a recessed 4th level to be treated as a 'roof form' element and distinguished in materiality.
- 3 provision of a setback to one side boundary of at least 2.5m to create a garden lane effect and streetscape rhythm.
- 4 a rear setback should be provided of sufficient dimensions to enable the planting of at least one medium canopy tree abutting the rear lane
- 5 1.5m minimum rear lane offset is required to facilitate the ongoing function of the laneway with all vehicle access or secure surface parking accessed from the rear lane except where sites with a broad frontage (>18m) permit side access for legibility of medical facilities.
- 6 above ground level, rear elevations should be setback a minimum of 4.5m from the rear boundary to manage building bulk, avoid a wedding cake profile, and provide sufficient spatial separation to ensure good management of privacy outcomes.
- 7 activate ground level frontages through medical uses, or provide planter box buffers to ground level residences.

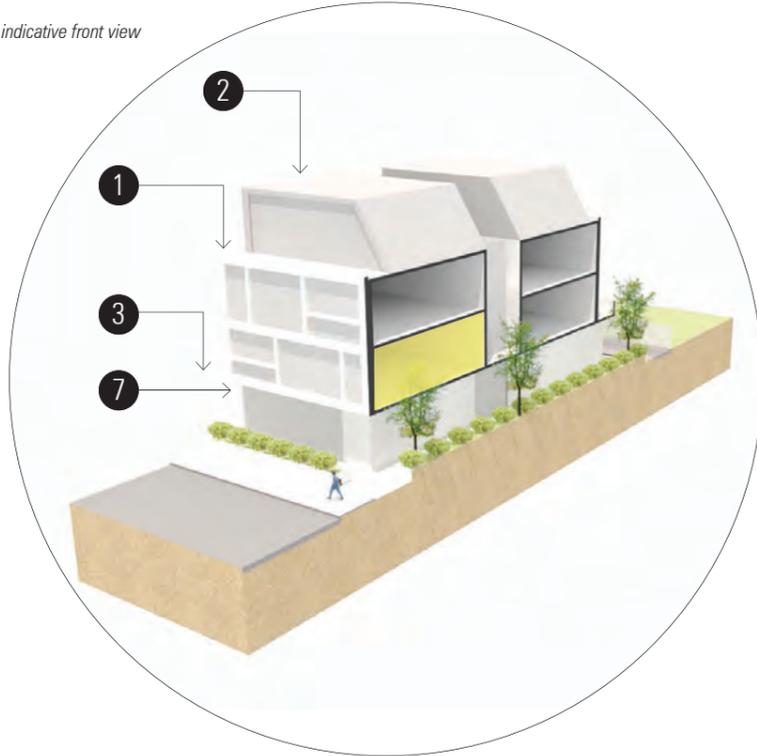


promote front setback with landscape treatments

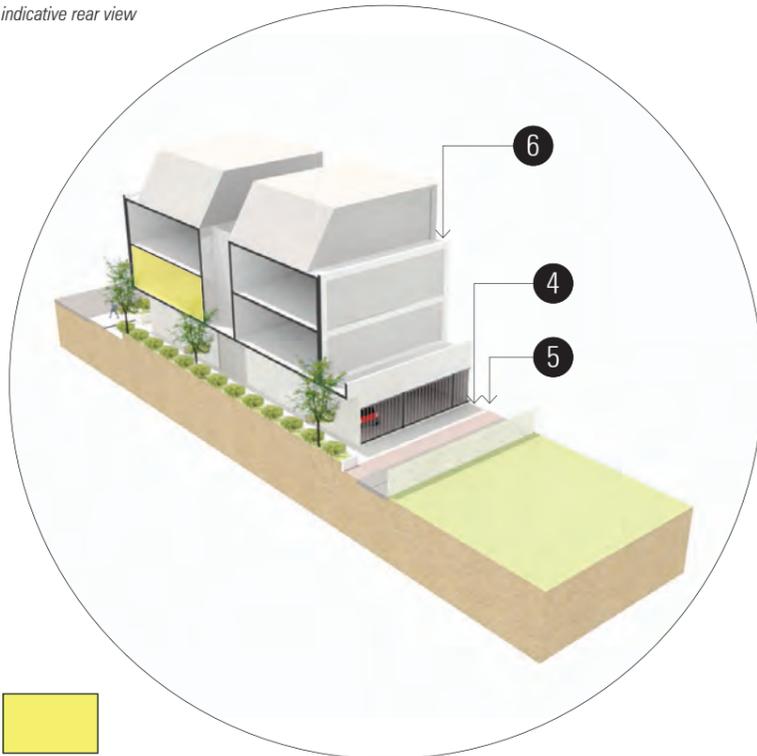


encourage recessed upper storey forms

indicative front view



indicative rear view



encourage flexible street-oriented tenancies to enable a range of uses

7 Neighbourhood Framework Plans

Precinct 1: Civic Centre



Figure 11 aerial context view



Malvern library



Terminal view along glenferrie road to malvern town hall



Malvern square adjacent to Giorgio's

Key Site Features

- Administrative and civic centre of the City of Stonnington;
- Existing function and image as the northern entry to the centre from the broad grain residential character of Glenferrie Road to the north;
- Significant heritage asset in Malvern Town Hall;
- Significant source of trips and custom in the school and other institutional buildings;
- Community orientated uses, including library, police station and public open space at Malvern Cricket Ground;
- Poor connectivity and image of back of house areas around the Malvern Cricket Ground; and
- Existing but poorly utilised public square at Malvern Square to the north western corner of Glenferrie Road and High Street junction.

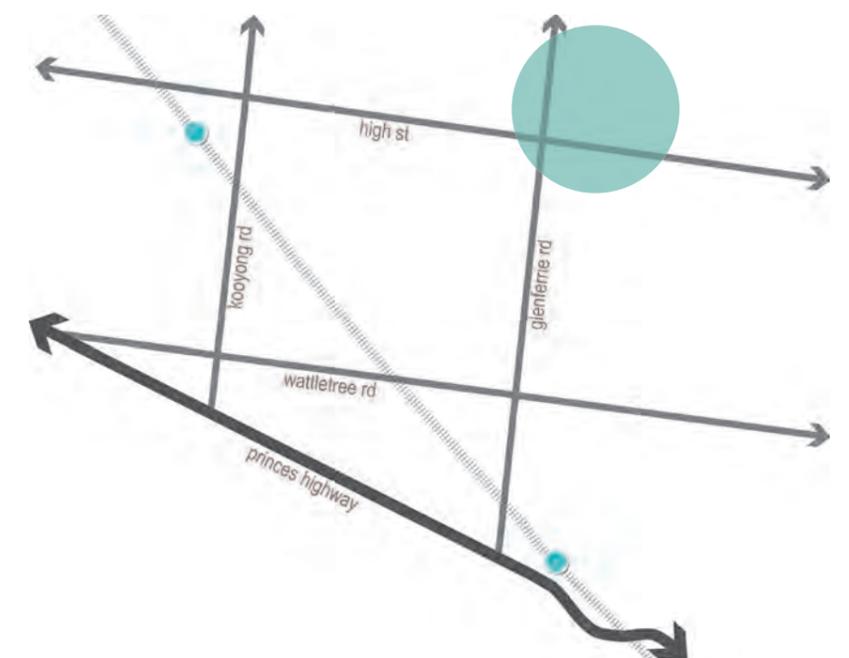
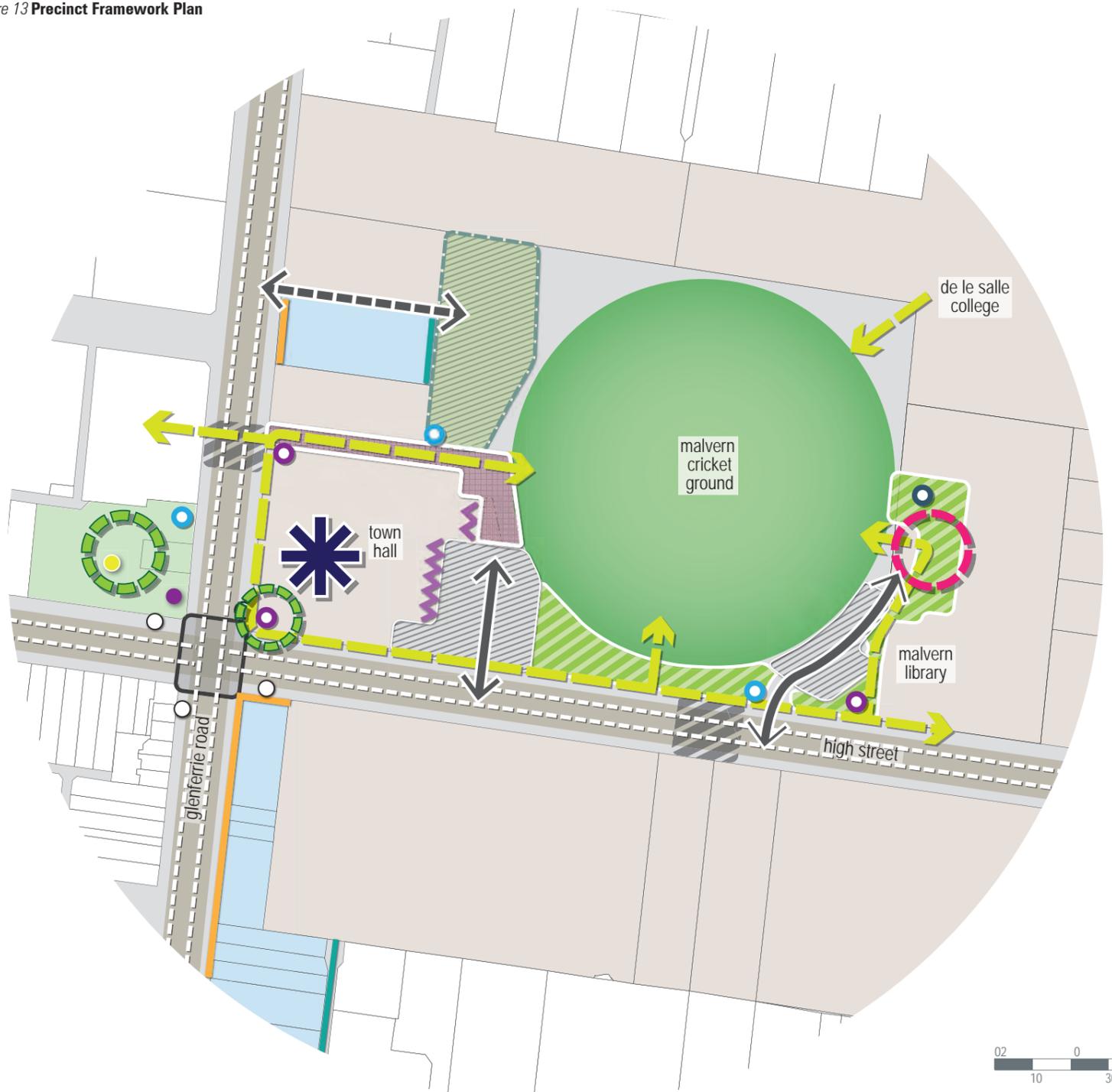


Figure 12 Precinct Identification

Figure 13 Precinct Framework Plan



Vision

The Civic Precinct will retain and strengthen its role as the central focus for municipal, community, educational and recreational activities in Stonnington. It will be a major destination attracting visitors from within and outside the municipality. As an integrated community and civic precinct set within an attractive landscape setting and supported by high quality public open space, the precinct will present itself as inviting to the local community, employees, customers and visitors alike.

Objectives

- to retain the primary focus as the municipal’s centre for civic services, education and recreation. Accommodate current and future requirements for Council and Community services and facilities in a manner which provides easy and direct access for the community;
- to recognise the existing Town Hall and Clocktower as a municipal landmark and to retain its role as a prominent built form in the Centre;
- to protect key vistas to Malvern Town Hall;
- to encourage future development around Malvern Cricket Ground to activate its interface and establish a safe and attractive environment;
- to encourage activities and linkages within the precinct and establish to be more closely integrated with the Town Hall, Malvern Square and Malvern Cricket Ground;
- to encourage activities and develop the existing Malvern Square as a premier public gathering space in the Centre;
- encourage the tactful siting of new public facilities within underutilised land between existing buildings reinforce the public realm and movement corridors;
- to strengthen the ‘leafy’ street character along the eastern side of High Street.

legend

main street	potential future vehicular access	glenferrie road interface	existing public art	street furniture hub
tram route	enhanced pedestrian connection	residential/ laneway interface	investigate landscape enhancement	way-finding/ public art
tram stop	preferred maximum building height 4 storey	improve interface presentation	potential pedestrian space	bike facility
existing signalised junction	building height subject to site specific investigation	existing landscaping	proposed shared path	
facillitate pedestrian crossing movements	existing car park	existing public open space	existing public space	
retain & improve vehicular access	existing landmark	existing public toilet	potential future activity node	

Precinct 2: Armadale Station



Figure 14 aerial context view



Internal view of kings arcade



The elaborate facade of kings arcade



Single storey period built form adjacent to the railway line

Key Site Features

- entry to the Centre from the west along High Street;
- proximity to rail, tram and bus services with a multi-modal opportunity;
- established boutique retail hub around Armadale Railway Station;
- existing Council surface car park off Morey Street;
- pedestrian access between Armadale Station and High Street via Kingsway Arcade; and
- characterised by back of house presentation and informal intimate streetscape setting.

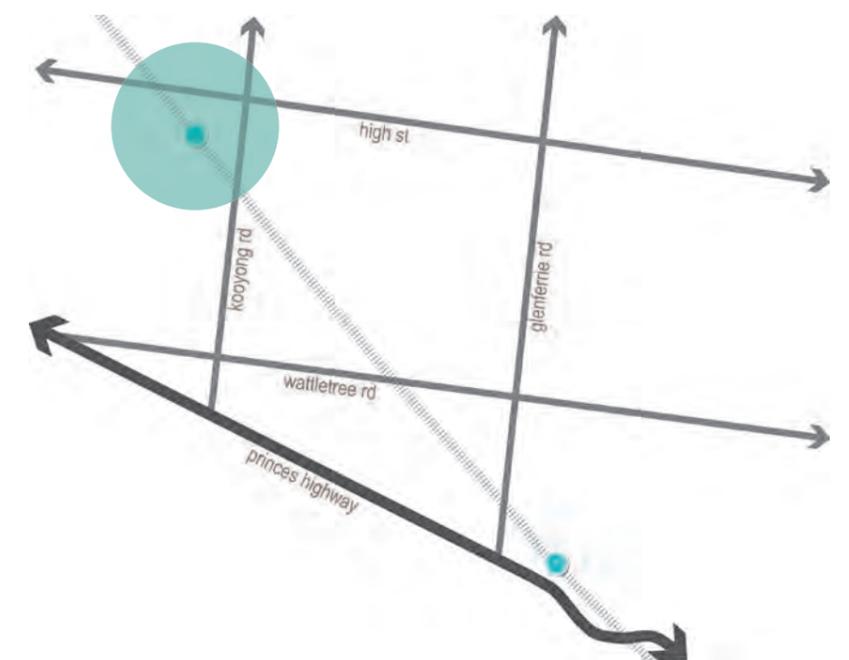


Figure 15 Precinct Identification

Figure 16 Precinct Framework Plan



Vision

Armadale Station Precinct will strengthen its role as a key attractor within the Centre and point of arrival. It will be experienced as a hidden 'gem' offering boutique retail experience complemented by high quality pedestrian realm. The Kingsway Arcade will retain its role to provide intimate connection to High Street and Kooyong Road with improved multi-modal linkages.

Objectives

- Enhance the legibility and image of the precinct as the western entry into the Centre;
- Bolster the identity of this precinct as a unique destination associated with Armadale Station and Kingsway Arcade;
- Establish high pedestrian amenity and gathering space around Armadale Station to facilitate safe access and to accommodate safe pedestrian refuge;
- Strengthen connections through the precinct to enhance the function of multi-modal links, connecting bus, train and tram services through public realm improvements;
- Facilitate north – south pedestrian movement through improved connections to and from the heritage Armadale Station railway bridge; and
- Carefully manage any new vehicle crossovers or building services from Morey Street to prioritise a pleasant pedestrian environment.

legend			
armadale railway station	tram stop	preferred maximum building height 4 storey	potential landscape enhancement
railway line (pakenham, cranbourne, frankston lines)	bus stop	preferred maximum building height 5 storey	proposed shared path
bridge	retain vehicular access	building height subject to site specific investigation	way-finding/ public art
main street	existing signalised junction	high street interface	way-finding/ public art
tram route	enhanced pedestrian connection	residential/ laneway interface	bike facility
bus route	potential future activity node	existing landscaping	

Precinct 3: Malvern Station



Figure 17 aerial context view



view down claremont avenue from station street



view from heritage rail bridge at malvern station



public realm along station street with median planting

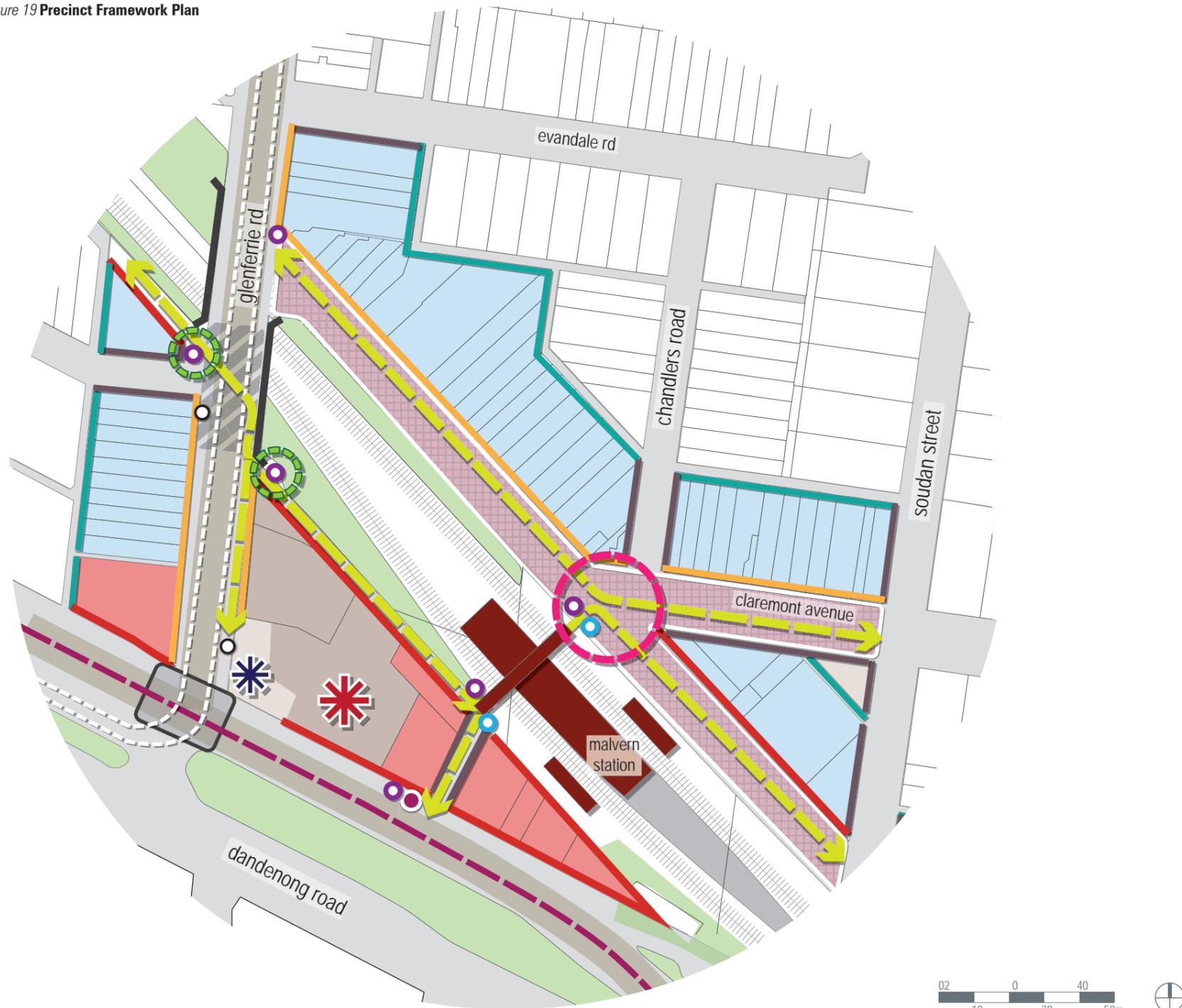
Key Site Features

- Historic role as an entry to the Centre from Dandenong Road to the south;
- Distinctive high-quality streetscape along Claremont Avenue;
- Existence of higher built form along Station Street to the east of Malvern Station;
- Significant heritage station as a focal point of the precinct;
- Proximity to rail, tram and bus services;
- Prominent highway exposure to the south and more sensitive residential frontage to the north;
- underutilised public space and pedestrian access; and
- Degraded and poorly surveilled public realm south of the railway line and low quality street environment along Dandenong Road.



Figure 18 Precinct Identification

Figure 19 Precinct Framework Plan



Vision

This precinct will redefine the southern approach into the Activity Centre from Dandenong Road through its built form definition at the south-eastern corner of Glenferrie Road. It will promote the establishment of a commercial hub with capacity to accommodate higher density residential for properties fronting Dandenong Road. To the north of the railway line, a mixture of uses will remain in this area, with complementary retail and creative industries retained and expanded on. Pedestrian permeability will be enhanced with a better east – west connection across Glenferrie Road, as well as north- south connection across the railway line via Malvern Station. A new-shared space around Malvern Station seeks to establish a consistent pedestrian friendly environment connecting Glenferrie Road to the Station.

Objectives

- Enhance the image of the precinct as the southern of the Centre and maintain its role in supporting the highest intensity of commercial and mixed use activity;
 - Establish a strong sense of arrival into the Centre from Dandenong Road with the south eastern corner providing a strategic redevelopment site ;
 - Retain individually significant heritage buildings and integrate intensive infill development where possible;
 - Encourage active ground floor uses with potential pedestrian access along the southern side of the railway reserve;
 - Clearly define desired pedestrian line of movement for pedestrian and cyclists between tram stops, railway station and key destinations, including Malvern Central and Cabrini Hospital;
 - Maximise connections to the north – south pedestrian connection through the Railway Station;
 - Establish high pedestrian amenity and a publicly accessible meeting/ gathering space as part of an 'activity node' to the north of Malvern Station to connect to the Station Street and Claremont Avenue; and
 - Direct higher intensity built form to the south along Dandenong Road with a scale transition to more modest forms to the north, responding to the existing residential interface; and
1. Support the establishment of a 'creative' hub centred around Claremont Street and Station Street, including potential street market or special events.

Legend

malvern railway station	bus stop	strategic redevelopment site	local street interface
railway line (pakenham, cranbourne, frankston lines)	existing signalised junction	building height subject to site specific investigation	residential/ laneway interface
bridge	proposed pedestrian crossing	strategic redevelopment site	existing public space
tram route	enhanced pedestrian connection	existing landmark	potential future activity node
bus route	preferred maximum building height 4 storey	highway and railway reserve interface	existing landscaping
tram stop	preferred maximum building height 6 storey	glenferrie road interface	proposed shared path

malvern railway station	bus stop
railway line (pakenham, cranbourne, frankston lines)	existing signalised junction
bridge	proposed pedestrian crossing
tram route	enhanced pedestrian connection
bus route	preferred maximum building height 4 storey
tram stop	preferred maximum building height 6 storey

strategic redevelopment site	building height subject to site specific investigation
strategic redevelopment site	existing landmark
highway and railway reserve interface	glenferrie road interface

local street interface	residential/ laneway interface
existing public space	potential future activity node
existing landscaping	proposed shared path



way-finding/ public art	bike facility
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Precinct 4: Malvern Central



Figure 20 aerial context view



Landmark art Deco clocktower on wattletree road



Positive street integration of malvern central at wattletree road



Laneway interface between malvern central and glenferrie road shops

Key Site Features

- Significant destination within the Centre, with Malvern Central providing a significant retail offering;
- Strong association with Cabrini Hospital to the east and the Wattletree Road spine;
- Poor integration, connectivity and legibility between the precinct and Malvern Station;
- Broad underutilised public depot land adjacent to the rail alignment;
- Car dominated environment due to the arrangement of ramping vehicle circulation into Malvern Central; and
- Mixed built form image along Wattletree Road, clearly distinct from the consistent character of Glenferrie Road.

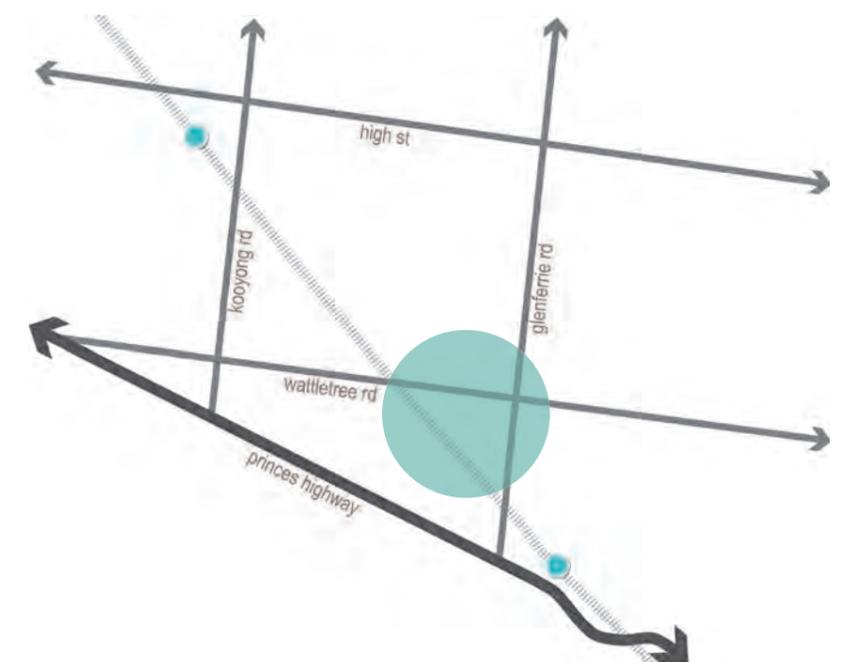
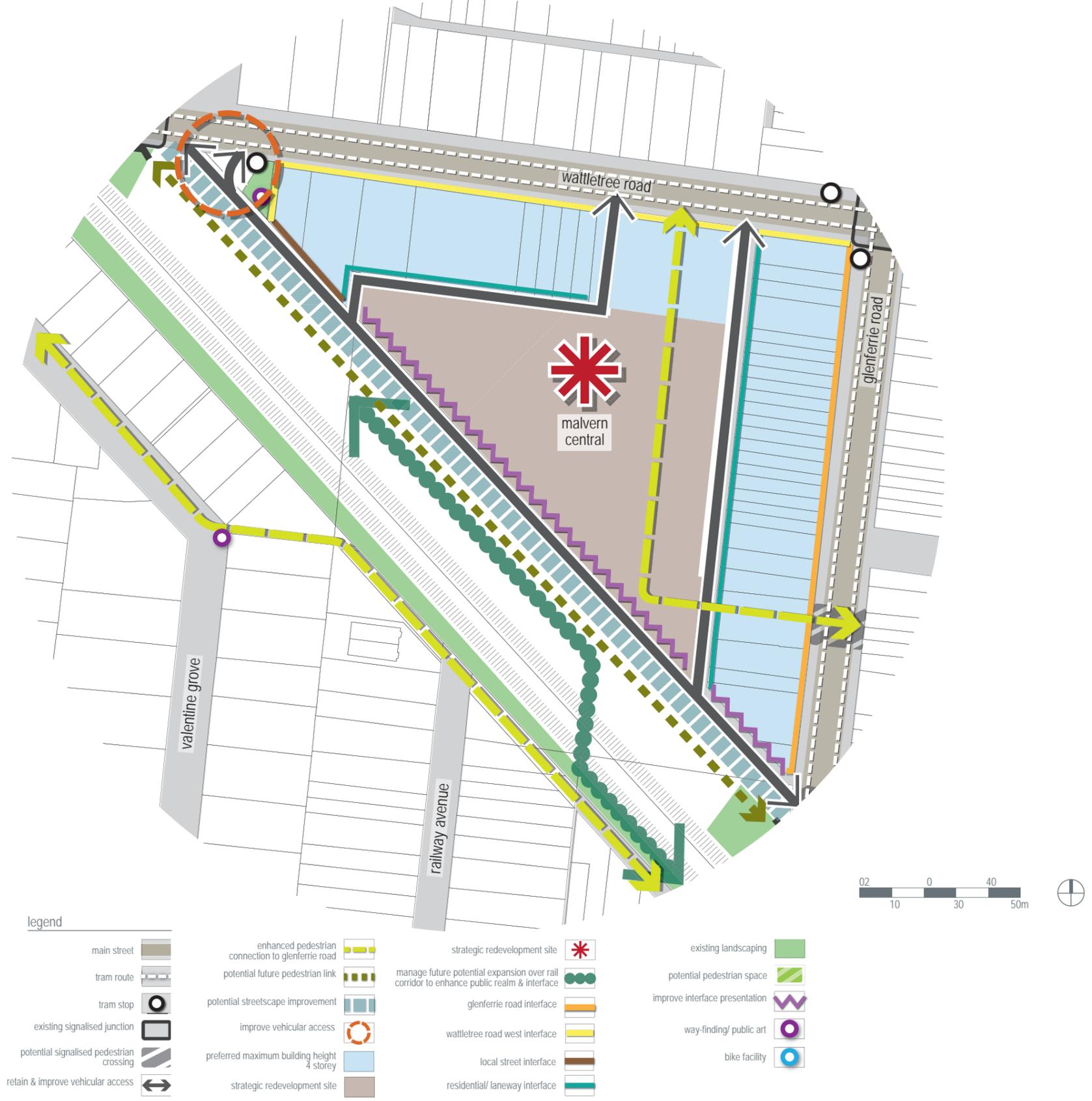


Figure 21 Precinct Identification

Figure 22 Precinct Framework Plan



Vision

The Malvern Central precinct will strengthen its role as the Centre’s regional shopping destination. Improved connections will be established to surrounding areas, including to Glenferrie Road and Malvern Station. This will be achieved through way-finding and public realm upgrades to enhance its legibility within the broader activity centre. Future redevelopment of this precinct will accommodate a higher intensity of residential accommodation in a mixed- use format, which will consider the heritage significance of period commercial forms fronting Glenferrie Road.

Objectives

- to achieve a high quality mixed use precinct including retail, office, residential and entertainment to establish a vibrant living and working environment;
- to enhance ground floor retail and pedestrian generated activities along Wattletree and Glenferrie Roads;
- manage positive future expansion of Malvern Central within a vertical footprint;
- explore opportunity for new connections between Malvern Central and Malvern Station via Glenferrie Road with integral public realm and landscape treatment;
- increase the intensity of built form within Wattletree West in response to the reduced heritage values;
- explore opportunities for a shared pedestrian and cyclist path along the northern edge of rail alignment generally between Malvern and Armadale Station framed by new built form to provide surveillance;
- explore opportunities for landscape works along the rail corridor to improve the image of the public works depot;
- to rationalise car park entry into Malvern Central to facilitate safer pedestrian movement; and
- to improve visual presentation of Malvern Central’s southern elevation.
- to ensure potential development over the railway corridor considers the community benefit and opportunity for public open space.

Precinct 5: Coldblo Tram Depot

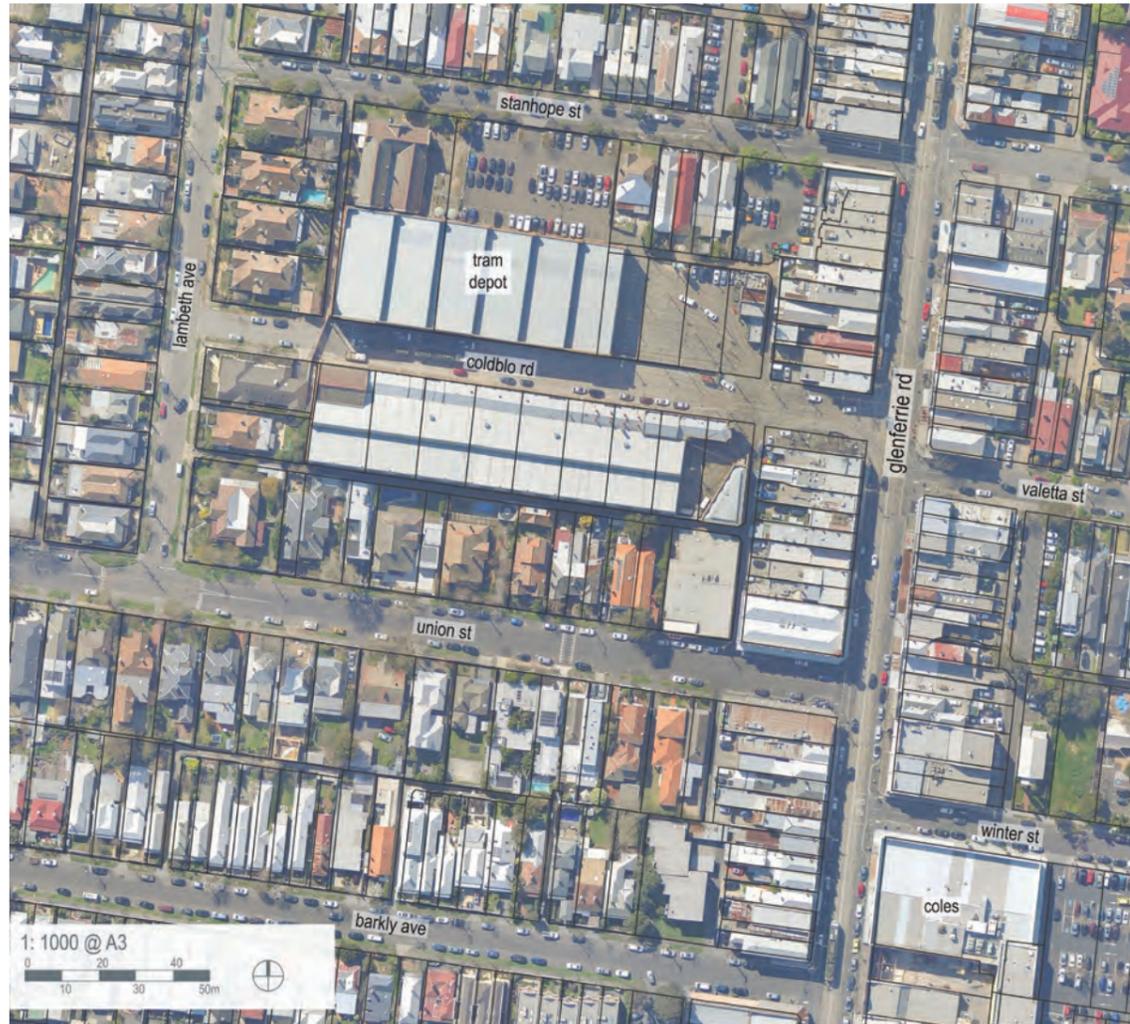


Figure 23 aerial context view



View along stanhope street to glenferrie road



View into coldblo tram depot from the entry



View along coldblo street from the residential hinterland to the west

Key Site Features

- Significant cultural and architectural heritage asset in the historical development of the Centre, with accompanying but underappreciated Tramway Museum on Stanhope Street;
- Ongoing function as a public infrastructure depot with complex vehicle and tram movement patterns;
- Strong connection and potential exposure to the Glenferrie Road spine;
- Limited public permeability east-west through the urban block;
- Centrally located along the Glenferrie Road spine, surrounded by traditional residential dwellings to the north, west and south; and
- Expansive car park presentation along Stanhope Street.

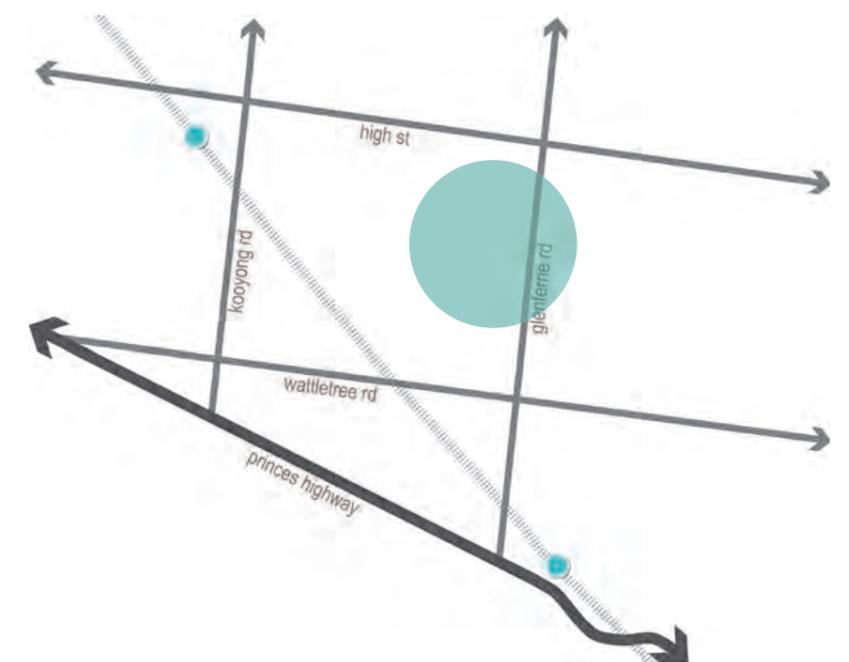
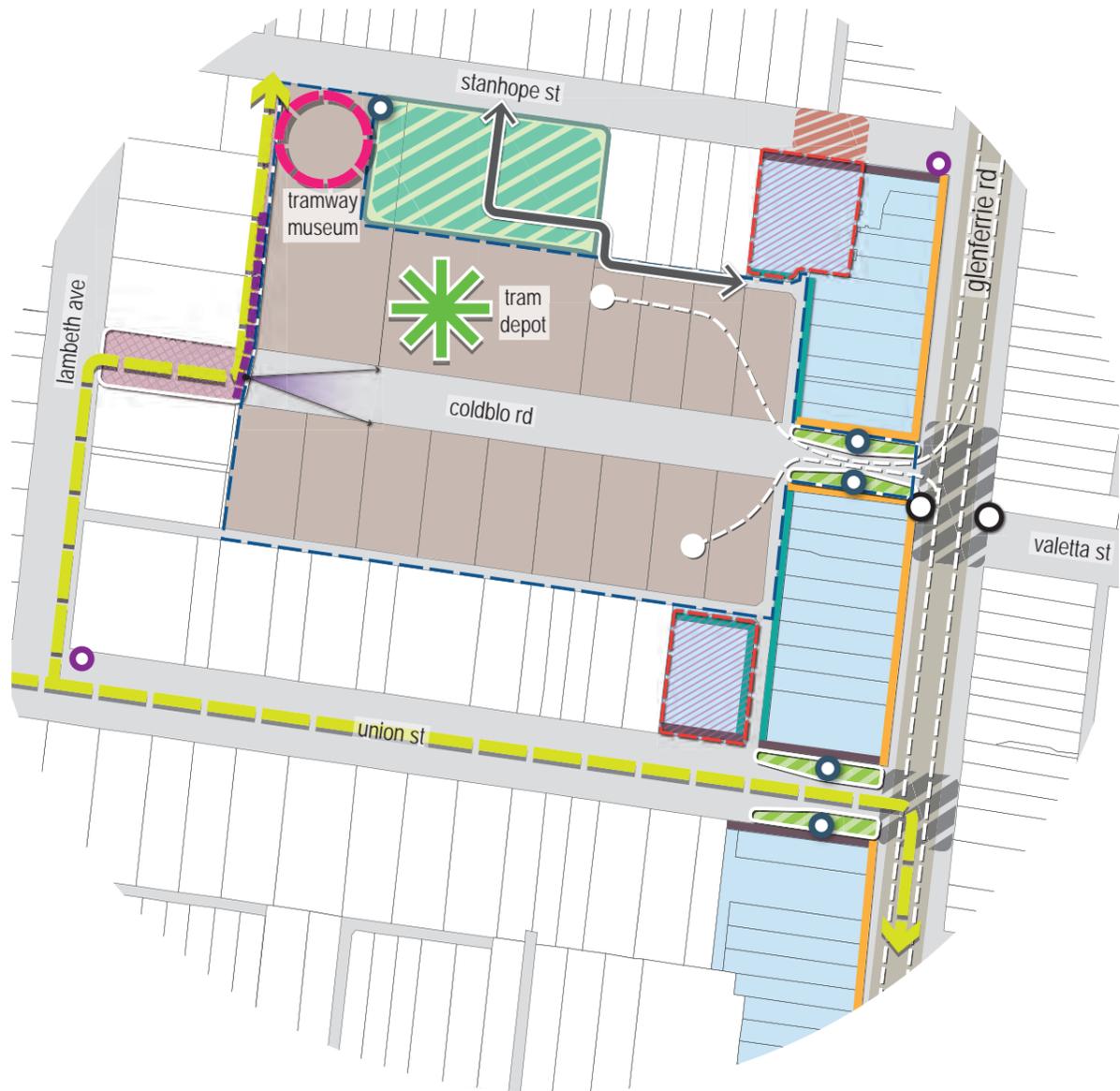


Figure 24 Precinct Identification

Figure 25 Precinct Framework Plan



Vision

The Coldblo Tram depot will retain its infrastructural role with opportunities for temporary or incremental provision of public access to the site through festivals, open days, or some other regular public event. It will foster community interest through facilitating visual and physical connections and to open up the facility to a wider audience. Future redevelopment of the Tram depot will emphasize the adaptive re-use of its existing high valued built forms.

Objectives

- Explore opportunities for greater appreciation of the heritage buildings within the Coldblo Tram Depot and adjacent Tramway Museum;
- Acknowledge opportunities for temporary community or trader group lead events in key areas of the site;
- Encourage improvement within and to the edges of the precinct, through landscaping and public realm improvements connecting to Glenferrie Road;
- To improve the Coldblo Street and Glenferrie Road junction and its role in facilitating safe and efficient access for all modes of transport;
- To improve pedestrian and cyclist connections throughout the precinct in particular along Union Street and Barkley Street; and
- Ensure that any future disposal of the Coldblo Tram Depot complex is carefully managed through the application of planning and design controls to protect heritage and integrate with the surrounding neighbourhood.



<p>tram depot infrastructure extents</p>	<p>remodel intersection</p>	<p>sensitive infill response</p>	<p>proposed shared path</p>
<p>key public transport infrastructure site</p>	<p>enhanced pedestrian connection</p>	<p>glenferrie road interface</p>	<p>potential future event space</p>
<p>main street</p>	<p>improve vehicular access</p>	<p>local street interface</p>	<p>investigate landscape enhancement</p>
<p>tram route</p>	<p>proposed pedestrian space</p>	<p>residential/ laneway interface</p>	<p>way-finding/ public art</p>
<p>tram stop</p>	<p>reduced maximum building height 4 storey</p>	<p>create visual link</p>	<p>way-finding/ signage</p>
<p>potential signalised pedestrian crossing</p>	<p>tram depot/ tramway museum (no heights specified)</p>	<p>potential activity node</p>	<p>street furniture hub</p>

Precinct 6: Drysdale Street Car Park

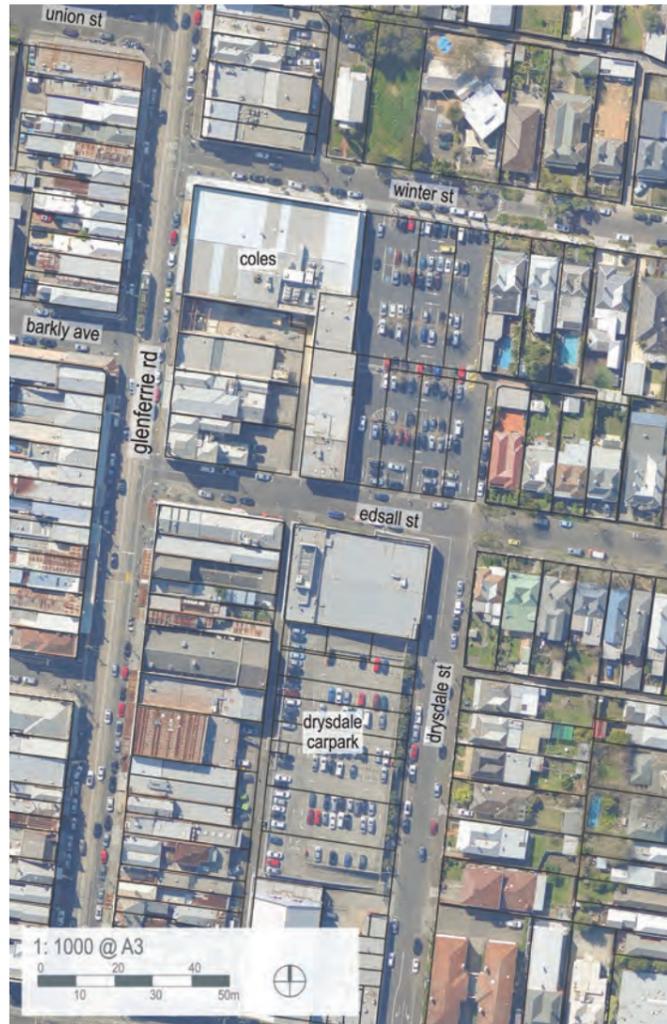


Figure 26 aerial context view



view of consistent street wall along glenferrie road



view through a post-war arcade to rear car parking



view to multi-level car park behind glenferrie road spine

Key Site Features

- Contains key retail anchors, which provide for local convenience shopping within the broader Centre context;
- Important source of custom and point of arrival to the Glenferrie Road local retail strip;
- Community facilities located within the threshold space between retail strip to the west and residential hinterland to the west;
- Proximity to Cabrini Hospital with direct linkages along Winter and Edsall Streets;
- Significant landholding acts as a buffer between residential land and Glenferrie Road commercial built form;
- Back of house presentation along the western end of Drysdale Street;
- Vehicle dominated environment with low levels of pedestrian amenity;
- Connection to Glenferrie Road is via Winter and Edsall Streets, as well as a privately owned arcade; and
- Limited provision of public realm elements such as lighting and pedestrian refuge zones.

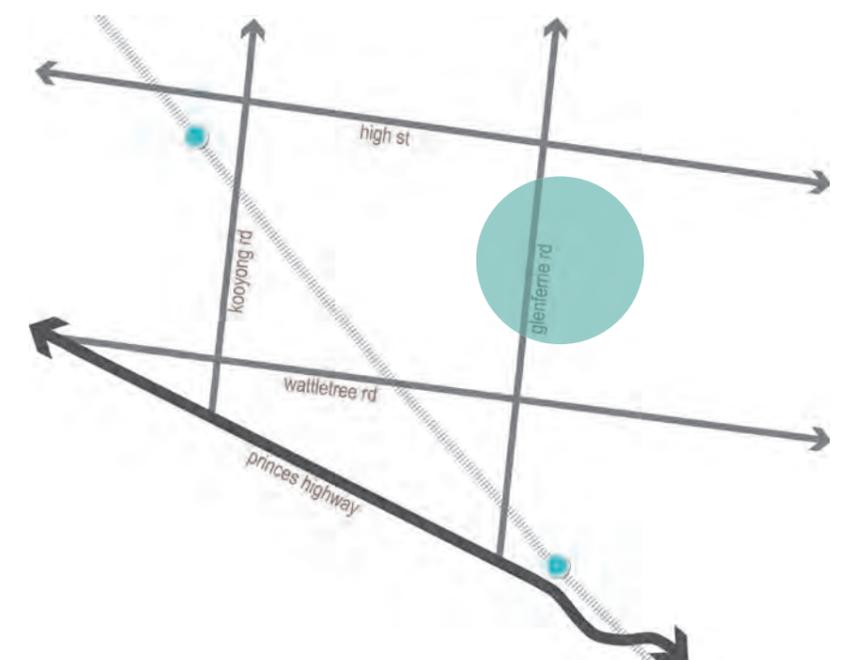
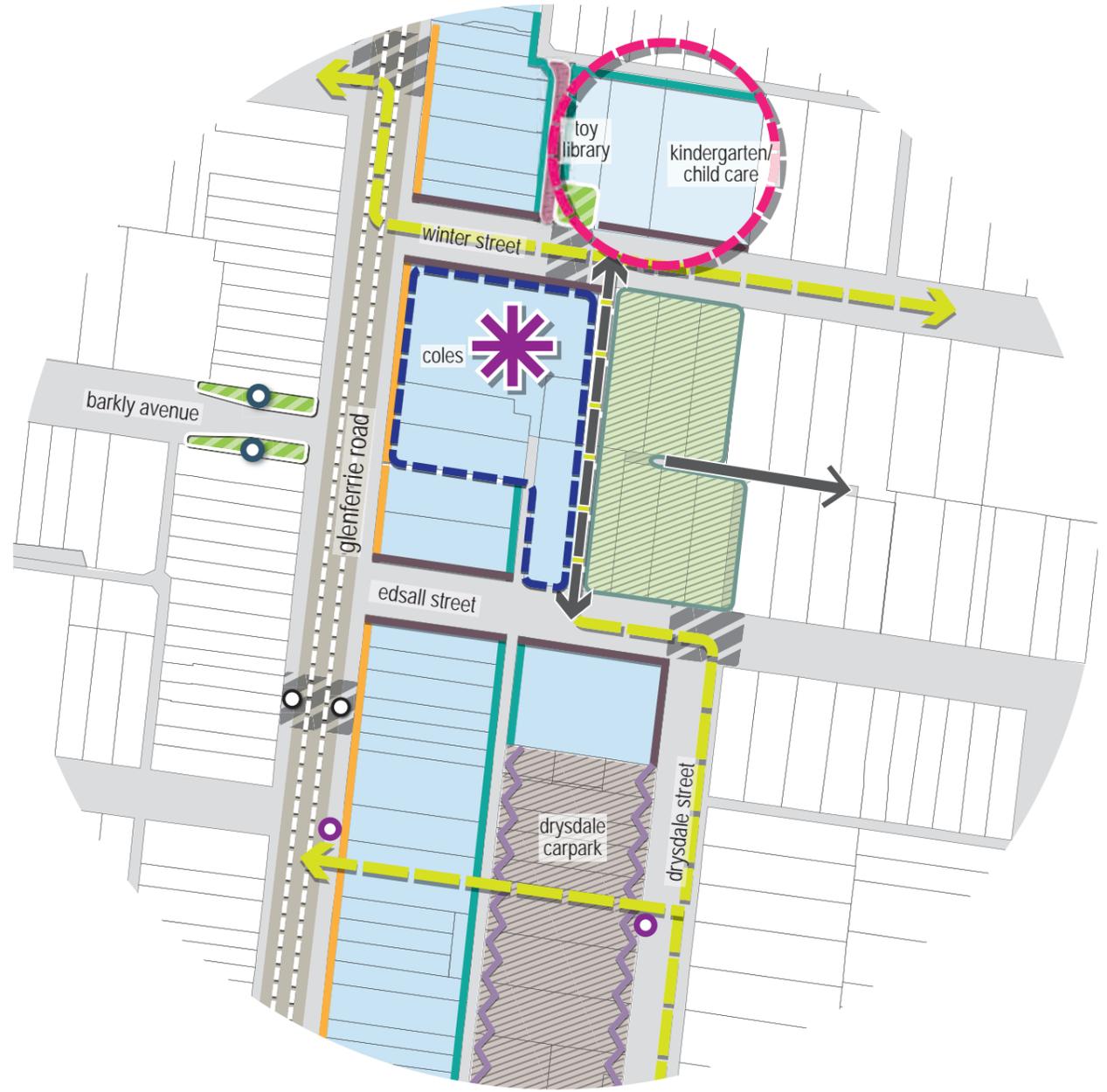


Figure 27 Precinct Identification

Figure 28 Precinct Framework Plan



Vision

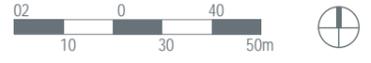
Opportunities exist for the anchor retail tenancy to expand and complement the vibrant, pedestrian – focused, mixed-use precinct. It will accommodate a range of retail offer, community facilities, and car parking in addition to modest development in shop- top format. New built form will provide active and attractive presentation to the public laneways and car park to the rear. Re-configuration of the Winter Street car park to introduce dedicated pedestrian paths to minimise potential conflicts. A new public open space will be accommodated around the existing Toy Library to provide a pedestrian refuge and resting area away from the busy main streets.

Objectives

- To reinforce and enhance the role of retail anchor tenancy in this precinct;
- To achieve an enhanced presence of community facilities including the upgrade of the Toy Library and to establish a public space at and around the community facility;
- To encourage shop-top infill development along Glenferrie Road with appropriate presentation to the car park;
- Manage positive expansion of the anchor retail tenancies vertically, and ensure the protection of period commercial forms further to the south;
- Encourage reinstatement of rhythmic active frontage treatments to Drysdale Street and Glenferrie Road, with attractive frontages to the car park interface;
- Intensify built form scale in the back-of-house area to provide surveillance an improved interface to the car park;
- Simplify and improve the quality of north-south pedestrian, bicycle and vehicle access through the at-grade car park including footpath by 20m minimum width along edge of retail tenancy; and
- Incorporate public realm improvements within the surface parking area to enhance the sense of place of an important arrival point to the Centre.

legend

main street	proposed formalised north-south street	potential retail expansion	retain landscape enhancement
tram route	preferred maximum building height 4 storey	main street interface	proposed shared path
tram stop	building height subject to site specific investigation	side street/residential street interface	way-finding/ public art
potential signalised pedestrian crossing	existing car park	rear/ laneway interface	street furniture hub
enhanced pedestrian connection	existing retail anchor	improve interface presentation	
retain vehicular access	potential community hub and public open space	proposed pedestrian space	



9 Implementation

How will the Structure Plan be Implemented?

The implementation of this Structure Plan will occur progressively over a long time period (10-25 years), and will require careful on-going management and continued communication with the local community, business owners and landowners. The co-ordination across a wide range of stakeholders and funding partners is necessary to ensure a productive approach to the development of this important Centre and improvements in keeping with the valuable role the centre plays within the City of Stonnington.

The Glenferrie Road / High Street Activity Centre Structure Plan outlines a series of objectives and strategies that will serve to enhance the character, image and potential of the centre. Key actions, which are directly drawn from stated strategies, will be prepared once the broader directions of the Structure Plan have been considered by the community. It is these actions which will follow the formulation of this document which will be critical to the implementation of this Plan. While many immediate actions will be the responsibility of departments within Stonnington City Council, initiatives will also need to be pursued by other government agencies and landowners as indicated in the tables.

While the Council's lead role in review of development proposals is critical, it is also acknowledged that the centre has a strong history of community engagement and involvement through associations such as the traders associations for Glenferrie Road and High Streets respectively. These groups will all have opportunities to influence the implementation of this plan and their support for the document will add weight and urgency to the actions contained within it.

In addition, the role of private and public organisations in facilitating the development of many of these ideas should not be underestimated through the use of public-private partnerships where appropriate. Where these are supported by the plan, Council has a role to play in facilitating the plans outcomes.

As with many Activity Centres, the Structure Plan area has a basic zoning structure that includes a strip of commercial zoned land surrounded by residential land, with little guidance provided to balance the existing State Planning Policy. The existing policy regarding development in Activity Centres has been developed on planning principles which seek to consolidate increased populations and intensity of development where there are existing nodes of activity, which as is the case with the Glenferrie Road / High Street Activity Centre.

Providing increased densities where there is excellent access to services, facilities and transport also makes for a more equitable and environmentally sustainable society. However, it is important to recognise the local conditions and to ensure that how this density is accommodated actually reflects the existing conditions, particularly within an Activity Centre with such a strong local character. By allowing this Structure Plan to carefully consider and identify areas which will be suitable to accommodate modest increases in density and height the position of protecting other areas within the centre from more intensive development is strengthened.

The first and critical step in realising the objectives of the Structure Plan is the establishment of appropriate planning controls to implement the over arching directions of the Structure Plan, which involves a range of implementation options, including: local polices and specific design controls which require formal implementation into the Stonnington Planning Scheme.

Once the Structure Plan and the associated recommendations for an appropriate suite of local polices and design controls has been considered and formally adopted by Council, it will be necessary to facilitate a Planning Scheme Amendment to formally incorporate the recommendations into the Stonnington Planning Scheme.

Finally, it must be accepted that changes in the centre will unfold over a long time period and the needs and aspirations of the community will change over time. Therefore the Structure Plan must be flexible enough to be adapted to changing circumstances, be they relating to economic, social or environmental factors. Continued monitoring and evaluation of the Structure Plan must be undertaken by the Council to ensure that it remains relevant to current practice and community expectation.

Funding Strategies

The Structure Plan requires a funding strategy to outline the means by which concepts may be implemented over time. Stonnington City Council will need to carefully consider how this will occur as part of the broader funding strategies. The role of a funding strategy will be to identify sources from which funds will be derived for this particular project. A funding strategy will need to look at all income sources and the potential of different sources to contribute to the total cost of implementing the plan. Importantly, in the Glenferrie Road / High Street context, the on-going partnership and collaboration between Council and the many active community associations and other agencies operating in the area will be crucial to identifying and accessing a broad range of funding opportunities to achieve the aims of this plan. Other elements of the funding equation are likely to include the following:

Funding from General Rates - The ability of the Council to fund public works from general rates is limited by budgetary constraints and the current commitment of funds for existing major capital works projects. However, in order to demonstrate its commitment to civic improvements in this important local centre, the Council should to provide some funds from general revenue. This could be either for specific projects, or on a dollar for dollar basis (or some other proportion) to match funds derived from other sources.

State Government Funds - The State Government makes funds available to local government for public works and urban improvements. This document will provide the Council with strong strategic justification to support applications to the State Government for project funds as they become available. It is also noted that some elements of this Structure Plan, such as cycling infrastructure, is outside of the control of Council and needs to be actioned by State Government agencies (eg. VicRoads).

Special Charges - The levying of a special charge to cover the cost of works would be an option for generating funds for works, in situation where existing property owners and businesses benefit (such as the provision of road upgrades or consolidated car parking opportunities for example). A special charge could also be levied to cover the administrative and operational costs of any implementation body established to implement the plan. Political and equity considerations need to be taken into account in deciding to introduce a special charge

Other Sources - A series of new and innovative approaches to development funding are also available, including the opportunities for 'partnership projects' that demonstrate collaboration and joint venturing between the private and public sector. In addition, funding is available through agencies such as Sustainability Victoria and other organisations that focus specific areas. More standard public / private partnerships could also be pursued by Council to achieve some key identified projects.

Other Actions

In addition to implementing the Structure Plan through the Stonnington Planning Scheme, a number of other actions should be initiated by Stonnington City Council, other authorities and local stakeholders to achieve the 'vision' of this plan. There is also further work which this plan has identified which will need to be undertaken before particular strategies can be pursued. These are also outlined on the following page. As mentioned in the introduction to this section, the development of the initiatives identified above will happen over a period of time as developments are proposed and funding becomes available. Priorities may also change over time, as selected initiatives of the plan become more critical or of greater importance to the Glenferrie Road / High Street community. To assist in the process, however, a number of priorities have been identified which should be the focus for the pursuit of funding. The resolution of some more specific areas through a process of more detailed concept design is also highlighted.

Identified actions will require the cooperation of a number of different parties including landowners and other government authorities. Prioritising the actions allows not only for the key projects to be highlighted but for additional weight to be added when seeking funding for specific projects identified as priorities. As opportunities for funding become available this Structure Plan will demonstrate the strategic importance of noted projects. The following timeframes have been adopted for this project (immediate: as soon as possible, short term: 2015 - 2020, medium term: 2020 - 2025 and long term: 2025-2035). The table outlining these actions can be found on the following page.



10 Action Plan

action table				
#	action	council responsibility	external / groups	priority
1	Identify an opportunity for the two business associations to work together on a project which aims to strengthen the identity of the activity centre while also celebrating the differences between the two 'arms' of High Street and Glenferrie Rd. (this could be a masterplan)	Public Spaces & Capital Works	Traders Associations, local business owners	Short Term
2	Set up working group with Cabrini Hospital and allied health care providers to establish further needs or progress a study, with respect to expansion of medical facilities along the designated Wattleree Road medical related spine.	City Strategy, Economic & Cultural Development, Aged Diversity & Health	Cabrini Hospital, health care providers	Short Term
3	Initiate additional contact with Malvern Central Management regarding pedestrian access upgrades and future redevelopment potential of Malvern Central.	City Strategy, Statutory Planning, Economic & Cultural Development	Malvern Central management	Short Term
4	Undertake detailed design to facilitate and implement upgrades to the public realm within the centre in line with the Structure Plan and with a focus on the 'pedestrian priority' areas, key public notes and the 'key movement corridors'.	Environment and Public Spaces, Infrastructure Services, Parks, Environment and Buildings, Public Spaces & Capital Works	VicRoads, Traders Groups, local business owners	Short to Medium Term
5	Investigate traffic yield control treatments (textured surface, speed bumps or raised thresholds) at the identified 'pedestrian safety' road areas.	City Strategy, Public Spaces & Capital Works, Transport & Parking.	VicRoads	Medium Term
6	Draft new Local Planning Policy and Design and Development Overlays as part of an amendment to the Stonnington Planning Scheme to implement the recommendations of the Structure Plan.	City Strategy, Statutory Planning		Short Term
7	Work with VicRoads to introduce on-road cycle lanes, road markings, wayfinding signage, cycle and pedestrian facilities and green bicycle boxes at traffic lights within existing street network.	Transport & Parking	VicRoads	Short Term
8	Provide additional bicycle storage facilities in areas of high demand, including at community facilities, parks, and in the vicinity of cafes.	Transport & Parking and Parks, Environment & Buildings	VicRoads	Short Term
9	Commence a detailed review of the community node surrounding Malvern Library, and investigate measures to improve pedestrian prioritisation and connectivity with the adjacent Malvern Cricket Ground and the nearby Malvern Town Hall / Town Square.	City Strategy, Public Spaces & Capital Works, Leisure & Libraries		Medium Term
10	Develop a strategy for the intersection of High Street and Glenferrie Road to become the symbolic 'heart' of the centre.	City Strategy, Leisure, Infrastructure Services	VicRoads, Yarra Trams	Short to Medium Term
11	Investigate the provision of a signalised pedestrian crossing on Glenferrie Road between the traffic signals at High Street and the signalised crossing south of Edsall Street to improve pedestrian crossing opportunities within this strip.	Transport and Parking,	VicRoads	Medium Term
12	Investigating a new link along the railway line adjacent Malvern Central.	City Strategy, Transport and Parking, Public Spaces & Capital Works	VicTrack, Malvern Central	Medium Term
13	Investigate the benefits of an eastbound evening peak hour tram lane on High Street to coincide with the existing clearway hours of operation.	Infrastructure Services, Transport and Parking	Yarra Trams, Vic Roads	Medium Term
14	Investigate a 'shared space' where all road users utilise the same area to reduce the dominance of motor vehicles in the predominantly pedestrian area of Morey Street and Kings Way in the vicinity of Armadale Station.	City Strategy, Transport and Parking, Public Spaces & Capital Works	PTV and VicTrack	Medium Term
15	Investigate the implementation of a 40 km/h speed limit through all streets within the Centre.	Transport and Parking	VicRoads, Yarra Trams	Short Term
16	Investigate a redesign of the intersection between Llaneast Street car park and Wattleree Road to improve operation and amenity (potentially by converting the Llaneast car park access to entry only and providing a footpath).	Transport and Parking, Public Spaces & Capital Works	VicRoads	Medium Term
17	Set up a working group with VicTrack seeking opportunity to provide aesthetic landscaping within the rail corridor.	City Strategy, Transport & Parking and Public Spaces & Capital Works	VicTrack	
18	Develop a set of 'public realm' guidelines for an identified style of street furniture, wayfinding signage and other fixtures to be utilised throughout the Study Area. The design of all elements of street furniture should reflect the identified 'branding' for the centre.	City Strategy, Economic and Cultural Development, Public Spaces & Capital Works	VicRoads	Short Term
19	Develop guidelines for vegetation / tree species selection for use in public realm works throughout the Study Area	Parks, Environment & Buildings	VicRoad, Yarra Trams	Short Term
20	Undertake feasibility of Winter Street community hub and public open space	City Strategy, Economic & Cultural Development, Children & Family Services, Public Spaces & Capital Works		Short Term

Figure 29 Implementation Action Plan



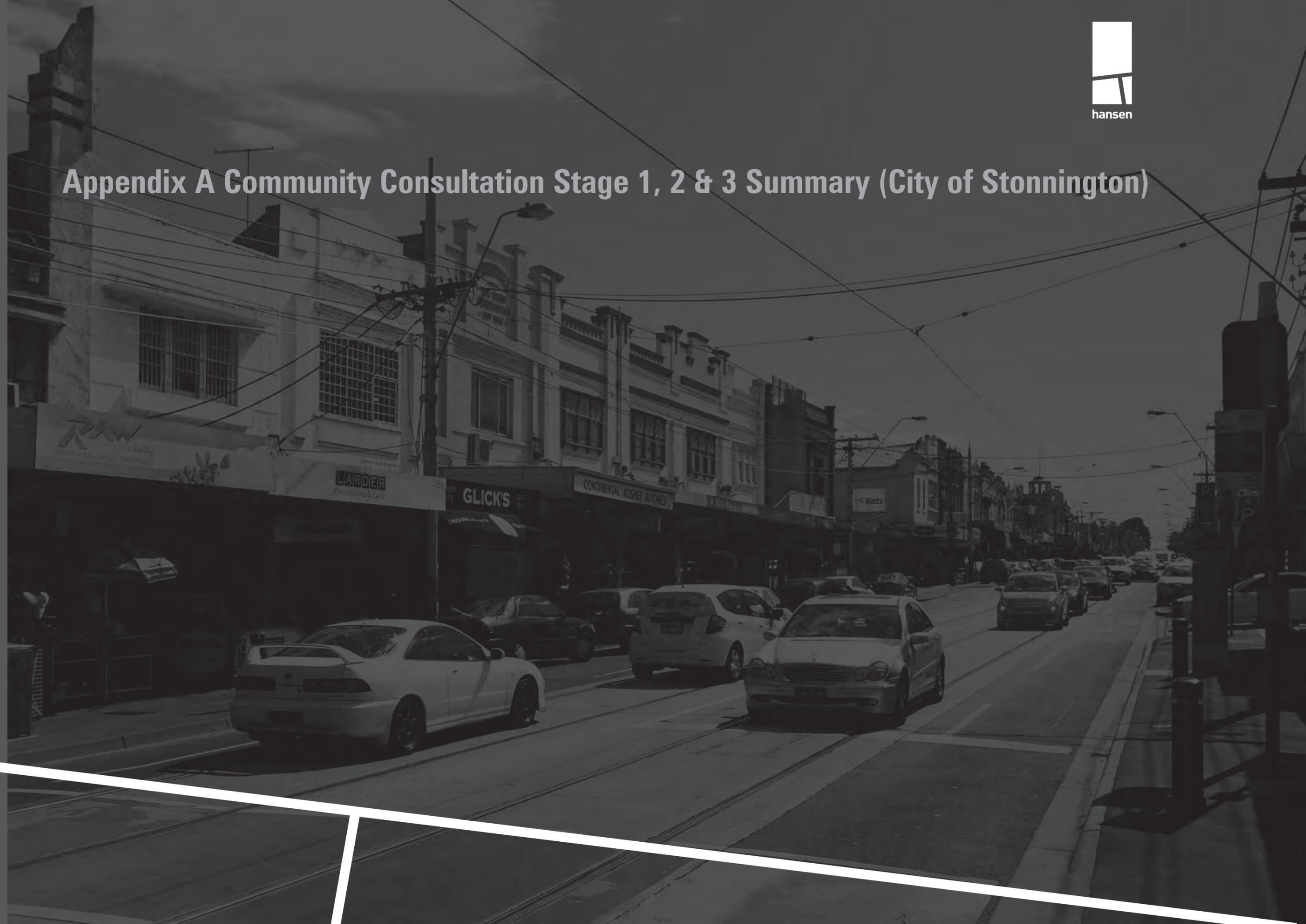
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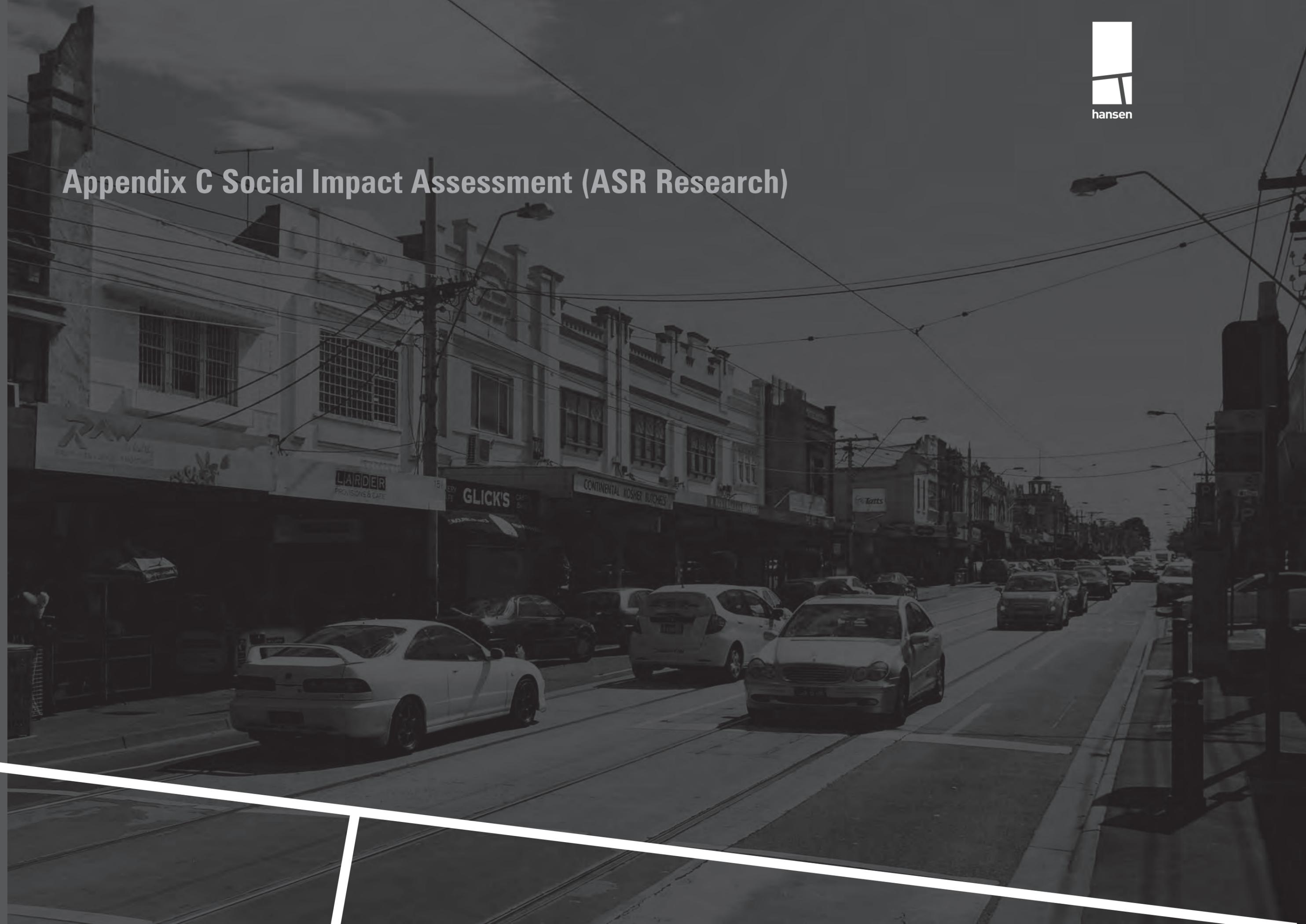
Appendix A Community Consultation Stage 1, 2 & 3 Summary (City of Stonnington)



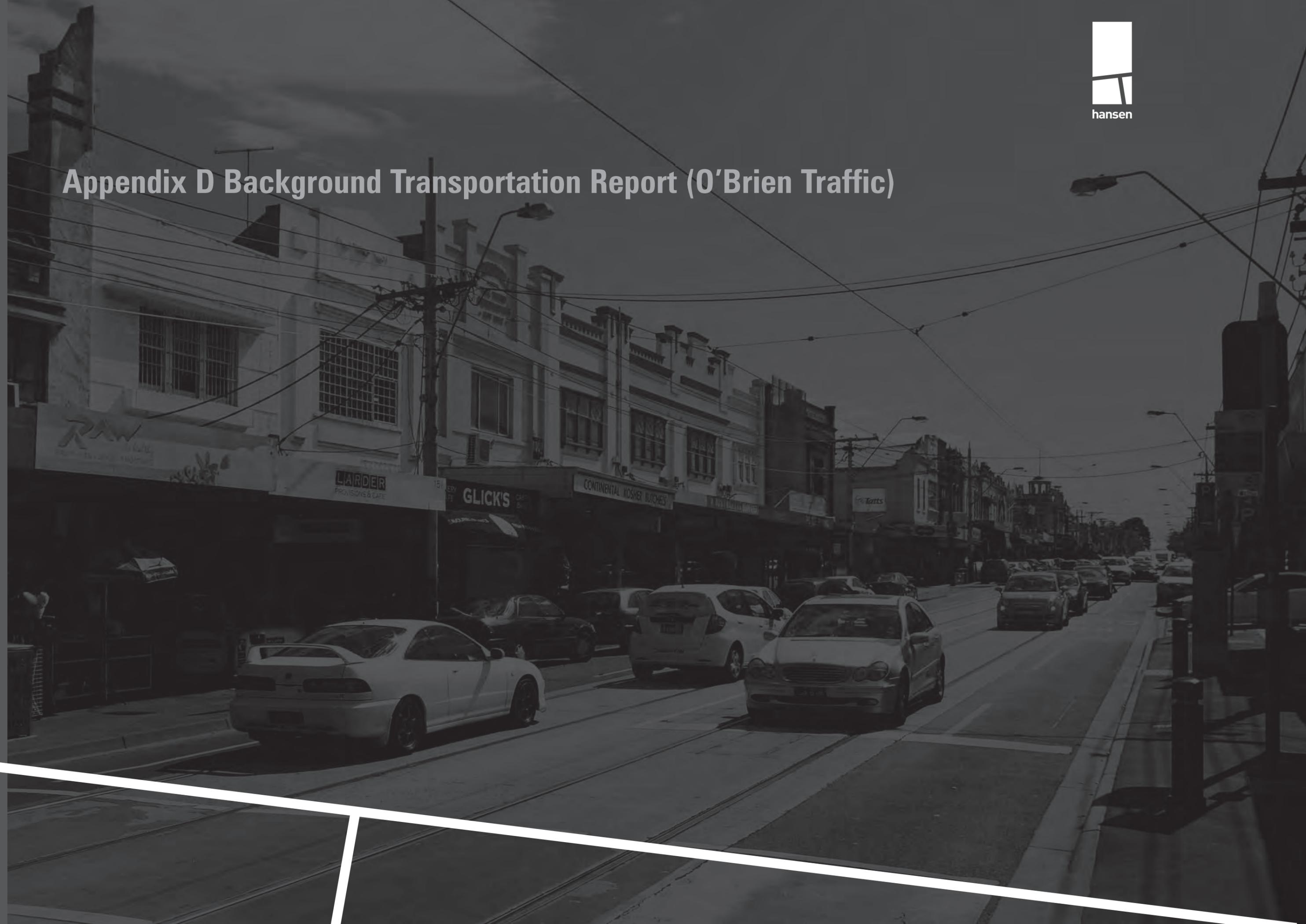
Appendix B Economic Technical Report (Essential Economics)



Appendix C Social Impact Assessment (ASR Research)



Appendix D Background Transportation Report (O'Brien Traffic)



GLENFERRIE ROAD AND HIGH STREET ACTIVITY CENTRE STRUCTURE PLAN

Planning for the centre now and in the future

Your feedback summarised

Consultation Stage One March 2014

Stonnington City Council has commenced the preparation of a Structure Plan for the Glenferrie Road and High Street Activity Centre. The Plan will set out a vision for the look and feel of the Centre, and provide parameters for future growth and change.

The project commenced in March 2014 by seeking the community's views on key features and characteristics of the Centre, areas for improvement, and what is envisaged for the future. Similar discussions were also conducted with other stakeholders such as traders, major landholders and other key services (such as schools, Malvern Central). Government departments and agencies also provided comment.

The draft Structure Plan is due for completion in early 2015. There will be further invitations for input into the Plan as it is being developed. The next stage will involve reviewing the identified issues and opportunities, and testing key elements and ideas shaping the Structure Plan. This is scheduled for mid 2014.

What you told us...

Some 97 residents, business owners and organisations provided feedback during the first consultation stage in March. The comments made through the surveys, interviews and focus group discussions have been collated into themes. Key information for each theme is summarised below:

Vision for the area

In 2040, the Glenferrie Road – High Street Activity Centre is thought of as a vibrant, inviting, attractive hub of relaxed shopping and leisure activities with a special charm and community feel.

It will be greener with a unique appearance and feel that enhances and protects the streetscapes that reflect the buildings of yesteryear.

The Centre will be safe, accessible and pedestrian friendly with efficient public transport and good access to car parking.

Businesses and Services

The range of businesses and services within the Centre are highly valued. The combination of larger stores and unique independent shops, cafes and restaurants form part of the appeal.

Feedback suggested that future planning for the Centre should encourage a mix of diverse retail, leisure and dining opportunities and create an attractive and appealing shopping ambience.

Community

Retaining and supporting a sense of community is very important. The Centre should continue to be developed as a place that is welcoming, useable and accessible for all.

The centre's sense of community could be enhanced with more open spaces, accessible buildings and facilities, and improved tram stops at schools and the Armadale train station.

There is seen to be an opportunity to ensure that the major education, civic and health services in the Centre support each other and are well connected.

Movement

Public transport services, pedestrian movement, traffic flow and ease of access to car parking were identified as being important to the Centre.

The consultation feedback highlighted mixed views about vehicle and pedestrian movement, with some supporting more car parking, and also support for more active and sustainable transport such as public transport, walking and cycling.

Public Spaces

A lack of green spaces, a public heart and other quaint public spaces was identified as an area for improvement, with a desire for more of these types of places in the Centre.

Creating more pleasant open space and public places to provide for congregation, rest and contemplation (such as seating, sculpture, toilets and landscaping) was suggested for the future.

Buildings and Infrastructure

There was a strong desire for the protection and enhancement of the old-style, heritage buildings and the picturesque streetscapes of Glenferrie Road and High Street.

The retention and maintenance of heritage buildings and the way that new buildings fit with the look and feel of the street were seen as a key issue. The variety of existing buildings and the potential for infill without loss of heritage value was identified as a challenge for the Structure Plan.

Having sufficient, appropriate infrastructure (such as waste management) to support users and residents of the area, and accommodate the demands of future need was reinforced.

Environmentally Sustainable Design

Environmental sustainability featured strongly in views about the future. The way the Centre is developed and how it is used should be environmentally sustainable, for example, through features such as recycling, water conservation and reuse, localised energy generation and burying overhead powerlines.

How to find out more / have your say

The involvement of residents, traders and other interested parties in this project is important to ensure that the Structure Plan reflects the community's aspirations for the Centre, and this community involvement is valued by Council. Your ongoing participation is encouraged.

For further details on this project or to register to be kept informed of future consultation opportunities please contact City Strategy on 8290 1395 or visit Council's website at www.stonnington.vic.gov.au/glenferriehigh

GLENFERRIE ROAD AND HIGH STREET ACTIVITY CENTRE STRUCTURE PLAN

Planning for the centre now and in the future

Consultation Stage Two - August 2014

Your feedback summarised

Stonnington City Council has commenced the preparation of a Structure Plan for the Glenferrie Road and High Street Activity Centre. The Plan will set out a vision for the look and feel of the Centre, and provide parameters for future growth and change.

The project commenced in March 2014 with Stage 1 consultation. This stage sought the community's views on key features and characteristics of the Centre, areas for improvement, and what is envisaged for the future. Similar discussions were also conducted with other stakeholders such as traders, major landholders and other key services (for example; Malvern Central). Government departments and agencies also provided comment. Some 97 residents, business owners and organisations provided feedback during the first consultation stage in March.

Stage 2 consultation on the key issues and opportunities was conducted from 18 August to 5 September 2014. This consultation sought the community's views on:

- a draft vision for the activity centre
- significant sites and what opportunities they present
- key themes including; linkages and connections; car parking; pedestrian, cycle, public transport friendly environment; and open / public space.

The next stage will involve reviewing the Draft Structure Plan which is scheduled for early 2015, with the draft Structure Plan due for completion in early 2015.

What you told us...

Through Stage 2 consultation the community was able to provide feedback at three discussion sessions with traders and residents. Approximately 50 residents, business owners and representatives from both Glenferrie Road and High Street Traders Associations attended the discussion sessions. The comments made through the discussions have been collated into themes. These themes are summarised below:

Vision

In summary, the visions expressed by the community were around the smart use of space, optimising opportunities, creating an accessible, people-focussed environment abundant with character and high quality amenity.

A thriving shopping precinct – for local needs and specialist retailers for the region.

- It is deemed beneficial to have a **mixture of regional and local functions**. There was a strong view that it would be beneficial for the centre to have a greater balance of uses to achieve its vibrancy and long term resilience by finding its niche and own unique offering.

Vibrant, inviting, attractive and clean with well-designed, 'green' streetscapes, pedestrian connections and congregation spaces.

- There was support for converting **more areas to green spaces**, retaining **street trees** and incorporating environmentally sustainable initiatives such as solar panels and green roofs. Amenity enhancements could include **activating public spaces** (car parks), improved lighting, public toilets, way-finding, landscaping and management of litter and graffiti.

Developed with respect for heritage buildings which will be enhanced and remain prominent.

- Heritage and its maintenance is deemed to be a very important element of the activity centre. There was a desire for a reasonable approach to be taken to development and protection that **balances the amenity of heritage with functional needs**.

A centre for local employment and housing within the main streets while protecting residential areas.

- Support for mixture of residential and retail / commercial use while **balancing the impact on existing residential amenity** and not displacing commercial uses. The scale of development should be identified and strictly managed.

Significant sites

A number of significant sites have been identified in the study area. Following is a summary of the main comments relating to the opportunities at these sites:

- The **Dandenong Road gateway** is lacking in character and would benefit from a gateway treatment and improved amenity. There was a desire to protect the heritage buildings given any development in this area. Potential uses that were suggested for development in this area included aged accommodation, a cinema (the original cinema building is still in this area), and residential. It was suggested that any development should include setbacks to provide space for greenery on Glenferrie Road.
- Overall, improvements were suggested to enhance the interface, way-finding, amenity and vibrancy of **Malvern Central** at all access points, but particularly with Glenferrie Road and Malvern Station. It was suggested that the centre could have a higher built form and could be appropriate for multi-level car parking. Other suggestions were made for enhancing links to the centre, including continuing Railway Avenue over the railway line to reconnect Dandenong Road with Wattletree Road, and continuing the laneway directly east of Malvern Central (running along the rears of shops facing the west side of Glenferrie Road). Some residents suggested a need for more entertainment features in the activity centre, eg. cinema, bars, bowling alley, music – and that the Malvern Central area would be an appropriate place for these uses.
- An opportunity was identified at the VicTrack **land between the railway line and Malvern Central** for the creation of a green corridor.
- Many suggestions were made for development over the **Malvern station** and the railway line while preserving the heritage value of the station, including Station Street / Claremont Avenue village. There were different views as to what the space should be used for, including; open space, car parking, commercial development (not necessarily retail) or residential. The need for improved accessibility to and through the station was reinforced. It was also suggested that the area could provide better for bicycle parking.
- The **Armadale station** area is seen as having great potential. Residents sought more vibrancy and activation of this area including a higher quality public realm to better provide for pedestrians and improved car parking conditions, way-finding and landscaping. Shared zones were suggested for this area and other pedestrian connections were also identified including to and through Kings Arcade / Kingsway which is also seen as a

unique environment to be enhanced. Building over the railway line was seen as an opportunity for achieving more open space in this area and also for increasing car parking supply.

- There was general support for the presence of a **health precinct along Wattletree Road** associated with Cabrini hospital. However, there was a strong view that the interface with residential areas needs to be managed, and the heritage value of the area is to be protected. It was suggested that any expansion of health services should be limited to Wattletree Road. Car parking for visitors and staff is a key issue in this area and needs to be managed. Residents reinforced the need for good pedestrian access in the area.
- The **tram depot** was identified as a site of great potential and opportunity for the activity centre. Protection of the heritage buildings was strongly supported.
- The opportunity was identified to activate the **Malvern Town Hall / civic precinct** more as a civic space. Suggestions were made to increase way-finding and to enhance pedestrian access to and through the site (eg. creating a walking circuit), and improve landscaping. Residents noted the opportunity to enhance the space around Northbrook House and the historic centre, including development of the gardens.
- There were mixed views about the current design, character and functionality of **Malvern Square** (in front of Georgio's restaurant at the corner of High Street and Glenferrie Road). Various suggestions were made for the site, including; incorporating more green into the landscape with shady trees, creepers and lawn, holding a market and other activities, making the site more level so that it can be used as more of a congregation space, and increasing public seating.
- Residents and traders stressed that all **car parks** should be retained. Further, it was noted that new developments should include parking provisions. The need was identified to improve the safety and amenity of car parks through way-finding, signage, enhanced lighting, landscaping and public toilets. Suggestions were also made to provide all-weather protection through roofing car parks, undergrounding car parks to provide for open space at-grade, and to consider removing some small buildings to create open space connections between the main streets and rear car parks. Car parks were also seen as presenting alternative opportunities, such as recreational areas when not in use. Creating multi-level car parks was suggested where there was an identified need for additional supply. To manage demand for car parking it was suggested that visitor car parking should be given priority while also providing safe areas for staff to park in longer term spaces (such as top level of multi-level car parks). Provision of bicycle parking at car parks was also suggested.

Theme: Pedestrian, cycle, public transport friendly environment

Strategies for creating a pedestrian, cycling and public transport friendly environment were sought from residents and traders. The following suggestions were made:

- Residents saw streets as 'community hubs' where pedestrian access should be prioritised and the street spaces should service as community spaces. It was noted that there is conflict between pedestrians, cars, bikes and trams on Glenferrie Road. Various suggestions were made to address the street environments, including reducing speed limits, limiting turning options and creating one-way areas in and around side streets.
- Priorities for pedestrian movement were noted as; widening footpaths and developing kerb 'outstands' with shelters and seating at side street junctions. Suggestions were made for improving some linkages and local pedestrian travel routes to provide better connections throughout the activity centre.
- Opportunities were identified to better link the Kooyong Road bus route and reduce the number of express trains that pass through the train stations in order to increase provision of public transport in the centre.

- Routes for bicycle travel were suggested including safe alternatives off Glenferrie Road and High Street, as well as improved way-finding and bicycle parking facilities. The need to improve the cycling route along Glenferrie Road was highlighted and creating dedicated bike lanes along the main streets and developing an off-road bicycle route along the railway line corridor were also suggested.

Theme: Open / public space

- There was a view that **more open space** was required in the activity centre and that there is **not enough green space**. A need was also expressed for general streetscape improvements. The vacant King David School site was noted as an opportunity for creating additional public open space.

Theme: Built form

- Delicate **development** of multi-storey buildings was considered to be appropriate in the activity centre, with protection of heritage buildings and amenity from the street face (ie. through the use of set-backs, no overlooking or overshadowing). Shop top development was generally supported at appropriate sites. It was recognised that the greatest opportunity for development in the activity centre is infill at individual sites – which should occur at a scale that reflects surrounding buildings.
- Better utilising **second storeys of buildings and shop tops** for residential / office spaces was suggested.

Theme: Land use

- Residents were mindful of the balance between the regional and local focus of the retail sector in the centre, and expressed a strong desire for the local catchment needs to be the primary focus. Residents were mindful of the current retail environment and suggested the need to consider **alternative purposes for shops** and the potential impact of a changing retail sector.
- A need was highlighted for development and use of land to **build a sense of community** through street life and activity. There was some support to build **night time activity** in the centre, providing it is appropriately located and sensitive to surrounding residences.
- A need was identified for additional **public toilets** throughout the activity centre.

How to find out more

The involvement of residents, traders and other interested parties in this project is important to ensure that the Structure Plan reflects the community's aspirations for the Centre, and this community involvement is highly valued by Council. Your ongoing participation is encouraged.

For further details on this project, or to register to be kept informed of future consultation opportunities, please contact City Strategy on 8290 1395 or visit Council's website at www.stonnington.vic.gov.au/glenferriehigh

CONSULTATION SUMMARY: STAGE 3

July 2015



2015 GLENFERRIE ROAD HIGH STREET ACTIVITY CENTRE STRUCTURE PLAN

Introduction

What is a Structure Plan?

The City of Stonnington is preparing a Structure Plan to plan for and manage the future development and growth of the Glenferrie Road and High Street activity centre to 2040. . Council has recently completed the Draft Structure Plan and has undertaken the associated Stage 3 Consultation phase. The Draft Structure Plan sets out a vision for the look and feel of the Centre, and provides design guidance and parameters for potential future growth and change.

Report Purpose

This report provides summary of the feedback received during the Stage 3 consultation phase. It is designed to acknowledge the responses, issues and opportunities identified by the community and stakeholders. The feedback has been paraphrased to illustrate the sentiment of the participants. The report is structured to respond to each of the core consultation questions.

Next Steps

The City of Stonnington will consider all the feedback received during the engagement period, together with directions set out in government policy and evidence from research studies that will be undertaken. The information will be used to inform the final Glenferrie Road, High Street Structure Plan.

Background Context

Glenferrie Road and High Street is a designated Major Activity Centre and transport hub approximately 8 kilometres southeast of Melbourne's Central Business District.

The area includes two distinct shopping areas along High Street, Armadale and Glenferrie Road, Malvern. Extending from west of Armadale Station near High Street to south-east of Malvern Station off Glenferrie Road, the area is large in scale and offers a mix of shopping, leisure and employment, together with health and education facilities within and around the centre. As a Major Activity Centre, state government policy identifies that the centre provides additional scope to accommodate ongoing investment and change in retail, office, community, services and residential markets.



Community and Stakeholder Engagement

Community consultation was undertaken as part of Stage 1 in March 2014. This first round of consultation sought the community's views on key features and characteristics of the Centre, including areas required for improvement and the overarching vision for the future. Similar discussions were also conducted with other stakeholders such as traders and major landholders as well as government departments and agencies.

Stage 2 consultation focused on the key issues and opportunities to be developed within the Structure Plan and was conducted from 18 August to 5 September 2014. This consultation sought the community's views on a draft vision for the activity centre, significant sites and what opportunities they present. Key themes were also discussed, including; linkages and connections; car parking; pedestrian, cycle, public transport friendly environment; and open / public space.

Stage 3 consultation was conducted from 16th March to 3rd April 2015 and allowed the community and stakeholders to review the Draft Structure Plan. Public Notification of the Stage 3 Community Consultation process was conducted by publishing a notice in the Stonnington Leader on 17th March, 2015 and sending 6000 brochures within and surrounding the Structure Plan area. Relevant information was made available on-line via the City of Stonnington website, where an information brochure, FAQ sheets and questionnaire could be downloaded. Refer to Appendix 1 for the Brochure and Appendix 2 for the online questionnaire template.

Methodology

The methodology undertaken throughout the Stage 3 consultation process was streamlined into 5 key stages to ensure simplicity and clarity of the project process. These stages include;

- Stage 1: prepare draft consultation engagement material
- Stage 2: finalise consultation engagement material
- Stage 3: undertake consultation processes
- Stage 4: analysis of consultation feedback
- Stage 5: prepare key findings and recommendations report

Consultation platforms

The community, including local residents, landholders and other stakeholders were given the opportunity to provide feedback via a number of platforms, including;

- *An online survey*

A total of 18 participants provided feedback in the Draft Structure Plan Online Survey. This questionnaire sought feedback on a range of issues including heritage and built form, landscape and open space, economic activity, car parking and public transport. Participants were given the opportunity to answer questions with 5 options ranging from 'Strongly Don't Support' through to 'Strongly Support'. There was also opportunity to provide detailed comment on each theme of the Draft Structure Plan.

- *Email submissions*

Email submissions commenting on the proposed initiatives with the Draft Structure Plan were encouraged from 17 March to 13 April 2015. A total of 22 submissions were received.

- *Community walking tour/ information session*

A Community 'Walkshop' was held on Saturday 21st of March 2015 at 10.00am to 12.00pm. A total of 5 people participated in the event. The format of this consultation initiative included 2 parts, comprising a 1 hour Study Area tour conducted by the Structure Plan authors and a 1 hour follow up workshop for additional questions and discussions. The purpose of this 'Walkshop' was to engage with a group of interested residents, workers and business owners on the Draft Structure Plan recommendations and to highlight particular initiatives 'on ground'. This was an opportunity to generate discussion, answer questions and receive feedback from various participants.

Summary of feedback

The key issues discussed during this phase of consultation include:

- The retention of heritage buildings and the impact of potential vertical expansion (upper level additions);
- The premise of preferred maximum building heights throughout the activity centre;
- Urban design initiatives to enhance the appearance and function of laneways and public spaces; and
- Key pedestrian, cycling and public transport aspects of the plan.



Consultation feedback

The feedback received from the online survey, email submissions and Walking tour are collated into **key** themes below:

Heritage values

In summary, there was strong support for the retention and restoration of heritage buildings along the Glenferrie Road and High street commercial spines. There were concerns regarding the future development along these streets with residents cautious of potential over-development of contemporary forms at upper levels. It was considered that new forms might potentially erode the valued character of these significant streetscapes. A number of submissions sought the need for clearer design guidelines to define 'sympathetic' additions to heritage forms.

Proposed preferred maximum building heights and setbacks

A variety of submissions were received pertaining to the proposed preferred building heights across the Study Area. In general terms, it was considered the preferred building height of 4 storeys across High Street and Glenferrie Road would result in excessive overshadowing and visual bulk effects. Feedback received also indicated the preference for maximum building height of up to 3 storey, particularly on sites where heritage buildings are to be retained. Furthermore, it was considered that a clearer set of assessment guidelines are required, which demonstrate how developments exceeding 4 storeys should be designed and assessed. Whilst there is no strong objections that increased development heights and density could be contemplated on the right sites, the Structure Plan would need to stipulate the required characteristics, which warrant strategic sites nomination, or where development above 4 storey could potentially be accommodated.

There was support for the preferred maximum 6 storey height designation in areas where it is currently proposed. These heights should also be translated into areas within close proximity to public transport and areas of increased commercial activity.

Location-specific feedback regarding preferred building heights can be summarised as followed;

- The preferred maximum building height of 6 storeys along Station Street (between Soudan, McKinley Avenue and Glendearg Grove) will not adequately allow appropriate transition to residential land to the north. Due to the presence of the intersections and railway embankment, there was preference for the Structure Plan to be amended to allow 4 storeys at this area and maintain 6 storeys where land truly interfaces Dandenong Road.
- Proposing a blanket preferred height limit of 4 storeys along Station Street is considered underwhelming given increasing development potential within this precinct. The preferred height of 6 storeys is warranted having regard to the sites strategic location within close proximity to public transport and commercial activity.
- It is considered that limiting building heights to 4 storeys to the south of Evandale Road along Glenferrie Road could potentially make development unviable. A preferred building height of 5-6 storeys (maximum height of 18-19m) is preferable.
- Encourage mixed-use (shop-top) development of 6-8 storeys along High Street to encourage increased activity and vibrancy within the centre.



Land use

In Summary, submitters were mindful of the impact of expanding commercial, medical and higher-density residential development on existing single dwelling residential land in the Study Area. The Structure Plan needs to consider the impact of ongoing development within areas that are subject to a variety of land uses.

A number of specific land use issues arose from the consultation phase including:

- The car park on the corner of Coonil and Wattletree Roads could be considered as part of the 'Coonil Estate' and not the Cabrini Medical Precinct.
- Council to publicly acquire the Williams Street carpark currently owned by Macro Foodstore on High Street.

At the other end of the spectrum, submissions supported the designation of Wattletree Road as a medical and health precinct, with the acknowledgement that development outcomes in this area will represent a different built form character than the surrounding residential hinterland due to the operational requirements of the hospital.

Furthermore, submissions supported the Draft Structure Plan's direction surrounding the facilitation of place making and the future expansion of Malvern Central. It is submitted that this precinct should be nominated within the Structure Plan as being a key gateway to the Activity Centre from Dandenong Road. It was also acknowledged the forecast growth of 800 to 900 additional dwellings is to be accommodated with the Activity Centre by 2031 and therefore residential development should be accommodated at Malvern Central to enhance the viability of the centre without undermining the amenity of the local area.

Landscape and public realm initiatives

Generally speaking, submissions received were strongly supportive of the Draft Structure Plan with regards to public realm upgrades and streetscape improvements throughout the Study Area. It was accepted that additional way-finding signage is required within the commercial/ retail precinct; however, the implementation of signage should be a part of a holistic approach to avoid creating visual clutter.

Feedback also suggested that the Study Area is considered to be at capacity with regards to public open space and the Structure Plan should therefore show greater emphasis towards public open space for existing and future residents. Additional public open space should offer various programs to cater for a wide range of age groups. This could include 'pocket playgrounds' for children. Overall, existing and proposed public spaces should ensure pedestrian safety and passive surveillance.

Other issues raised include the need to enhance pedestrian movement throughout the Study Area for people with various levels of mobility is also a considerable concern for residents, including the need for additional accessible public toilet facilities. The need for additional 'pause points' along Glenferrie Road and High Street was also raised which can also accommodate additional bike parking. This could also work in conjunction with the implementation of community art and installations.

Specific suggestions relating to public realm improvement include;

- A sign at Malvern Station to direct pedestrian movement to Glenferrie Road;
- Facilitate pedestrian access to the east of Malvern Station;
- A pedestrian crossing adjacent to the Glenferrie Road and Winter Street (at the entrance of Coles).



Car parking and traffic management

Ensuring the Structure Plan considers opportunities to manage vehicle congestion, public transport as well as integrated pedestrian and cycling networks was a key issue identified within the consultation feedback. It was acknowledged that additional public car parking within the Study Area is required in the immediate future, with concerns pertaining to the impact of future development within an already congested Activity Centre.

Other feedback sought for the removal of car parking along Glenferrie Road and High Street to effectively provide safe and convenient pedestrian and cycle networks as well as improve upon the existing tram network along these major roads. Feedback suggested that tram 'super stops' along these routes could significantly improve access and movement throughout the Study Area.

Overall, the consultation feedback indicated that further information and research is required regarding the provision of car parking and public transport upgrades for new residential and commercial development across the study area.

What do people like about the Draft Structure Plan?

In summary, online feedback received suggest that residents are particularly satisfied with the acknowledgement of heritage values and the intention to retain heritage buildings. Consideration towards the enhancement and creation of community hubs as well as improvements to public open spaces and streetscapes is also an admirable feature of the Draft Structure Plan. Residents also commend the strong focus on local business opportunities to stimulate the economy along High Street and Glenferrie Road. Overall, feedback suggested that Structure Plan should be seen as a Community focused document with clear references to the effective utilisation of the Study Area for residents, visitors and employees etc.

What do people dislike about the Draft Structure Plan?

Overall, the key concerns from the online feedback relate to car parking, traffic congestion, preferred building heights and pedestrian/ cycle networks. It is considered the Structure Plan should ensure that new development provides sufficient off-street parking as well as additional car parking in the immediate future for visitors and shoppers. The preferred maximum height of building nominated throughout various precincts is considered too high. In most cases, it is accepted that three storeys should be the maximum preferred height or upper levels should be significantly setback. Further consideration towards pedestrian and cycle movement across the Study Area is required as well as the impact of traffic congestion as a result of the proposed preferred maximum building heights and increased dwelling density.



Other

The Key Themes - Framework Plan provided in Council's Community Consultation brochure inaccurately illustrated the Precinct Boundary as encompassing land to the south of Derril Avenue, extending to Thanet Street. It is noted that this plan was inadvertently misinterpreted during the compiling of consultation documentation, which gave rise to questions and concerns about exactly what land close to Cabrini Hospital was in or out of the Structure Plan boundary. The Draft Key Themes Plan prepared by the Structure Plan Consultant correctly illustrates the proposed Activity Centre Boundary including Cabrini Medical Precinct Boundary.

A Legend error was identified on page 15 'Economic Activity Map' incorrectly identifies Cabrini Hospital as a 'retail anchor'. This item has been rectified and identifies the land as an 'employment node'.



GLENFERRIE ROAD HIGH STREET STRUCTURE PLAN

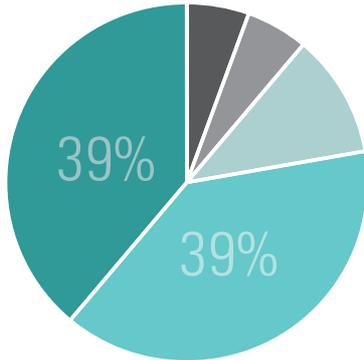
FAST FACTS

ONLINE COMMUNITY SURVEY

18 PARTICIPANTS

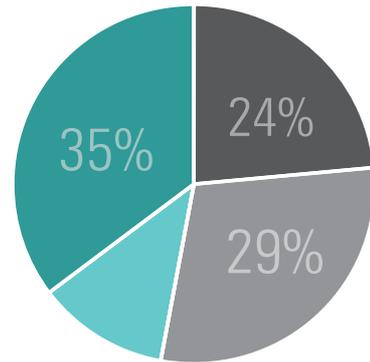


RETAIN HERITAGE BUILDINGS, AND ALLOW SYMPATHETIC ADDITIONS AT UPPER LEVELS

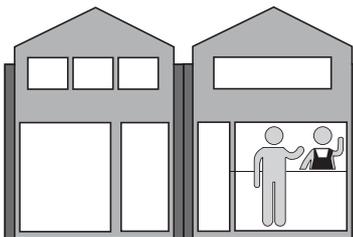
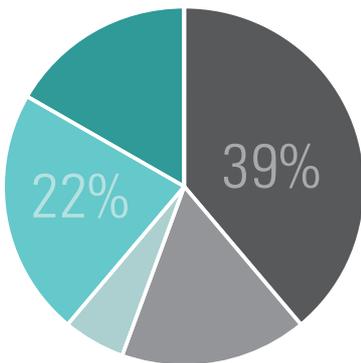


DIRECT POTENTIAL TALLER BUILT FORM OF A PREFERRED MAXIMUM HEIGHT OF 5 TO 8 STOREYS FOR MALVERN CENTRAL

AND THE KEY STRATEGIC REDEVELOPMENT SITE SOUTH OF MALVERN STATION

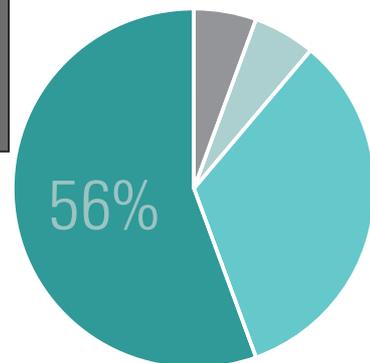


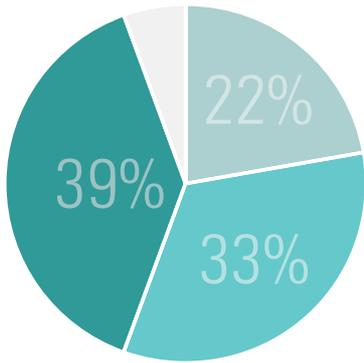
ENSURE THE PREFERRED MAXIMUM HEIGHT OF BUILDINGS THROUGHOUT THE ACTIVITY CENTRE IS PREDOMINANTLY 4 STOREYS



ACTIVELY ENCOURAGE EMPLOYMENT, COMMERCIAL ACTIVITY

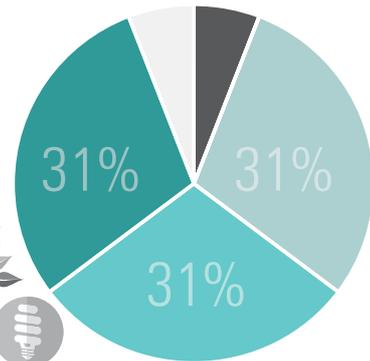
AND RETENTION OF SMALL SCALE BUSINESS AND BOUTIQUE SHOPS THROUGH PLANNING POLICY





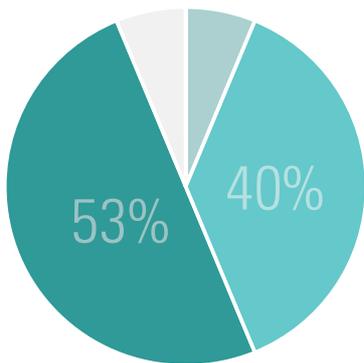
INVESTIGATE POTENTIAL FOR
TEMPORARY EVENT SPACES
AT KEY LOCATIONS

ENHANCE AND UPGRADE THE APPEARANCE OF LANEWAYS
TO REAR OF COMMERCIAL PROPERTIES WHERE THEY PROVIDE
THROUGH LINKS AND ACCESS TO PUBLIC CAR PARKS

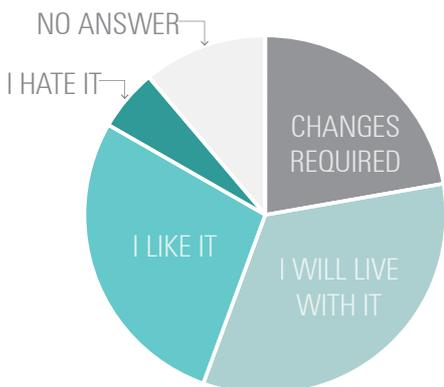
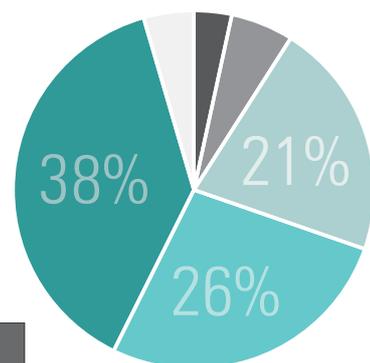


INVESTIGATE POTENTIAL

PUBLIC REALM UPGRADES
AND IMPROVEMENTS AT KEY LOCATIONS



TO WHAT EXTENT DO YOU SUPPORT THE FOLLOWING KEY
PEDESTRIAN AND CYCLING
ASPECTS OF THE PLAN?



OVERALL LEVEL OF COMFORT WITH STRUCTURE PLAN



APPENDIX 1

CONSULTATION BROCHURE

GLENFERRIE ROAD AND HIGH STREET ACTIVITY CENTRE DRAFT STRUCTURE PLAN – HAVE YOUR SAY



Planning for the centre now and in the future

Dear Resident,

Stonnington City Council has consulted widely with people who live, work and visit the Glenferrie Road and High Street area to prepare a Draft Structure Plan.

This is your opportunity to view and provide feedback before the plan is finalised and translated into planning controls in the Stonnington Planning Scheme.

The Draft Glenferrie Road and High Street Structure Plan sets out a shared long-term vision for future growth and change in the area. The Plan aims to facilitate positive change in relation to the use of land, transport, open space and public realm. It will also cover neighbourhood character, development, building height and density, economic development and the appearance of the precincts.

This Draft Structure Plan is based on your earlier feedback. I hope you take the time to visit Council's website, complete a survey, attend the workshop or one of the walking tours. Council is seeking feedback on the Draft Structure Plan from 17 March to 13 April 2015.

This brochure aims to provide you with information on how you can view the Draft Structure Plan and provide feedback.

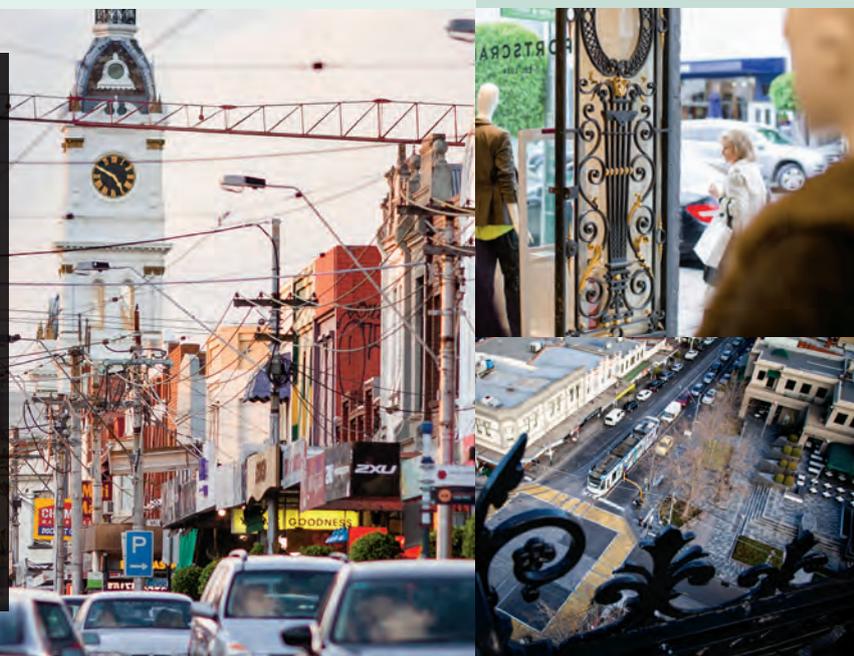
For more information, please visit: www.stonnington.vic.gov.au/glenferriehigh



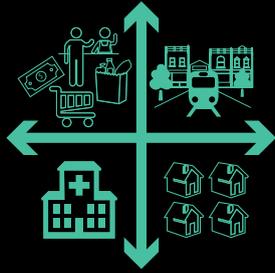
Cr Melina Sehr
Mayor,
City of Stonnington

Why do we need a Structure Plan?

Defined by its two intersecting main streets, Glenferrie Road and High Street Activity Centre is an important economic/shopping hub in Melbourne's inner south east. It provides a range of retail and hospitality services to the immediate suburbs as well as speciality retailing with a regional catchment. Within the Melbourne metropolitan context this area is expected to be the focus of future development, both residential and non-residential. The Structure Plan focuses on the commercial zones of the centre, broadly located along the main strip shopping centre frontages of Glenferrie Road and High Street.



What is the Vision? In 2040, Glenferrie Road and High Street Activity Centre will be:



A thriving shopping centre, serving the day to day needs of the local community, coupled with a range of specialist retailers serving a wider regional area.



A vibrant, inviting, attractive and clean centre with well-designed 'green' streetscapes, pedestrian connections and community gathering nodes.

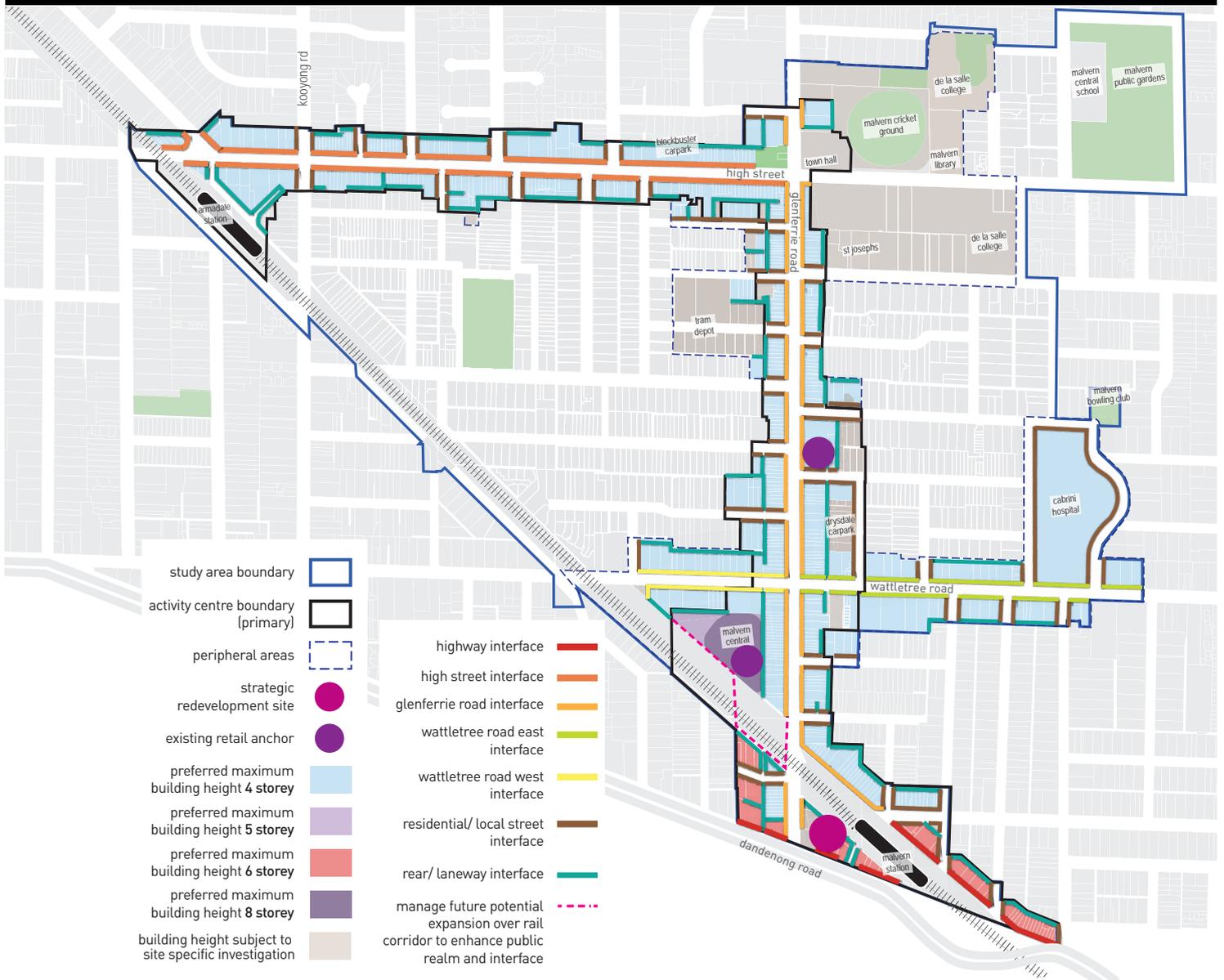


A centre where new development respects and enhances the form and scale of heritage buildings, which retain prominence.



A centre for localised employment and housing within the commercial spines whilst protecting the valued residential hinterland.

Built form plan – heights and setbacks



What you told us

You said

Our response

Dandenong Road Gateway lacks character and would benefit from a gateway treatment and improved amenity.

Enhance the precinct to the southern side of the station as the 'gateway' to the centre, including potential for a landmark building.
Direct higher intensity built form along Dandenong Road.

Malvern Central to accommodate a potential taller built form, expanded range of commercial uses and enhancing links to Glenferrie Road.

Encourage high quality mixed use precinct with potential higher built form.
Potential additional link to Malvern Central from Glenferrie Road.

Development over the railway line while preserving the heritage value of **Malvern Station**, Station Street and Claremont Avenue village.

Investigate improvements to pedestrian movement and amenity between tram stops, railway station and key destinations.

Improved accessibility to the station and better bicycle parking.

Establish community focused 'activity node' to the north of Malvern Station.

Armadale Station/ Kings Arcade: potential for more vibrancy and higher quality public realm.

Investigate creation of pedestrian and vehicular 'shared space' along Morey Street and Kingsway.

A shared space for pedestrians and cars to improve environment for pedestrians; improved car parking conditions, way-finding and landscaping.

Improve defined pedestrian movement between tram stops, railway station and key destinations.

Wattletree Road: support for health precinct but interface with residential areas needs to be managed, heritage value protected and expansion of health services should be limited to Wattletree Road.

Wattletree Road (east of Glenferrie Road) to include maximum building heights and rear setbacks. Side setbacks to accommodate landscaping.

Car parking for visitors and staff is a key issue.

Wattletree Road (west of Glenferrie Road): Active frontages at ground level with weather protection canopies. Rear setback ground level to ensure functional use of rear laneway.

Tram depot precinct presents some new opportunities. Protection of the heritage listed buildings was strongly supported.

Greater public access to area for temporary community events.

Landscaping and public realm improvements at Glenferrie Road.

Improved pedestrian and cycle connections through and around the precinct.

Malvern Town Hall and Civic Precinct: improved way-finding, enhanced pedestrian access to and through the site (e.g. creating a walking circuit), and improve landscaping.

Improve the integration of Malvern Cricket Ground and Malvern Library with the Town Hall and Civic Precinct.

Improve defined pedestrian movement between tram stops, railway station and key destinations.

Malvern Square (corner Glenferrie Road and High Street): incorporate more green into the landscape with shady trees/creepers/lawn.

Encourage activities within the existing Malvern Square to highlight it as a gathering space in the centre.

Use as a market space, making the site more level so that it can be used as a congregation space with additional public seating.

All **car parks should be retained** and new developments should include sufficient parking.

Retention of public car parking spaces with public realm improvements to improve amenity.

Improve the safety and amenity of car parks through way-finding, signage, enhanced lighting, landscaping, weather protection and public toilets.

Improve the quality of north-south pedestrian, bicycle and vehicle access through the at-grade car parks.

Redesign the intersection between Llaneast Street car park and Wattletree Road to address road safety issues.

How to find out more and have your say?

Council is undertaking consultation from 17 March to 13 April 2015. This provides the opportunity to review the Draft Structure Plan and comment on the proposed initiatives.

Register to participate in a workshop or walking tour

COMMUNITY WALKING TOUR 1:

Saturday 21 March 2015 at 10am–12pm

Meeting point at Malvern Town Hall, outside the Service Centre entrance (on High Street)

COMMUNITY WALKING TOUR 2:

Tuesday 24 March 2015 at 10am–12pm

Meeting point at Malvern Town Hall, corner Glenferrie Road and High Street

COMMUNITY WORKSHOP:

Malvern Town Hall, corner Glenferrie Road and High Street (entry via Glenferrie Road)

Tuesday 24 March 2015 at 6–8pm

Call 8290 1395 to register

Fill out a survey

Fill out or download a survey from www.stonnington.vic.gov.au/glenferriehigh

Collect a hard copy survey from Council Service Centres or libraries.

Provide feedback

- Visit www.stonnington.vic.gov.au/glenferriehigh
- Write to City of Stonnington, PO Box 21, Prahran 3181
- Email strategicplanning@stonnington.vic.gov.au

More information

To view the Draft Structure Plan visit Council's Service Centres

- Planning Counter, Prahran Town Hall, corner Chapel and Greville Streets
- Service Centre, Malvern Town Hall, corner Glenferrie Road and High Street

Or visit www.stonnington.vic.gov.au/glenferriehigh

Or call 8290 1395

Next steps

Council will consider the feedback before endorsing the final Plan.

The Draft Structure Plan will inform the preparation of new planning controls through a Planning Scheme Amendment which will be subject to a formal consultation process.

MULTILINGUAL INFORMATION Call the Stonnington Community Link. A multilingual telephone information service.

General Enquiries	8290 1333
Mandarin 普通話	9280 0730
Cantonese 廣東話	9280 0731

Greek	Ελληνικά	9280 0732
Italian	Italiano	9280 0733
Polish	Polski	9280 0734

Russian	Русский	9280 0735
Indonesian	Bahasa Indonesia	9280 0737
All other languages		9280 0736

CITY OF STONNINGTON

T: 8290 1333

F: 9521 2255

E: council@stonnington.vic.gov.au

PO Box 21 Prahran, Victoria 3181

SERVICE CENTRES

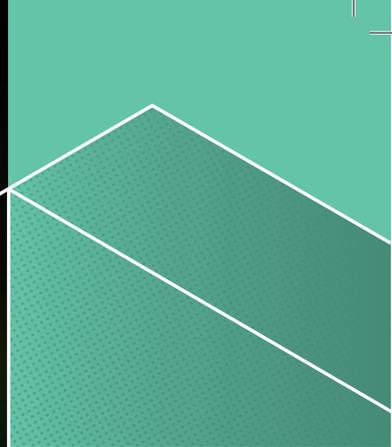
Corner Glenferrie Road and High Street, Malvern

Corner Chapel and Greville Streets, Prahran

293 Tooronga Road, Malvern

www.stonnington.vic.gov.au





GLENFERRIE ROAD AND HIGH STREET ACTIVITY CENTRE DRAFT STRUCTURE PLAN



FRAMEWORK PLAN – KEY THEMES

What you told us

You said

Our response

Allow for Mobility and Safe Access

Streets are ‘community hubs’ where pedestrian access should be prioritised.

Conflict noted between pedestrians, cars, bikes and trams on Glenferrie Road.

Need to improve the cycling routes by creating dedicated bike lanes.

Identify opportunities for public realm upgrades to streetscape, landscaping, shared spaces, way finding, pedestrian and working with VicRoads to give cycle prioritisation.

Potential for additional pedestrian crossings along Glenferrie Road, including: Malvern Station, Union Street and Malvern Town Hall.

Green and For Everyone

More **open space** is required.

Need for general streetscape improvements.

Potential pedestrian focused public space at intersections of Union Street and Coldblo Tram Depot with Glenferrie Road.

Improved integration of open space adjacent to Malvern Town Hall (Malvern Cricket Ground).

Facilitate Place Making / Celebrate Heritage and Embrace the Future

Retention of **heritage** is an important element.

New buildings to reflect scale of surrounding buildings and consideration to overshadowing and overlooking.

Greatest opportunity for development is infill sites with no heritage value.

Retain heritage fabric and balance the opportunity for modest and sympathetic upper floor redevelopment.

Setback upper level development above heritage fabric.

New built form to be setback from sensitive interfaces.

Contribute to Local Economy

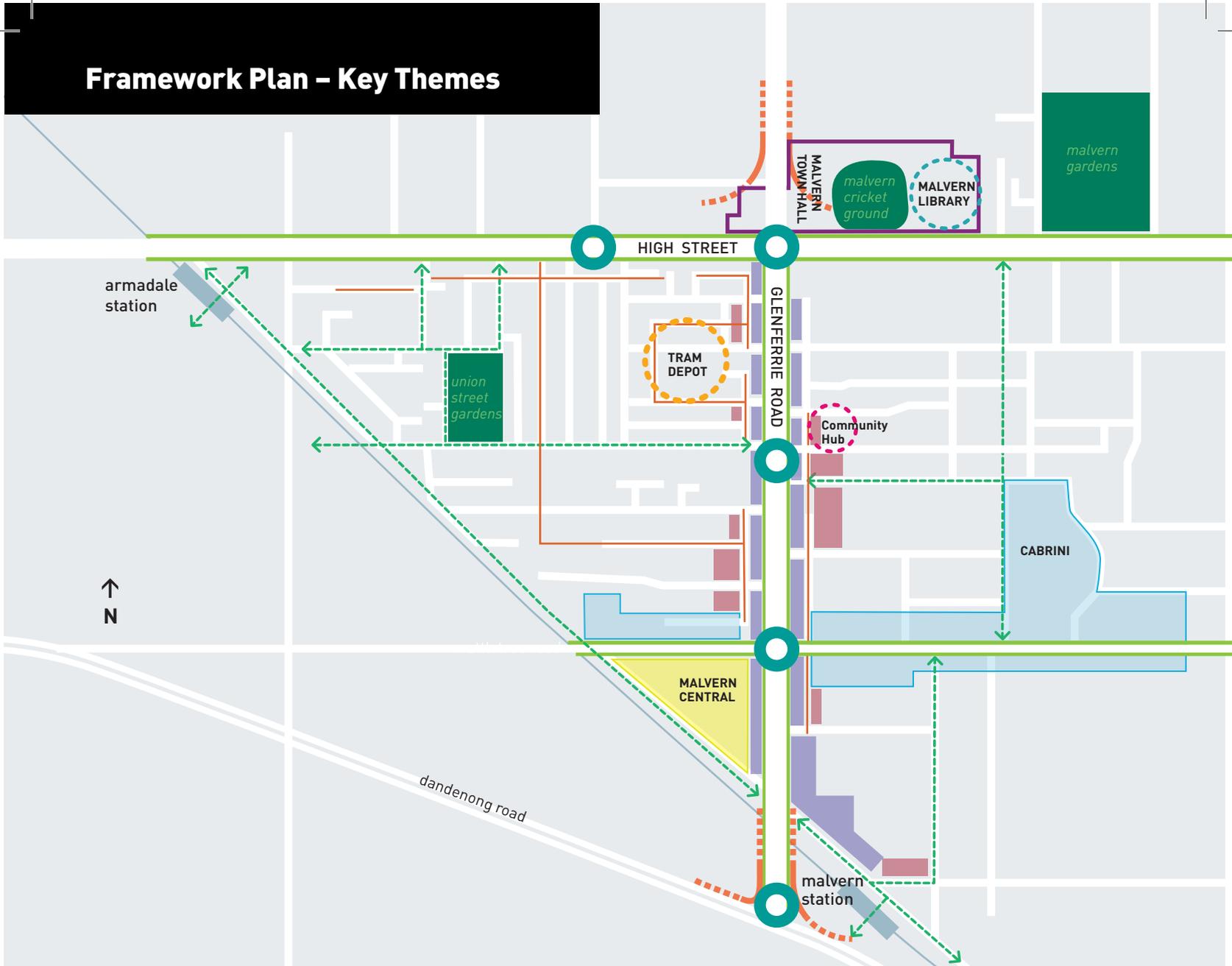
Support for **mixture of uses**, while balancing impact on existing residential amenity and not displacing commercial uses.

Development and use of land should build a sense of community through street life and activity, including support for night time uses (ensuring appropriate location and sensitive to amenity).

A need was identified for additional public toilets.

Strengthen local and regional role of the centre, focusing on the unique differences and combined strengths of Glenferrie Road and High Street.

Framework Plan – Key Themes



Contribute to Local Economy

- Acknowledge **High Street** as a regional commercial and retail destination
- Strengthen local and retail role of **Glenferrie Road**

 Provide guidance to possible future expansion of **Malvern Central**

 Consolidate medical and health facilities along **Wattletree Road**

 Encourage entrepreneurship, creative industry and special focal points in the Centre

Facilitate Place Making

- Establish active and attractive street presentations along **Glenferrie Road** and **High Street**
- Establish clear urban form and design guidance through key strategic development areas
-  Establish new and strengthen clearly defined gateway and entries into the Centre
-  Consolidate and strengthen community node around Malvern Library and Malvern Cricket Ground

Allow for Mobility and Safe Access

- Establish clear streets hierarchy and users' priorities
-  Encourage and facilitate more sustainable modes of travel within and around the Centre
-  Establish clearly defined pedestrian linkages and manage road crossing points

Celebrate Heritage and Embrace the Future

- Protect and enhance significant heritage fabric along **Glenferrie Road** and **High Street**

 Protect and enhance the laneway network within the Centre

 Encourage development to provide a secondary address to rear lanes behind Glenferrie Road

 Encourage development to provide a secondary address to the rear at commercial interfaces

Green and For Everyone

-  Consolidate and strengthen the community node around the Civic Space
-  Strengthen public meeting space and vegetation at and around important junctions
-  Improve access to all public open spaces
-  Establish potential event spaces at key sites
-  Establish additional vegetation in the public realm
-  Potential community hub and public open space



APPENDIX 2

ONLINE QUESTIONNAIRE TEMPLATE

Glenferrie Rd and High St Draft Structure Plan Survey

Privacy Statement

Information provided by you will only be used for the purposes for which it was collected. Personal information provided will not be disclosed to any outside organisation or third party. Individuals about whom "personal information" is provided in this Survey may apply to the City of Stonnington's Privacy Officer on telephone number 8290 1333 for access to or correction of the information. It is requested that surveys be completed by people aged 15 years and over only.

To complete this survey, please:

- Tick the box or circle adjacent to your selected response
- Return your completed survey in the reply paid envelope provided

	Strongly don't support	Don't support	Neutral	Support	Strongly support	Can't say	What are your reasons for the level of support?
Actively encourage employment, commercial activity and retention of small scale business and boutique shops through planning policy.	<input type="radio"/>	<input type="radio"/>	<input type="radio"/>	<input type="radio"/>	<input type="radio"/>	<input type="radio"/>	
Enhance and upgrade the appearance of laneways to rear of commercial properties where they provide through links and access to public carparks (including new development fronting laneways).	<input type="radio"/>	<input type="radio"/>	<input type="radio"/>	<input type="radio"/>	<input type="radio"/>	<input type="radio"/>	
Investigate potential for temporary event spaces (markets, special events etc.) at key locations including: Kings Arcade/ Armadale Station, Coldblo Tram Depot and Station Street/ Malvern Station.	<input type="radio"/>	<input type="radio"/>	<input type="radio"/>	<input type="radio"/>	<input type="radio"/>	<input type="radio"/>	
Investigate potential public realm upgrades and improvements (e.g. new trees, seats etc.) at locations including: the Malvern Town Hall Civic Precinct, Armadale Station Precinct, Malvern Station Precinct, Malvern Central Precinct, Coldblo Tram Depot and Drysdale Street Carpark.	<input type="radio"/>	<input type="radio"/>	<input type="radio"/>	<input type="radio"/>	<input type="radio"/>	<input type="radio"/>	

Q2: To what extent do you support the following key **pedestrian and cycling** aspects of the plan?

	Strongly don't support	Don't support	Neutral	Support	Strongly support	Can't say	What are your reasons for the level of support?
Improved and simplified signage to key places.	<input type="radio"/>	<input type="radio"/>	<input type="radio"/>	<input type="radio"/>	<input type="radio"/>	<input type="radio"/>	
Improved pedestrian crossings.	<input type="radio"/>	<input type="radio"/>	<input type="radio"/>	<input type="radio"/>	<input type="radio"/>	<input type="radio"/>	
Creation of community gathering spaces at side street intersections.	<input type="radio"/>	<input type="radio"/>	<input type="radio"/>	<input type="radio"/>	<input type="radio"/>	<input type="radio"/>	
Public realm guidelines for street furniture and bicycle parking facilities.	<input type="radio"/>	<input type="radio"/>	<input type="radio"/>	<input type="radio"/>	<input type="radio"/>	<input type="radio"/>	
Working with VicRoads for on-road cycle lanes.	<input type="radio"/>	<input type="radio"/>	<input type="radio"/>	<input type="radio"/>	<input type="radio"/>	<input type="radio"/>	

Q3: What do you like **most** about the Draft Structure Plan?

Q4: What do you like **least** about the Draft Structure Plan?

Q5: What is your overall level of comfort with the Draft Structure Plan?

I love it - I am 100-80% comfortable with the draft plan, only minor tweaks, if any, are required. I am very happy

I like it - I am 80-60% comfortable with the draft plan. Some small changes required but I am mostly happy

I will live with it - I am 60-40% comfortable with the draft plan. Some significant changes are required but I can accept it as it is

I am uncomfortable with it - I am 40-20% comfortable with the draft plan. There are lots of changes required

I hate it - I am 20-0% comfortable with the draft plan. It needs an overhaul I can't see it working at all

Q6: Any other comments about the Draft Structure Plan?

Q7: Please nominate the gender description that you most identify with?

- Female
- Male
- Other _____

Q8: What is your age range?

- <18
- 18-24
- 25-34
- 35-49
- 50-69
- 70+

Q9: What is your interest in and relationship to the Structure Plan area? (Please tick as many as apply).

- Local resident
- Business Owner/ Trader: (please specify business general location) _____
- Landholder
- Developer
- Employee
- Student
- Visitor from elsewhere in Stonnington/ Melbourne
- Other _____

Q10. Please state your post code.

Postcode _____

Q11. If you would like to receive updates regarding the Draft Structure Plan please provide your contact details below

Name _____

Email _____

This completes the survey. Thank you for your time.



ESSENTIAL ECONOMICS

Glenferrie Road – High Street Activity Centre Structure Plan

Economic Technical Report

Prepared for

City of Stonnington

by

Essential Economics Pty Ltd

September 2014

Authorship

Report stage	Author	Date	Review	Date
Background Report	Andrew Rossiter Sean Stephens	26 September 2014	John Henshall	29 September 2014

Disclaimer

Although every effort has been made to ensure the accuracy of the material and the integrity of the analysis presented herein, Essential Economics Pty Ltd accepts no liability for any actions taken on the basis of the contents of this report.

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ABN 92 079 850 427

Project Reference: 14005

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INTRODUCTION

Background

The *Plan Melbourne* metropolitan strategy classifies Glenferrie Road-High Streets as an Activity Centre for the purposes of metropolitan planning. Activity Centres are defined as important locations for the future development of retail, commercial, community and residential infrastructure. Through the removal of retail floorspace and office caps, activity centres may grow unrestricted. A key direction of Plan Melbourne is the '20-minute neighbourhood'. Activity centres, such as Glenferrie Road-High Street, play an important role in enabling 20-minute neighbourhoods by providing access to a wide range of goods and services in centres that are planned and coordinated by local governments.

In this context, Structure Plans are to be developed at the municipal level for Activity Centres in metropolitan Melbourne in order to guide development in these centres over the next 20 years in a manner which is consistent with the principles of *Plan Melbourne*.

As part of this structure planning process, the Glenferrie Road-High Street Structure Plan is now being prepared for the City of Stonnington. As input to the Structure Plan, economic analysis is required in order to understand the role of the Activity Centre from a retail, commercial and residential perspective, and to identify opportunities for growth and change in these sectors in a way which is consistent with the objectives for metropolitan planning.

This economic analysis is therefore intended to guide and inform the Structure Planning process for the Glenferrie Road-High Street Activity Centre.

Terms and Definitions

Retail Activity

Retail activity is defined in this report as the *Retail Trade* industry division according to the industry classifications used by the Australian Bureau of Statistics (ABS) in the Australian and New Zealand Standard Industry Classification 2006 (ANZSIC2006), but modified to exclude:

- Motor vehicle and motor vehicle parts retailing
- Garden supplies retailing
- Marine equipment retailing
- Non-store retailing.

This definition is consistent with other approaches in undertaking retail-economic analysis in Australia.

Retail Categories

Retail floorspace and retail expenditure is divided into three broad product categories and a number of sub-categories. These categories and sub-categories are defined as follows:

- **Food**, which includes all household spending on take-home food, groceries, liquor, cafes, restaurants and take-away food outlets. Take home food spending is usually directed to retail stores including supermarkets, fresh food stores, bakeries, butchers, etc.
- **Non-food retailing**, including spending on apparel, homewares, bulky items, and other leisure and general merchandise
- **Retail services**, which comprises household spending on services such as hair dressing, dry cleaning, video hire, and optometry.

1 CONTEXT

This Chapter presents a general description of Glenferrie Road-High Street activity centre, a summary of background documents relevant to the centre, and commentary on key market trends of relevance.

1.1 Glenferrie Road-High Street Activity Centre Description

Glenferrie Road-High Street Activity Centre (Glenferrie Road-High Street) is a street-based activity centre, located approximately 7km south-east of the Melbourne CBD, in the City of Stonnington. The centre is 'transport rich' in terms of access to road, rail, tram and active forms of transport. Within the centre a high standard of pedestrian activity and amenity exists.

Glenferrie Road-High Street includes Glenferrie Road (from Dandenong Road to High Street, including Malvern Town Hall,) as well as High Street (from Malvern Public Gardens to the Armadale Rail Station bridge).

Glenferrie Road-High Street is characterised by a wide diversity of retail and commercial land uses across an extensive area. The distance from the intersection of Glenferrie Road and Dandenong Road, to Armadale train station via Glenferrie Road and High Street is almost 2km. Glenferrie Road and High Street are at a 90 degree angle and the intersection of both roads is the only functional link between each commercial strip.

The centre is almost entirely street-based, with the only enclosed components being the Malvern Central shopping centre and a number of arcades primarily linking to rear carparking. Despite its relatively large size a sense of vibrancy and activity is retained throughout virtually the all centre. The overall height and scale of buildings remains relatively modest relative to development occurring at nearby centres, including Chapel Street, Caulfield and Elsternwick.

The Cabrini Hospital is located a short distance east of the Glenferrie Road strip and has relatively strong influence on the centre. Other important land uses in the centre include the municipal and administrative functions (Malvern Town Hall, City of Stonnington offices, Malvern police station) that are located at the intersection of Glenferrie Road and High Street. These uses form an important 'anchor' to the intersection of the Glenferrie Road and High Street strips that serves to increase the degree of integration between the two.

1.2 Relevant Background Documents

Plan Melbourne 2014

The *Plan Melbourne* metropolitan planning strategy, released in October 2013, will guide Melbourne's housing, commercial and industrial development to 2050. With regard to activity centre planning, the Strategy no longer distinguishes between Principal Activity Centres (PACs) and Major Activity Centres (MACs). Therefore, Glenferrie Road-High Street centre is now classified simply as an 'Activity Centre'.

Through the removal of retail floorspace and office caps, activity centres may grow unrestricted. A key direction of Plan Melbourne is the '20-minute neighbourhood'. Activity centres, such as Glenferrie Road-High Street, play an important role in enabling 20-minute neighbourhoods by providing access to a wide range of goods and services in centres that are planned and coordinated by local governments.

City of Stonnington Activity Areas Economic Analysis 2013

The *City of Stonnington Activity Areas Economic Analysis 2013* was prepared by HillPDA and identifies the existing role and function of all activity areas within the municipality. In regard to the Glenferrie Road-High Street Activity Centre, the analysis outlines the key characteristics of the centre, together with its floorpace supply and key activity generators, as well as a broad trade area definition.

The analysis also includes forecast demand for retail floorpace within the City's existing activity areas. It found a current undersupply of 13,000m² of retail floorpace in Major Activity Centres and Neighbourhood Centres. The analysis also estimates that over the period to 2031, these centres will require an additional 40,000m² of retail floorpace, at an average rate of +2,000m² per annum. The study does not nominate specific centres to accommodate the additional retail floorpace.

Building Prosperity: Economic Development Strategy 2012-2016

The *Building Prosperity: Economic Development Strategy 2012-2016* for the City of Stonnington recognises that the City's precincts are a hub for retail and business activity and that the high profile precincts (including Glenferrie Road) are strongly linked to the identity, character, amenity and liveability of the city. A key strategy is "to maintain attractive vibrant precincts which are embraced by residents and visitors and where businesses thrive". As such, a key action of the Strategy is the development of a Structure Plan for Glenferrie Road, Malvern and High Street, Armadale. The Strategy also emphasises the importance of tourism to the City's economy with shopping, dining, entertainment, and arts and culture activities able to attract visitors and generate employment.

1.3 Key Industry Trends

Several industry trends are particularly relevant to retail, commercial and residential land use outcomes for Glenferrie Road-High Street.

Retail Trends

Retailing is one of the most dynamic sectors of the Australian economy, with the industry continually evolving to reflect new formats, products and consumer preferences. Understanding and responding to changes in the retail sector are important components of activity centre planning.

A number of retail trends are underway that need to be considered in assessing the future retail development opportunities in Glenferrie Road-High Street. These trends relate to factors such as the structure and size of the retail industry, demographic and behavioural shifts, the emergence of new forms of retailing that respond to these changes, and technical innovations that will also influence future retail outcomes.

Industry Structure

In terms of industry structure, the trends suggest an ongoing outlook in which smaller independent operators continue to lose market share to the retail 'giants' and major national chains (such as Coles, Woolworths and the major bulky goods and homemaker chains), as well as losing market share to franchises in the food and specialty retail sector. This situation effectively means that for many street-based centres, the presence of these major chains contributes to a centre's ability to attract visitation and spending.

For Glenferrie Road-High Street, this situation creates the challenge of maintaining and encouraging major anchor retailers and national brand operators seeking to locate in the centre in order to attract regular shoppers. It is also necessary to maintain the important point of difference relating to independent specialty retailing and other cultural characteristics that are widely recognised and valued features of Glenferrie Road-High Street, and where the centre as a whole has a significant advantage compared with other activity centres in greater Melbourne.

Internet Retailing

Another factor relevant to Glenferrie Road-High Street is the growth of internet retailing and the implications this has for traditional shopfront retailing. At present, an estimated 4-6% of retail sales in Australia are sourced via the internet, with growth in internet sales of between 10% to 15% per annum over the past few years. This rapid growth has had a major influence on particular retail categories, including books, apparel, music and other home entertainment, all of which are well-suited to online selling. In contrast, product categories such as fresh food and cafe/restaurant dining have had little or no impact from internet-based competition.

Demographic and Behavioural Shifts

One trend with particular relevance for Glenferrie Road-High Street is the growth of 'recreational retailing' or the popularity of shopping as a form of entertainment and social interaction. In Melbourne, centres such as Chapel Street, Chadstone, Bourke Street Mall, and Bay Street (among others) are now seen as leisure and entertainment destinations, as much as locations for retail and shopping. Glenferrie Road-High Street is well-placed to cement its reputation as one of Melbourne's pre-eminent leisure shopping destination.

Office Trends

Recent trends have seen office development occurring increasingly in several key markets, predominantly the CBD and City Fringe, as many suburban commercial tenants migrate to the CBD and surrounds. According to recent research by CBRE and Colliers International, this trend is likely to continue with significant tenant moves scheduled over the medium term.

At the suburban level the research also indicates that the suburban market is becoming increasingly two-tiered with development not occurring uniformly across suburban markets as once expected. The Inner-East office market (within which Glenferrie-Road-High Street is situated, albeit in a highly competitive environment) is one of the most resilient suburban markets, mainly due to:

- Proximity to the CBD,
- Available white-collar workforce,
- Strong transport links, and
- Proximity to amenities including retail, cultural and community facilities.

Additionally, ongoing trend of consolidating existing office sites in the inner east into residential development sites is contributing to falling office vacancy rates, as limited office development is coming on stream. Having said this, the highly competitive nature of the office market is likely to be even stronger in view of current economic uncertainty and the strong competition that is emerging to attract any potential large-scale tenants.

Residential Trends

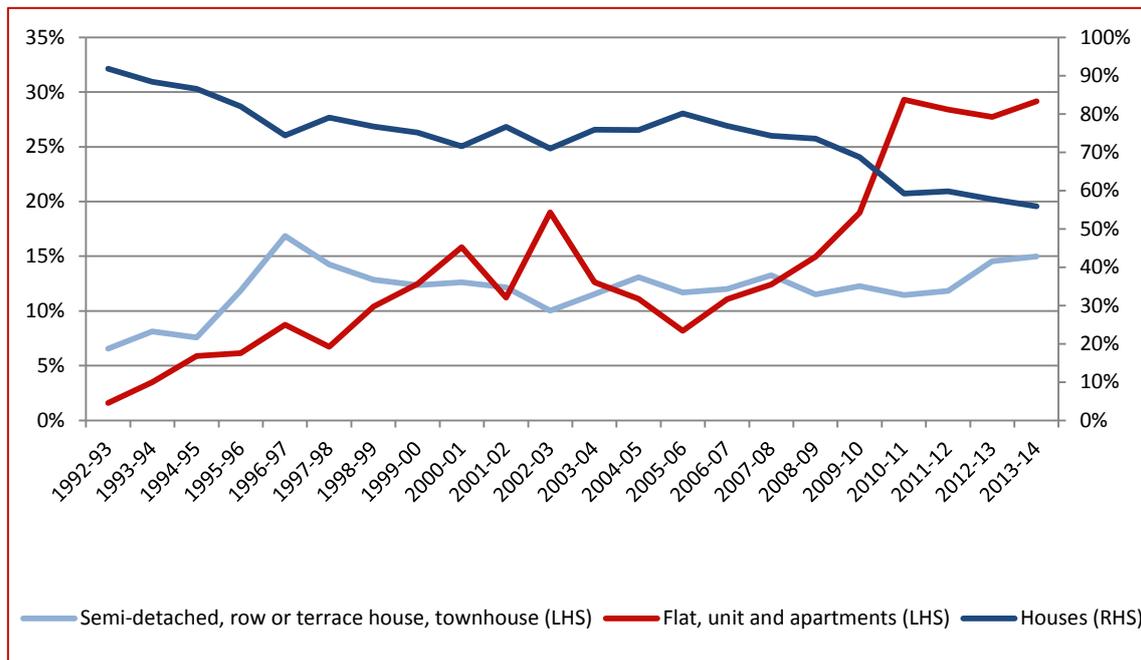
Nationally, the past 20 years or so has seen a significant shift the types of residential buildings being built. Overall, an increased demand for medium and higher-density housing is occurring in inner-middle ring metropolitan areas, as public attitudes are becoming more positive towards the benefits of urban living, including greater amenity as well as the affordability of higher-density living.

In Victoria, detached housing remains the predominant type of dwelling being built, however its share of total dwelling approvals has fallen consistently from 92% in 1992-93, to 56% in 2013-14, as shown in Figure 1.1. This is according to building approvals data sourced from ABS.

During the same period, medium to high-density residential building approvals has increased from just 8% of total dwelling approvals in 1992-93, to approximately 44% in 2013-14, including:

- **Flat, unit or apartment approvals** which have increased from 2% of total dwelling approvals in 1992-93, to approximately 29% in 2013-14.
- **Semi-Detached housing approvals** which increased from 7% of total dwelling approvals in 1992-93, to approximately 15% in 2013-14.

Figure 1.1: Type of Structure as a Share of Residential Building Approvals, 1992-93 to 2013-14, Victoria



Source: ABS, Building Approvals, Australia (Cat No: 8731.0)

The growing market acceptance of high-density residential development is most evident in Melbourne. Along with strong rates of population growth, and general planning policy support for increased residential densities, apartments have become an increasingly important part of the residential sector. This is discussed in greater detail in Sections 2 and 3 of this report.

2 GLENFERRIE ROAD–HIGH STREET TODAY

This Chapter provides a general overview of the current role and operation of Glenferrie Road-High Street Activity Centre. It includes an summary of the results of a detailed shopfront tenancy audit, as well as a description of the commercial office, housing and other infrastructure situated within the centre.

2.1 Retail and Commercial Overview

Total retail floorspace provision in the Glenferrie Road-High Street Activity Centre is estimated at approximately 70,000m², according the *City of Stonnington Activity Areas Economic Analysis 2013*.

In terms of total retail floorspace provision, Glenferrie Road-High Street is a relatively large centre compared with nearby centres such as Camberwell (58,000m²), Carnegie (28,500m²) and St Kilda (45,000m²). However, Glenferrie Road-High Street lacks the size of the nearby Chapel Street (150,000m²) and Chadstone (155,000m²) centres that have a scale of metropolitan importance.

The Malvern Central shopping centre accounts for 20% of the total retail floorspace in Glenferrie Road-High Street, and is the single most important generator of retail sales and visitation.

Commercial office floorspace in Glenferrie Road-High Street comprises approximately 30,000m² of floorspace, although the centre is not a significant office location in terms of the inner city office market.

The Glenferrie Road and High Street strips have very different business mixes. Glenferrie Road has a more traditional mix of businesses, with an emphasis on meeting the day-to-day needs of local residents and visitors. In contrast, the High Street strip includes a large provision of specialised bridal and fashion boutiques, antique stores and high-end fashion and beauty shops.

The vacancy rate for Glenferrie Road-High Street is estimated at approximately 6% of total commercial floorspace, according the *City of Stonnington Activity Areas Economic Analysis 2013*, which is at the upper end of reasonable expectations for a well-performing centre.

2.2 Study Area

The Glenferrie Road-High Street Activity Centre Structure Plan Study Area is illustrated in Figure 2.1 and includes:

- Glenferrie Road (from Dandenong Road to across High Street, including Malvern Town Hall),
- High Street (from Malvern Public Gardens to the Armadale Rail Station bridge), and
- Wattletree Road (from the rail bridge west of Malvern Central to Cabrini Hospital).

The study area includes the commercial core of the activity centre as well as the adjoining residential areas, and adjoins land in the City of Glen Eira to the south of Glenferrie Road. The study area contains the following areas of relevance:

- **Activity Centre Boundary** is focused on the commercial zones of the centre which are predominantly located along the main street shopping centre frontages of Glenferrie Road and High Street.

- **Peripheral Areas of Interest** outside of the core Activity Centre Boundary have been included to capture Council buildings, community uses, child care centres, schools etc, in addition to acknowledging the Cabrini Hospital and the 'commercial and medial related spine' located along Wattletree Road.

2.3 Precincts

In order to better understand how Glenferrie Road-High Street Activity Centre operates, the centre has been divided into eight precincts. These precincts are also shown in Figure 2.1, and include the following attributes:

- **Glenferrie Road** is the main suburban street shopping centre, with frontages along Glenferrie Road. Spanning approximately 900m, between High Street in the north and the Malvern Train Station bridge in the south, Glenferrie Road is characterised by high-intensity uses and high levels of pedestrian traffic. The Glenferrie Road precinct includes both the Malvern Tram Depot located behind Glenferrie Road to the west, as well as car parks which are located behind Glenferrie Road to the east.
- **High Street** is a destination strip shopping centre with frontages along High Street. High Street extends approximately 980m from the Glenferrie Road intersection in the east to Armadale Trail Station bridge in the west. High Street is characterised by slightly lower intensity uses, such as showrooms, fashion boutiques and furniture/antique businesses
- **Civic and Institution** is a community/civic precinct centred around Malvern Town Hall located on the corner of Glenferrie Road/High Streets and extending to the east to incorporate a number of schools and community based uses.
- **Wattletree Road Corridor** is a secondary 'commercial and medical related spine' located along Wattletree Road, extending from the railway line to the west to Cabrini Hospital to the east.
- **Armadale Station** is a public transport node centre around Armadale Station to the west of the study area, which incorporates a heritage node centre on the Kingsway Arcade heritage precinct.
- **Malvern Station** is a public transport node focused around Malvern Station to the south of the study area, which incorporates a small localised commercial precinct centred along Station Street and the intersection with Claremont Avenue
- **Malvern Central** is a sub-regional shopping centre and major retail node located on the south-eastern corner of Glenferrie Road and Wattletree Road. Set immediately behind properties fronting Glenferrie Road, pedestrian access to Malvern is via Wattletree Road
- **Princes Highway Precinct** is a commercial/service-industrial precinct situated on land between Pakenham-Cranbourne-Frankston railway corridor and the Princes Highway.

non-retail uses also have the option of locating in dedicated office space or other purpose-built tenancies not typically suited to a retail use.

Non-shopfront tenancies include those typically used for dedicated office space or for other non-retail activities such as light industry or other specialised uses. These commercial tenancies are not suited to a retail use and have been excluded from the tenancy audit, and are covered separately later in this Chapter.

In order to reflect a range of geographic influences on how particular areas of the centre function, and the different roles and markets served by areas the centre, the following areas have been adopted for the subsequent analysis:

- **High Street:** incorporates the entire High Street Strip and the Armadale Station Precinct, as well as the western fringe of the Civic Precinct around the Glenferrie Road Intersection,
- **Glenferrie Road:** incorporates the entire Glenferrie Road Strip,
- **Malvern Station / Dandenong Road:** incorporates both the Malvern Station and the Princes Highway Precinct, and
- **Malvern Central** shopping centre.

Note that the Wattletree Road Corridor has been excluded from the analysis as it does not contain retail or commercial tenancies of significance.

Summary of Results

A detailed breakdown of retail and non-retail commercial shopfront floorspace in Glenferrie Road- High Street is presented in Table 2.1 and shown graphically in Figure 2.2.

A total 617 shopfront tenancies were identified in Glenferrie-High Street Activity Centre, comprising 489 retail tenancies, 76 other commercial tenancies and 52 vacancies.

Overall **retail tenancies account for 79.3% of total shopfront tenants**, which is high for a street-based activity centre and highlights the role of Glenferrie Road-High Street as one of Melbourne's sought after retail destinations. Among the retail categories, non-food retailing has that largest representation, accounting for 40.8% of shopfront tenancies, while food retailers accounted for 23.5%. Retail services represent 14.9% of total shopfront tenancies.

Commercial shopfront businesses (ie, non-retail) comprise only 12.3% of total shopfront tenancies, which is well below the typical 30% usually found in street-based activity areas across Melbourne. The reason for this is that commercial office floorspace is readily available in the area (either above groundfloor retail or in dedicated office buildings). Businesses such as medical consulting rooms and professional services are able to secure specially designed tenancies that would not be suited to a retail use. This is a major asset, and highlights the broad range of commercial tenancy options available to businesses in Glenferrie Road-High Street.

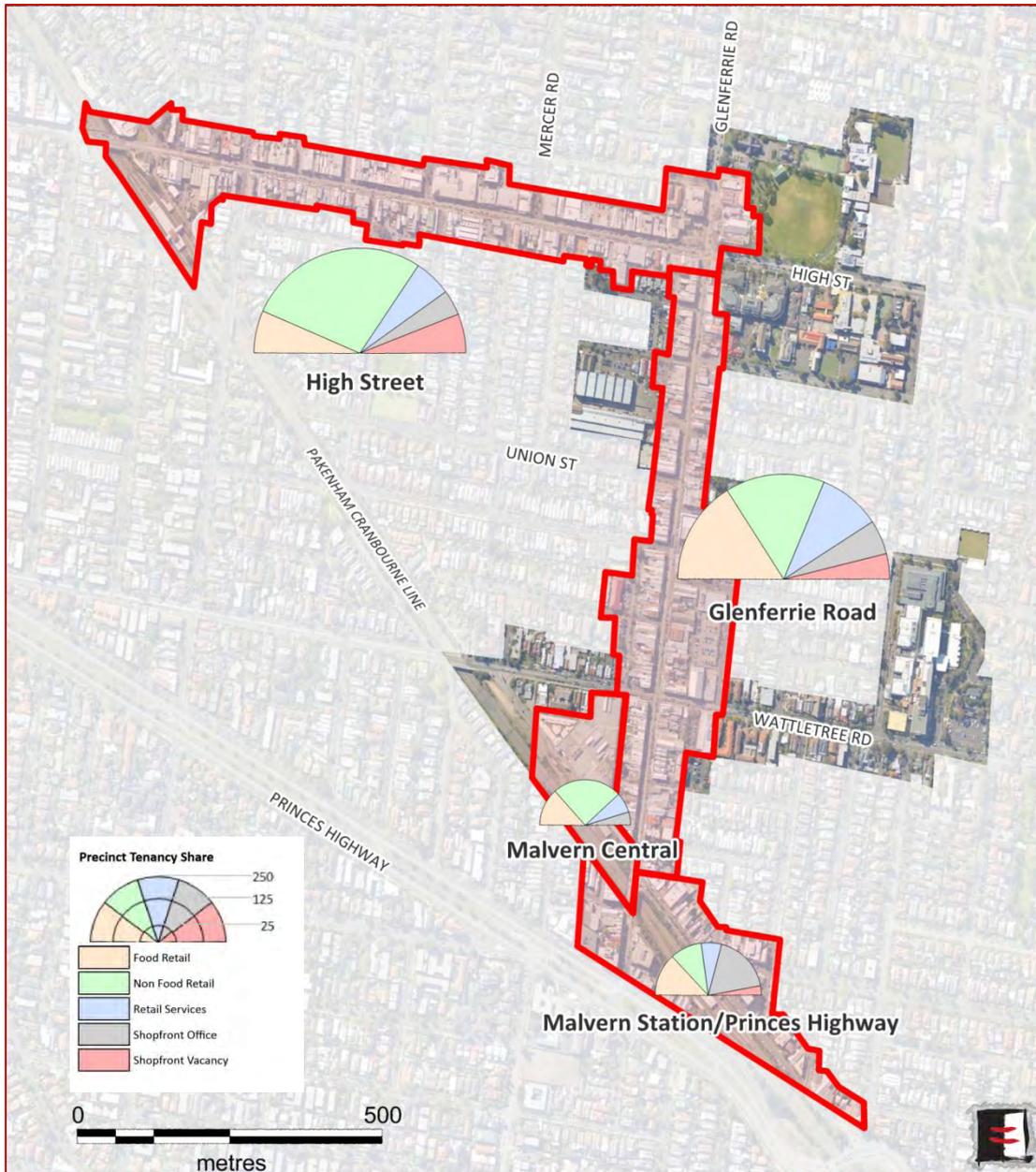
The total of 52 vacant shopfront tenancies accounts for 8.4% of total shopfront tenancies. A vacancy rate of 8.4% is in the order of 2 to 3 percentage points higher than the typical range of vacancies for a well-performing street-based centre of between 4% and 6%.

Table 2.1 Shopfront Tenancies, Glenferrie Road- High Street, June 2014

	High Street		Glenferrie Road		Malvern Central		Malvern Station/ Princess Highway		Total	
	Number	% of Shopfront	Number	% of Shopfront	Number	% of Shopfront	Number	% of Shopfront	Number	% of Shopfront
Food, Liquor and Groceries	6	2.5%	23	9.3%	8	14.5%	8	11.1%	45	7.3%
Food Catering	<u>26</u>	10.7%	<u>56</u>	22.8%	<u>7</u>	12.7%	<u>11</u>	15.3%	<u>100</u>	16.2%
Total Food Retail	32	13.1%	79	32.1%	15	27.3%	19	26.4%	145	23.5%
Apparel	82	33.6%	27	11.0%	17	30.9%	4	5.6%	130	21.1%
Homewares	23	9.4%	15	6.1%	5	9.1%	3	4.2%	46	7.5%
General Merchandise	12	4.9%	32	13.0%	5	9.1%	2	2.8%	51	8.3%
Bulky Merchandise	<u>19</u>	7.8%	<u>1</u>	0.4%	<u>0</u>	0.0%	<u>5</u>	6.9%	<u>25</u>	4.1%
Total Non-Food Retail	136	55.7%	75	30.5%	27	49.1%	14	19.4%	252	40.8%
Retail Services	28	11.5%	47	19.1%	8	14.5%	9	12.5%	92	14.9%
Total Retail	196	80.3%	201	81.7%	50	90.9%	42	58.3%	489	79.3%
Shopfront Office	19	7.8%	26	10.6%	5	9.1%	26	36.1%	76	12.3%
Shopfront Vacancy	29	11.9%	19	7.7%	0	0.0%	4	5.6%	52	8.4%
Total Shopfront	244	100.0%	246	100.0%	55	100.0%	72	100.0%	617	100.0%

Source: Essential Economics, June 2014

Figure 2.2 Shopfront Tenancy Audit, Glenferrie Road-High Street, June 2014



Produced by Essential Economics, with Hansen Partnership, MapInfo and Nearmap

Distribution of Shopfront Tenancies in Glenferrie Road-High Street

A review of the shopfront audit data at the precinct level identifies significant differences in the types of business present within Glenferrie Road-High Street. Key outcomes identified in the analysis by precinct include the following:

High Street

- Contains approximately 200 retail businesses which account for approximately 80% of total shopfront tenancies.
- The role of High Street as an established Melbourne shopping destination is re-enforced by the size of the non-food retail offer (136 retailers, or 56% of total shopfront floorspace). Non-food retail is dominated by high-end apparel (82 traders or 33% of total shopfront tenancies), of which around one-third, or 25 are bridal boutiques. In this respect, High Street is a significant fashion destination in its own right. The centre also contains traders from other non-food retail categories, including a number of niche retailers such as antique shops and traders selling homewares and furniture.
- Food retail accounts for a small, but emerging share of High Street shopfronts, and is dominated by cafes and restaurants (26 in total) which account for 11% of total shopfront tenancies. This emerging representation of cafes/restaurants highlights a recent trend in the overall retail mix of High Street.
- Non-retail commercial shopfront businesses account for only 8% total shopfront tenancies, which is very low for a strip-based centre. These types include medical suites and other personal and professional services such as designers, real estate agents, travel agents, gyms and other personal and professional services.
- The overall share of vacant shopfronts is relatively high in High Street, accounting for 12% of total shopfront tenancies. A concentration of vacant tenancies was identified in the north-eastern area of High Street, around Huntingtower Road and Mercer Road.
- Kings Arcade contains 13 retail tenancies, with the majority selling apparel and other retail services including hairdressing and beauty. Only 1 vacancy was identified.
- Civic Precinct contained 3-5 retailers.

Glenferrie Road

- Similar to High Street, Glenferrie Road contains a total of 246 shopfront tenancies, with retail tenancies accounting for approximately 82%, and non-retail commercial shopfront 11% of total shopfronts, which is similar to High Street.
- However, food retailing accounts 32% of shopfront tenancies, which is more representative of a traditional street-based centre serving local residents.
- The food catering presence in the Glenferrie centre is significant, accounting for 23% of shopfront tenancies, and including 43 cafes and restaurants and 13 takeaway food outlets. This food catering offer is aimed at residents, as well as workers in the centre and surrounding precincts. A cluster is identified around the tram depot.

- The share of Food, Liquor and Groceries retailers (9.3% of total shopfronts) is below what would normally be expected in a centre of this size and type, however a strong representation of fresh food retailers exists (approx. 14, including 6 bakers, 4 green grocers).
- The Coles supermarket (2,500m²) is a limited-range supermarket that is too small to contain the full range of food and grocery products available in larger supermarkets.
- A significant non-food retail offer comprising 31% of total shopfronts, with a large number of general merchandise retailers, as well as apparel, reflects the overall 'diversity of retail offer' on Glenferrie Road.
- Another strong presence in the centre is the number of retail services consisting of mostly beauty salons, hairdressers and other personal care retailers. Together, this category accounts for 19% of total shopfront tenancies on Glenferrie Road.
- An overall shopfront vacancy rate of 7.7% is at the upper limit a 'well-performing' centre.

Malvern Central

- Contains approximately 15,000m² of retail floorspace
- 50 tenancies, including a David Jones small format department store, a full-line Woolworths of 3,400m² and 48 speciality stores
- 49% non-food retail, dominated by apparel and an increasing presence of premium brands.
- Turnover in June 2013 estimated at \$96.8 million, with an average trading level of \$6,500/m² according to Shopping Centre News Little Guns 2012 – however, recent centre refit as well as a new tenancy mix (including the new David Jones) means that current turnover is likely to be in the order \$120m, or \$8,000/m² in 2014.

Malvern Station/Princes Highway

- Relatively small local retail offer (42 traders).
- 36% of shopfront are non-retail commercial shopfront business, including 6 architects, and a number of financial services and personal services.
- Low Vacancy Rate of 5.6%.

2.5 Estimated Existing Retail Sales in Glenferrie Road-High Street

The existing Glenferrie Road-High Street Activity Centre has an estimated 70,000m² in retail floorspace and achieves an estimated \$400.6 million in retail sales. Of this total sales figure, Malvern Central accounts for an estimated \$120 million and the balance of the overall activity centre accounts for \$280.6 million. These estimates are shown in Table 2.2.

As shown later in this report, the Main Trade Area (MTA) served by the activity centre has available retail spending estimated at \$1,068.3 million in 2014. With the centre drawing an estimated 80% or \$320.5 million of its sales from the MTA, it is evident that the centre has a 30% market share of the MTA's available spending. The MTA's balance of 70% in available retail spend is directed to numerous other centres in the MTA and beyond, some of which are illustrated later in Figure 3.1.

Table 2.2: Estimated Annual Retail Sales at Glenferrie Road-High Street Activity Centre, 2014

Location	Retail Floorspace	Average Sales Density	Estimated Total Sales
Malvern Central	15,000m ²	\$8,000/m ²	\$120.0 million
Balance of Activity Centre	55,000m ²	\$5,100/m ²	\$280.6 million
Total Activity Centre	70,000m²	\$5,720/m²(*)	\$400.6 million

Source: Essential Economics

Note: (*) derived average

These retail sales estimates subsequently form part of the assessment of retail potential in the activity centre in Chapter 3.

2.6 Commercial Office

In terms of commercial office space, Glenferrie Road-High Street is part of the “Inner East” office market which is located in the high-income inner-eastern suburbs of Melbourne in close proximity to the Melbourne CBD. Major commercial centres in this market include Hawthorn (over 200,000m²), Camberwell (over 100,000m²) and Kew Junction (over 50,000m²).

According to the *City of Stonnington Activity Areas Economic Analysis 2013* Glenferrie Road-High Street contains an estimated 30,000m² of commercial office space, which is predominantly situated above shopfront tenancies, or as ground floor shopfronts.

In terms of dedicated office floorpace, the tenancy audit undertaken by Essential Economics identified a total of approximately 10,000m² of dedicated office floorspace. This dedicated office floorspace is primarily located around the intersection of Dandenong Road and Glenferrie Road, and includes:

- 2,700m² located at 2-6 Glenferrie Road (3 storey)
- 2,100m² at 1-11 Glenferrie Road owner occupied (3 storey)
- 3,400m² 615 Dandenong Road (3 storey)

In this context Glenferrie Road-High Street is not considered a major commercial office location, however, additional office development is located in proximity to the centre (particularly along High Street to the west) and therefore needs to be taken into consideration when assessing the overall demand for commercial office floorspace.

2.7 Community Infrastructure and Services

Glenferrie Road-High Street has an important role in providing the community with a broad range of infrastructure and services. This role includes:

- **Cabrini Hospital:** with 508 beds and a diverse range of medical and para-medical services located on-site, the Cabrini Hospital attracts significant numbers of staff, patients and visitors who would otherwise not visit the activity centre. Likewise, the Cabrini Hospital supports a large number of consulting rooms and medical services that are located within or on the fringe of the activity centre.

- **Social and Civic Infrastructure:** including Council Offices, City Hall, Malvern Police Station and Malvern Library which located around the north-eastern intersection of Glenferrie Road and High Street and employ a total of approximately 750 persons, according the 2011 Census data. Council are currently relocating staff from the Prahran Office to the old Transfield Building located at 311 Glenferrie Road.
- **Education:** including De La Salle College (1,200 Students and 150 Staff) and St Joseph's Primary (250 Students and 30 Staff) located within the study area.
- **Malvern Tram Depot:** located at 21 Coldblo Road, on the western periphery of Glenferrie Road, the Malvern Tram Depot is an important State asset and an integral part of the centre. Consultation with Yarra Trams confirms that the depot currently services 6 regular tram routes as well as employing approximately 200 staff.
- Other uses of relevance to the centre include the Firestation Print Studio, located on Wills Street, as well as a range of personal, professional and business services.

2.8 Employment

The economic role of Glenferrie Road-High Street can also be examined with reference to Journey-to-Work data derived from the Census. Accordingly, approximately 9,500 persons work within walking distance of the Glenferrie Road-High Street, and include:

- 4,400 Managers and Professionals (46% of workforce)
- 1,400 Clerical and Administrative employees (15% of workforce)
- 1,200 Sales staff (13% of workforce)
- 1,000 Community and Personal Service employees (11% of workforce)
- 800 Technicians and Trades workers (8% of workforce), and
- 700 Machinery Operators, Drivers and Labourers (7% of workforce).

These people at work in the area contribute to retail expenditures, especially in regard to convenience retail (eg takeaway food), but also in other areas of retail and services.

2.9 Housing and Resident Population

Analysis of ABS Census data shows recent changes in population and housing in the Glenferrie Road-High Street Activity Centre, the period 2006 to 2011. As shown in Table 2.2 below, in 2011 the Centre contains an estimated 165 dwellings accommodating approximately 270 residents, representing an increase of +55 dwellings (+50%) and +85 residents (+46%) over the period. Overall, the average household size in the Centre is 1.6 persons, which is in line with household sizes in activity centres across Melbourne.

- **High Street** comprises a slightly higher average household size of 1.8 persons per household. Over the period 2006 to 2011 the total number of dwellings in High Street increased from 40 to 45 (+13%), while the population increased from 60 residents to 80 (+33%). Currently, several small 'boutique' apartment developments are under-construction on the southern side of High Street, while a 5-storey development at 48 units at 948-960 High Street has recently been approved.

- **Glenferrie** also comprises an average household size of 1.6 persons per household. Over the period 2006 to 2011 the total number of dwellings in Glenferrie Road increased from 55 to 60 (+9%), while the population increased from 80 residents to 95(+19%).
- **Malvern Station/Princes Highway** comprises an average household size of 1.6 persons per household. Over the period 2006 to 2011 the total number of dwellings in this location increased by an estimated 45 dwellings to 60 in total, while the population increased by 50 residents to 95 in total. A 5-storey development consisting of 40 apartments at 65 Station Street was constructed in 2006 and therefore accounted for almost the entire increase in total dwellings in the area.

In comparison, the broader study area has an average household size of 2.0 persons per household which is indicative of the larger detached dwellings and family homes in the centre’s residential areas. Over the period 2006 to 2011 the number of dwellings in the broader study area increased from 1,340 to 1,400 representing an increase of 60 dwellings, or +4%. Over the same period, the resident population declined slightly from 2,770 to 2,760 persons.

Table 2.2: Dwelling and Population Growth, 2006 to 2011, Glenferrie Road-High Street Activity Centre

	2006		2011		Change	
	Dwellings	Population	Dwellings	Population	Dwellings	Population
High Street	40	60	45	80	+5 (+13%)	+20 (+33%)
Glenferrie Road	55	80	60	95	+5 (+9%)	+15 (+19%)
Malvern Station/Princess Highway	15	45	60	95	+45 (+300%)	+50 (+111%)
Total GR-HS Activity Centre	110	185	165	270	+55 (+50%)	+85 (46%)
<i>Study Area</i>	<i>1345</i>	<i>2770</i>	<i>1380</i>	<i>2760</i>	<i>+35 (+3%)</i>	<i>-10 (-1%)</i>

Source: ABS Census, 2006 and 2011, Essential Economics

While some higher density-residential is occurring on High Street and around the Malvern Station Precinct, the Centre has not experienced the same levels of residential development as other street-based activity centres. For example, the Chapel Street Activity Centre (which is similar in size to Glenferrie Road-High Street) contained an estimated 7,000 dwellings in 2011, increasing by +1,060 dwellings from 2006 levels, according to the *Chapel ReVision Economic Advice* prepared by SGS Economics and Planning.

Given the proximity of Glenferrie Road-High to central Melbourne and it’s desirable neighbourhood location, the centre can be expected to experience increasing pressure for additional residential development.

2.10 Summary: Role and Function of Glenferrie Road-High Street

Glenferrie Road-High Street occupies a unique role in the context of the Melbourne retail environment. The centre serves a wider role than is typical for similar-sized centres and provides a diverse range of fresh food, café/restaurant, apparel and comparison shopping, retail and professional services. The centre can be characterised by the following features and roles:

- **High Profile Retail Location:** High Street and Glenferrie Road have a high profile and level of name-recognition across Melbourne and beyond.
- **Broad geographic draw:** High Street in particular tends to draw from a relatively wide geographic area, especially the café/dining and apparel stores.
- **Street-based centre:** A strong sense of street-based activity
- **Diverse trading/retail offer:** The breadth of retail and commercial services provided along Glenferrie Road in particular allows the precinct to attract customers with a wide range of needs. The range of retail and service options also assists in increasing the length of a visit.
- **Strong cultural heritage:** Glenferrie Road and High Street are endowed with a long history and numerous historic buildings. These elements add to the ‘flavour’ of the street, creating a more vibrant and engaging environment.
- **Commercial Office:** Glenferrie Road-High Street contains around 10,000m² of dedicated office floorspace and is not considered a major commercial office location.
- **Residential:** small resident population and low levels of housing provision. This locality is likely to continue to experience increased demand for residential development.

3 RETAIL ASSESSMENT

This Chapter provides an overview of the competitive environment in which Glenferrie Road-High Street operates in both the retail and commercial office markets. This analysis provides a further understanding of the broader market context in which the centre is located, as well as providing an indication of its outlook for future development.

3.1 Trade Area Definition

Glenferrie Road-High Street serves a main trade area (MTA) which reflects the geographic region within which residents are likely to regularly use the centre for a range of day-to-day retail and other needs.

The boundaries of the MTA are based on a number of factors which include the strength of the retail offer in Glenferrie Road-High Street, the location and performance of competing centres, access to the centre via main road routes and via public transport, and existing travel to work patterns.

As shown in Figure 3.1 the following page, the MTA is located entirely to the south of the Monash Freeway and is generally bounded by the East Malvern Train Station, Waverly Oval and Darling Road to the east, Dandenong Road and Balaclava Road to the south, and Orrong Road, Chomley Street and Toorak Village to the west.

Further to serving local residents, Glenferrie Road-High Street is also a destination for people living across the broader inner and middle ring suburbs of Melbourne's eastern suburbs and beyond. This includes, both local workers and people who make special-purpose visits to one or other of the centre's retail, commercial or community attractions (such as the cluster of bridal boutiques located on High Street, Armadale).

3.2 Competition

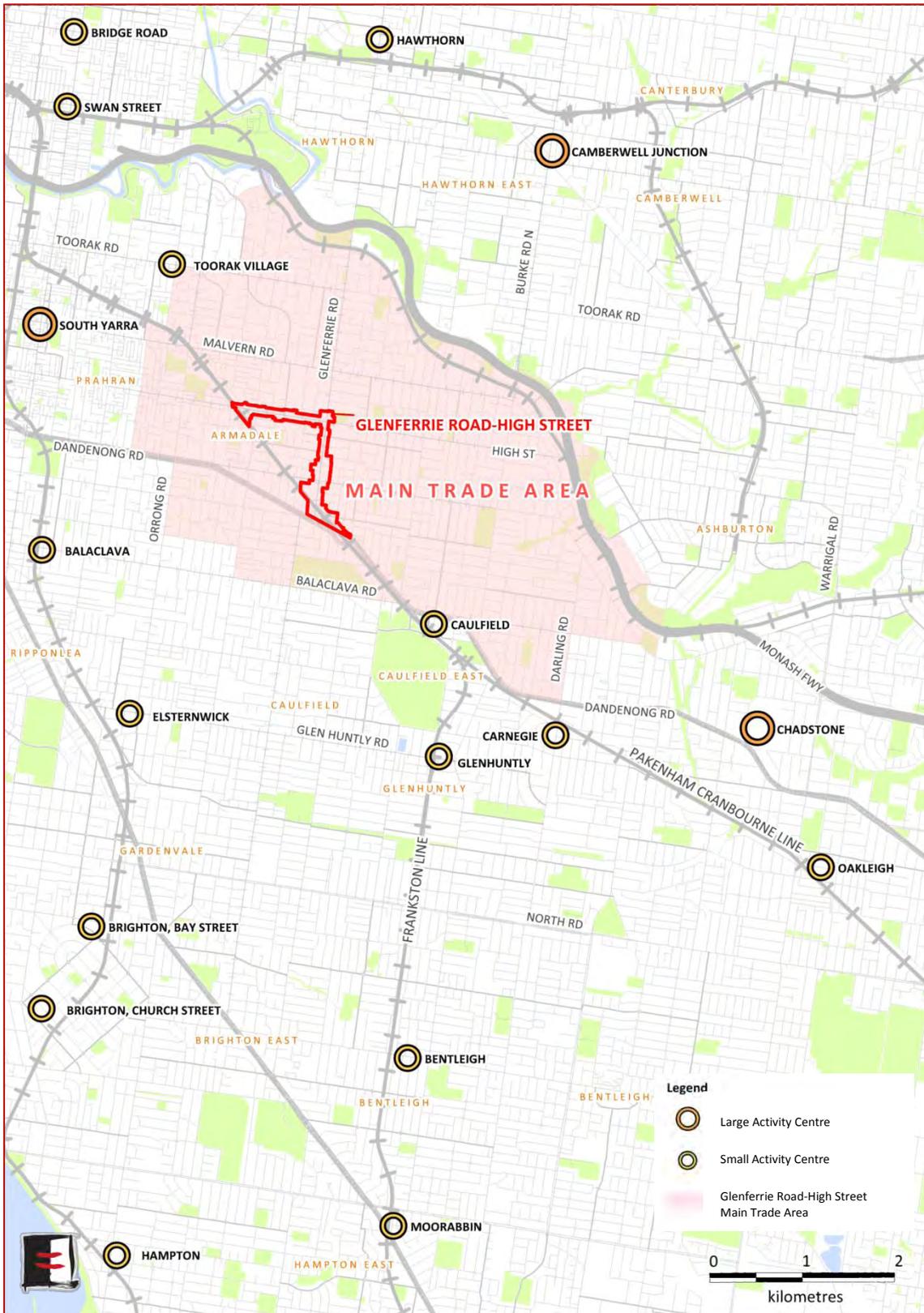
MTA residents visit a range of other activity centres and retail destinations which compete directly or indirectly with the Glenferrie Road-High Street Activity Centre.

The Melbourne CBD, South Yarra (Chapel Street), Chadstone Shopping Centre and Camberwell Junction are the larger higher-order centres in the region.

Toorak Village and Glenferrie Road (Hawthorn) draw trade primarily from the north-west of the MTA, while Caulfield, Carnegie, Glen Huntly, Elsternwick and Balaclava activity centres draw trade primarily from the south and south-west.

A number of smaller Neighbourhood Centres and groups of shops are also located in the MTA which serve the basic convenience needs of local residents. These centres are shown in Figure 3.1:

Figure 3.1: Glenferrie Road-High Street Main Trade Area



Produced by Essential Economics with MapInfo and StreetPro

3.3 Trade Area Population Forecasts

The population of the MTA served by the Centre is estimated at 62,190 persons in 2014.

In the period 2001 to 2014, the MTA population increased by +6,780 persons, at an average rate of +520 persons pa, or + 0.9% pa. This average rate of population growth is lower than the rate of +1.8% pa which occurred in Greater Melbourne over the same period; this reflects the generally well-established nature of the surrounding area.

As shown in Table 3.1, over the period 2001 to 2014 population growth in the MTA has accelerated and is estimated at +690 persons pa, or expanding at +1.1% pa between 2011 and 2014.

Over the period to 2031, MTA population is expected to increase by approximately 6,000 persons, to 68,190 persons in 2031, according to *Victoria in Future* and projections prepared by id Consulting. This represents a lower average rate of population growth of +350 persons pa, or +0.5% relative to current population growth levels.

The average growth rate of +0.5% pa for the MTA to 2031 is lower than the forecast growth rate of +1.3% pa forecast for Greater Melbourne over the same period. However, this growth rate for the MTA is generally consistent with the rates of population growth forecast for similar inner-middle-ring suburban areas of Melbourne.

Table 3.1: Population Trends and Forecasts for Main Trade Area, 2001-2031

	2001	2011	2014	2021	2031
Population (No.)					
Main Trade Area	55,410	60,120	62,190	64,890	68,190
Average Annual Growth (No.)					
Main Trade Area	-	+470	+690	+390	+330
Average Annual Growth (%)					
Main Trade Area	-	+0.8%	+1.1%	+0.6%	+0.5%
Greater Melbourne	-	+1.8	+2.0%	+1.9%	+1.6%

Source: ABS, *Victoria in Future*, id Consulting and Essential Economics

Note: Figures Rounded

3.4 Socio-Economic Characteristics

Table 3.2 presents a summary of socio-economic characteristics of MTA residents, based on data from the 2011 ABS Census of Population and Housing. These trade area characteristics are compared against the metropolitan Melbourne averages.

The data reveals the following about the demographic and socio-economic profile of MTA residents:

- **Income** levels in the MTA are significantly higher than the Greater Melbourne average, with median individual incomes +50% above and median household incomes +35% above the Greater Melbourne median.
- **Age Profile** of MTA residents is similar to the Greater Melbourne profile, with a median age of MTA residents of 37 years compared to a 36 years median for Melbourne. However, the

MTA does have a higher proportion 'young professional' residents compared to Melbourne. Approximately 27% of MTA residents are aged between 20 and 34 years compared to the Greater Melbourne average 23%.

- **Family Composition and Household Size.** A higher proportion of families in the MTA are couples without children, compared to Greater Melbourne (42% cf 35%). The MTA also has a lower proportion of one parent families (11% cf 15%). As such, the average household size of 2.3 persons per household in the MTA is significantly lower than the average of 2.6 persons per household for Greater Melbourne.
- **Origin.** The MTA has a relatively low degree of ethnic diversity compared to Greater Melbourne. 70% of MTA residents were born in Australia, with approximately 80% speaking only English at home. These percentages compare to approximately 67% and 70% for Greater Melbourne respectively.
- **Dwelling Type.** The MTA compared to Melbourne, is characterised by a high number of apartments (39% of housing stock cf 15% of housing stock) and semi-detached dwellings and terraces (18% of housing stock cf 12%).
- **Tenure.** The share of dwellings rented in the MTA (37%) is significantly higher compared to Greater Melbourne (28%), while the share of homes that are owned with a mortgage is significantly lower in the MTA (26%) compared to Greater Melbourne (38%).
- **Housing Costs.** Housing costs in the MTA in terms of both average rents (\$350/wk) and mortgage repayments (\$2,510/mth) are significantly higher than the Greater Melbourne averages (\$310/wk and \$1,840/mth).
- **Internet, Car Ownership etc.** Levels of car ownership are slightly lower in the MTA compared to Greater Melbourne (89% cf 91%), while a higher share of households in the MTA are connected to the internet (87% cf 82%).
- **Employment.** Unemployment in the MTA (4.3%) is lower than the Greater Melbourne average (5.5%), with a high labour force participation rate (69%) relative to Greater Melbourne (66%).
- **Occupation.** Approximately 90% of the MTA labour force are employed in white-collar occupations (Managers & Professionals, and Clerical & Sales workers), compared to Greater Melbourne (72%).

Overall the MTA has a socio-economic profile which is significantly higher than the metropolitan Melbourne average. This aspect is reflected in the higher levels of retail spending by MTA residents compared with the metropolitan area, as shown in the following section.

Table 3.2: Socio-Economic Characteristics, Main Trade Area, 2011

Category	Main Trade Area	Greater Melbourne
<u>Income</u>		
Median individual income (annual)	\$46,210	\$30,810
Variation from Greater Melbourne median	+50.0%	0.0%
% of persons (15 years or older) earning \$1,000pw or more	45.3%	29.2%
Median household income (annual)	\$94,000	\$69,510
Variation from Greater Melbourne median	+35.2%	0.0%
% of Households earning \$2,500pw or more	38.2%	21.6%
<u>Age Structure</u>		
0-4 years	5.4%	6.5%
5-19 years	15.4%	18.3%
20-34 years	26.6%	22.9%
35-64 years	37.5%	39.1%
65-84 years	12.5%	11.3%
85 years and over	2.6%	1.8%
<i>Median Age (years)</i>	37	36
<u>Country of Birth</u>		
Australia	70.3%	66.8%
Other Major English Speaking Countries	8.7%	7.6%
Other Overseas Born	21.0%	25.6%
<i>% speak English only at home</i>	78.7%	69.5%
<u>Family Composition</u>		
Couple family with no children	41.6%	34.8%
Couple family with children - Total	44.8%	47.9%
One parent family - Total	10.5%	15.3%
<u>Dwelling Structure (Occupied Private Dwellings)</u>		
Separate house	43.2%	72.7%
Semi-detached, row or terrace house, townhouse etc.	17.6%	11.6%
Flat, unit or apartment	38.8%	15.3%
<i>Occupancy rate</i>	90.2%	91.0%
Average household size	2.3	2.6
<u>Tenure Type (Occupied Private Dwellings)</u>		
Owned outright	35.4%	33.5%
Owned with a mortgage	26.3%	37.7%
Rented	37.4%	28.0%
<u>Housing Costs</u>		
Median monthly mortgage repayment	\$2,510	\$1,840
Variation from Greater Melbourne median	36.4%	0.0%
Median weekly rents	\$350	\$310
Variation from Greater Melbourne median	12.9%	0.0%
Share of occupied dwellings connected to the internet	87.3%	82.0%
<u>Car Ownership per Dwelling</u>		
None	11.3%	9.4%
One	42.1%	36.1%
Two	35.0%	38.0%
Three or more	11.6%	16.5%

Source: ABS Census 2011, and Essential Economics

Note: MESC Major English Speaking Countries – Canada, Ireland, New Zealand, South Africa, UK and USA

3.5 Retail Spending Analysis

Estimates of per capita retail spending by MTA residents have been prepared with reference to the *MarketInfo* retail spending model. *MarketInfo* is a micro-simulation model prepared by MDS Pty Ltd which uses data from the ABS Household Expenditure Survey, the ABS Census of Population and Housing, ABS Australian National Accounts, and other relevant sources.

Table 3.3 presents estimates of per capita retail spending in 2014 by trade area sector and compares these with the Greater Melbourne average. The spending data is presented in four major spending category groupings:

- **Food, Liquor and Groceries** – Fresh food, groceries and take-home liquor.
- **Food Catering** – Cafes, restaurants and takeaway food
- **Non-Food** – Apparel, homewares, bulky merchandise and general merchandise
- **Services** – Hairdressers, beauty salons etc.

MTA per capita retail spending is well above the Melbourne average for all four retail product categories. Total spending per capita by MTA residents is estimated at \$16,970 in 2014, which is +22% above the Melbourne average of \$13,960.

MTA per capita spending on Food Catering and Services is +45% and +42% higher than the Greater Melbourne average, respectively. The high levels of MTA spending on these largely discretionary retail categories reflect the higher incomes and greater spending potential of MTA residents.

Table 3.3: MTA Per Capita Retail Expenditure, 2014 (\$2014)

	Food, Liquor and Groceries	Food Catering	Non Food	Services	Total Retail
Main Trade Area	\$5,830	\$2,760	\$7,600	\$780	\$16,970
<i>Greater Melbourne Average</i>	<i>\$5,510</i>	<i>\$1,900</i>	<i>\$6,000</i>	<i>\$550</i>	<i>\$13,960</i>
Variation from Greater Melbourne Average	+6%	+45%	+27%	+42%	+22%

Source: MarketInfo; Essential Economics

Estimates of total retail expenditure by MTA residents are shown in Table 3.4 by applying the average spending levels described in Table 3.3 to the population estimates presented in Table 3.1.

Forecasts of spending have been prepared for the period to 2031, with application of real growth in spending per capita based on historical growth rates in retail spending over the past 20 years, as derived from ABS Australian National Accounts data.

Total spending by MTA residents is forecast to increase from \$1,068 million in 2014 to \$1,231 million in 2021, and to \$1,500 million in 2031. Annual growth of +2.0% is forecast over the entire period (expressed in constant 2014 dollars).

Spending on non-food product categories by MTA residents is forecast to increase from \$481 million to \$746 million over the period, and at a higher overall growth rate of +2.6% per annum. All figures are represented in constant 2014 dollars and therefore the effects of inflation are excluded from the analysis.

Table 3.4: MTA Total Retail Expenditure, 2014 to 2031 (\$2014)

Year	Food, Liquor and Groceries	Food Catering	Non Food	Services	Total Retail
<u>Total Spending</u>					
2014	\$364.2m	\$174.1m	\$480.5m	\$49.5m	\$1,068.3m
2021	\$393.5m	\$198.2m	\$578.0m	\$61.4m	\$1,231.1m
2031	\$434.7m	\$235.8m	\$745.8m	\$82.6m	\$1,498.8m
<u>Spending Growth</u>					
2014-2021	\$29.3m	\$24.1m	\$97.5m	\$11.9m	\$162.8m
2021-2031	\$41.2m	\$37.6m	\$167.8m	\$21.2m	\$267.7m
<u>Growth Rate</u>					
2014-2021	+1.1%	+1.9%	+2.7%	+3.1%	+2.0%
2021-2031	+1.0%	+1.8%	+2.6%	+3.0%	+2.0%

Source: MarketInfo; Essential Economics

3.6 Retail Potential

Three scenarios are assessed as a means of identifying potential for retail floorspace growth in the activity centre in the period 2014 to 2031. Basic inputs to the scenarios involve forecast growth in the MTA population (Table 3.1), associated retail spending (Table 3.4), and the opportunity for the centre to attract increased sales from both MTA and non-MTA residents (as shown in the scenarios).

The scenarios are as follows:

- Scenario 1: No Action to Enhance Retailing – Reduction in Market Share
- Scenario 2: Maintain Existing Market Share through Retail Enhancement
- Scenario 3: Increase Market Share through further Retail Enhancement

In essence, these scenarios indicate the various outcomes if no action is taken to improve retail activity in the centre (Scenario 1), compared with efforts required to at least maintain market share (Scenario 2) or to increase market share (Scenario 3).

For each of the scenarios, the existing retail floorspace is set at 70,000m², the retail sales density is set at \$6,775/m² (which represents an average increase of 1% pa on the 2014 figure - Refer Table 2.2); and 80% of sales continue to be drawn from the MTA. The only variable is the market share that is achieved by 2031.

Table 3.5 shows the three scenarios and the calculations to derive at potentially supportable retail floorspace. These forecasts of retail floorspace potential are only an indicative guide of potential development outcomes.

Under **Scenario 1**, where no new actions are taken to increase market share through enhancement of retailing in the centre, no new retail floorspace is supportable to 2031 and the existing vacant floorspace of 7,000m² would likely continue to remain vacant over time.

Under **Scenario 2**, where market share is maintained at 30%, an increase of some 6,000m² is supportable, as well as the occupation of the 7,000m² of existing (2014) vacant retail floorspace.

Under **Scenario 3**, where a very modest increase in market share of 32% is achieved (up from today's 30%), an additional 11,500m² of new retail floorspace can be supported, as well as the occupation of the existing 7,000m² in vacant retail floorspace.

Important initiatives would need to be undertaken to increase market share in the face of continuing retail competition and to generate new uses for existing and new retail floorspace, as in Scenarios 1 and 2.

Floorspace growth could be accommodated through a number of initiatives, such as the expansion of the existing Malvern Central Shopping Centre, development of a future supermarket expansion on Glenferrie Road, and/or the redevelopment and intensification of the existing built form in the overall centre, particularly along High Street and around Malvern Station/Dandenong Road and the Armadale Station.

Table 3.5: Scenarios Showing Glenferrie Road-High Street Potential Retail Floorspace Potential, 2014 to 2031 (\$2014)

Variable	Scenario 1 No Action – Loss of Market Share	Scenario 2 Maintain Market Share at Current 30%	Scenario 3 Increase Market Share to 32%
Available MTA retail expenditures	\$1,489.8m	\$1,489.8m	\$1,489.8m
Market share	25% (*)	30%	32%
Sales to MTA residents (80% of all sales)	\$379.4m	\$449.6m	\$479.4m
Sales beyond MTA (20% of all sales)	\$94.9m	\$112.5m	\$119.9m
Total retail sales	\$474.3m	\$562.1m	\$599.3m
Retail floorspace supportable at \$6,775/m ²	70,000m ²	83,000m ²	88,500m ²
Existing retail floorspace	70,000m ²	70,000m ²	70,000m ²
Additional retail floorspace supportable	Nil	+13,000m ²	+18,500m ²
Existing vacant retail floorspace	7,000m ²	7,000m ²	7,000m ²
Supportable new retail floorspace	Nil	+6,000m²	+11,500m²

Source: MarketInfo and Essential Economics

Note: Scenario 1 data interpolated from existing situation; derived Market Share is 25.3% (rounded to 25%).

4 COMMERCIAL OFFICE ASSESSMENT

Limited expansion of the commercial office market in Glenferrie Road-High Street is forecast over the next 10 to 15 years. As already noted, very limited redevelopment sites are available within the centre, which makes large-scale office development difficult. Nonetheless, the general Malvern/Armadale office market, of which Glenferrie Road-High Street is integral, is expected to remain a key office precinct for metropolitan Melbourne due to the centre's strong transport links, proximity to the CBD and the high skills level of the surrounding residential population.

Glenferrie Road-High Street is in a strong position to maintain a successful commercial office sector with a low vacancy rate into the future.

Over time, incremental growth or redevelopment of office space in the centre would be expected to occur. This is encouraged in order to ensure that the quality of office stock in the centre remains strong, particularly in the northern and southern parts of the centre where most of the dedicated office space exists.

As an aspirational goal for the Glenferrie Road-High Street Structure Plan, it is appropriate to allow for the development of approximately 20,000m² of commercial floorspace, with the majority located in the general Princes Highway Malvern Station Precincts area. Such an outcome is likely to be achieved over the longer-term in view of current economic conditions. Which do not support strong levels of office development.

However, seeking some intensive office use in Glenferrie Road-High Street is consistent with activity centre policy aspirations, yet reflects the significant physical constraints within the centre and the strong development potential in the nearby Caulfield area and in the balance of the Inner-East office market.

It is also important that the structure plan recognise the smaller-scale office functions in Glenferrie Road-High Street. These uses rely on exposure and accessibility to the large number of people using the centre and can be accommodated in shopfronts locations (e.g. banks), shop-top tenancies or in office developments providing relatively small tenancies.

5 RESIDENTIAL ASSESSMENT

5.1 Dwelling Trends by Type

As discussed in Section 1.3, increasing market acceptance of high-density residential development is evident in metropolitan Melbourne, especially in the inner and middle-ring suburbs.

In inner Melbourne - which includes the Local Government Areas of Darebin (C), Melbourne (C), Moonee Valley (C), Moreland (C), Port Phillip (C), Stonnington (C) and Yarra (C)- the share of total dwelling stock comprising flats, units or apartments has increased from 43% in 2001 to 50% in 2011, as shown in Figure 4.1. Furthermore, the share of higher density development, in flats, units or apartments of four or more storeys has increased from 31% in 2001 to 43% in 2011.

Over the same period, the share of total dwelling stock comprising flats, units or apartments in Armadale, Malvern and Glen Iris has remained consistent at approximately 40% of total dwelling stock. However, the share of higher-density development in flats, units or apartments of four or more storeys has doubled from 3% of dwelling stock in 2001, to 6% in 2011, highlighting the trend of higher-density development expanding outwards from the CBD and inner suburbs.

Table 5.1: Share of Flats, Units and Apartments of Total Dwelling Stock, 2001-2011

Category	Inner Melbourne	Armadale, Malvern and Glen Iris
<u>Flats, units and apartments</u>		
2001	43%	40%
2006	48%	41%
2011	50%	40%
<u>Flats, units and apartments of 4 or more storeys</u>		
2001	31%	3%
2006	40%	5%
2011	44%	6%

Source: ABS Census, 2011

Note: Armadale, Malvern and Glen Iris refer to the SA2 geography (ASGS) used by the ABS

5.2 Forecast Dwelling Demand

Over the period to 2011 to 2031, dwelling demand in the City of Stonnington is forecast to increase by additional + 15,390 dwellings, according to DTPLI's *Victoria in Future 2014*. Since 2011, some of this demand has already been met, mostly in the form higher density development in areas including Forrest Hill and the ChapelVision Area.

Since 2011, a total of approximately 2,200 apartments have either been constructed or are under-construction in the City of Stonnington, according to data sourced from Oliver Hume Research. The data highlights that the product mix, is essentially in 1 and 2 bedroom apartments.

Small area dwelling projections for the areas in which Glenferrie Road-High Street is situated, are shown in Table 5.2, and are based on recent id consulting forecasts commissioned by the City of Stonnington.

The combined area of Armadale and Malvern South extends from Malvern Road in the north to Dandenong Road in the south ,and from Toorong Road in the east to Orrong Road in the west.

Table 5.2: Forecast Dwelling Growth, Armadale and Malvern South, 2014 to 2031

Study Area	2014	2021	2031	2014-2031
<u>Number of Dwellings (No.)</u>				
Armadale	4,750	5,130	5,430	680
Malvern South	<u>3,140</u>	<u>3,350</u>	<u>3,500</u>	<u>360</u>
Total Armadale/Malvern South	7,890	8,480	8,930	1,040
<u>Average Annual Growth (No.)</u>				
Armadale		+55	+30	+40
Malvern		<u>+30</u>	<u>+15</u>	<u>+20</u>
Total Armadale/Malvern South		+85	+45	+60
<u>Average Annual Growth Rate (%)</u>				
Armadale		+1.1%	+0.6%	+0.8%
Glenferrie Road		+0.9%	+0.4%	+0.6%
Total Armadale/Malvern South		+1.0%	+0.5%	+0.7%

Source: id Consulting Note: Figures Rounded

The forecast show that the combined areas of Armadale and Malvern South are forecast to experience strong dwelling growth of approximately +60 dwellings per annum between 2014 and 2031. Armadale is expected to accommodate about two-thirds of the forecast dwelling growth.

The forecasts are considered to be on the conservatively low side and are based on known residential patterns at the time the forecasts were prepared.

Due to lack of appropriately zoned land in the surrounding area it is expected that Glenferrie Road-High Street, as an activity centre, will likely to accommodate in the order of 75%-85% of the projected dwelling demand for Armadale and Malvern South.

On this basis it is considered prudent to plan for dwelling growth in the activity centre in the order of +800 to +900 dwellings by 2031, at an average rate in the order of +50 dwellings pa.

This represents a modest 6-7% of total dwelling growth forecast for the City of Stonnington (VIF 2014), which highlights the important ongoing role of areas such as Forrest Hill and the ChapelVision Area in accommodating forecast residential demand.

6 ISSUES AND OPPORTUNITIES

This Chapter identifies the issues facing Glenferrie Road-High Street Activity Centre, with a commentary on the type of response required to address these issues. Opportunities to implement specific actions to promote centre revitalisation are also identified.

6.1 Issues

The following issues have been identified for Glenferrie Road-High Street that will require an appropriate response in the Structure Plan.

Large Physical Size of Centre

The walking distance from the intersection of Glenferrie Road and Dandenong Road, to Armadale train station via Glenferrie Road and High Street, is almost 2km. This is an extensive distance and is only marginally less than the length of Chapel Street from the Yarra River to Dandenong Road. Glenferrie Road and High Street are at a 90 degree angle and the intersection of both roads is the only functional link between both commercial strips. This has implications for how both strips interact with each other and how the broader Activity Centre is used by shoppers and other visitors.

Response: *The Structure Plan should adopt a precinct-based approach that provides specific guidance for planning and development to particular parts of the centre. Specifically, it needs to reflect how customers and visitors use each precinct, and potentially visit multiple precincts in the one trip. An understanding of how the Glenferrie Road and High Street strips can integrate more closely will be a critical outcome.*

Additionally, the issues relevant to businesses and traders across the centre will vary significantly. For example on High Street, changes in the tenancy mix are evident as a result of a relative decline in the bridal, high-end fashion, and antiques traders in this part of the centre.

Diversity of Uses

A high degree of diversity among traders and stakeholders means that the Structure Plan needs to deal with a complex range of issues relevant to particular individual components of the centre.

For example, historically, traders in Glenferrie Road are more closely linked to serving local residents daily shopping needs such as basic food and groceries. In contrast, traders in High Street attract a greater share of spending by non-local residents and discretionary spending on categories such as antiques and high-end fashion. In this context, High Street is generally more vulnerable to economic cycles than Glenferrie Road and in downward cycles High Street benefits more from the linkages to Glenferrie Road in terms of access to these shoppers than vice-versa.

Additionally, two Traders Associations (High Street Armadale Traders Association and the Glenferrie Road Malvern Business Association) operate within the centre with their own objectives for centre planning and development.

Response: *While a precinct-based approach is supported, the need for integration throughout the centre needs to be prioritised, including a need for trader groups to collaborate regularly. Notably, improved*

links near the intersection of High Street and Glenferrie Road are critical to the ongoing successful integration of the centre.

Vacancies

Currently, the overall vacancy rate across the centre is estimated at approximately 6%, which is at the higher end of the typical range of vacancies of between 4% and 6% for an activity centre that is performing in a reasonable manner, as evidenced regionally.

While no 'dead spots' are identified with regard to pedestrian activity, vacancy rates in the High Street strip are significantly higher than in Glenferrie Road, with a particular cluster of vacancies at the eastern end of High Street.

Vacancy rates are a key indicator of relative 'centre health', with a higher-than-typical vacancy rate creating negative impacts on shopper amenity and perception of a centre.

Response: *The number, type and location of vacant tenancies in Glenferrie Road-High Street need to be monitored closely and regularly. The Structure Plan needs to provide both short-term and permanent actions to activate underutilised areas and premises, including vacant tenancies.*

Accommodating Retail Demand

The current Main Trade Area population 62,190 persons is forecast to increase to 68,190 persons by 2031, reflecting annual growth of 350 persons or 0.5% per annum. Combined with real growth in per capita retail expenditure by residents (+1.5% pa), total retail spending by residents is forecast to increase by an average of \$25 million or +2.0% per annum over the period to 2031 (expressed in constant 2014 dollars).

As a result, Glenferrie Road-High Street should capture a share of the overall increase in total retail expenditure by Main Trade Area residents over the forecast period. This can be reflected in either:

- The occupancy of existing vacant tenancies with new businesses, or
- Increased sales by existing businesses, or
- The creation of new retail shopfront floorspace through development opportunities.

Response: *Capacity exists for a significant share of increased retail demand to be accommodated through the filling of current vacancies in the centre, and through increasing sales levels for existing traders. The Structure Plan should therefore prioritise more efficient uses of existing commercial floorspace, while providing for modest allowances for increased floorspace as urban form allows.*

Accommodating Residential Demand

Over the period to 2031, increased demand for dwellings in the City of Stonnington is forecast to occur. According to *Victoria in Future 2014* (VIF), an additional +15,390 dwellings are forecast for the municipality between 2011 and 2031, representing an increase of +4,530 dwellings on previous VIF forecasts (2012, figures rounded).

A portion of this demand (68%) can potentially be accommodated in the Forrest Hill and the ChapelVision Area (10,400 dwellings in total), but the balance in dwelling demand will increase the burden on Glenferrie Road-High Street to accommodate residential development.

Response: Evidence across Melbourne shows demand for higher-density residential development spreads outwards over time. As Chapel Street develops further, Glenferrie Road-High Street is expected to experience increasing pressure to accommodate higher-density residential. The Structure Plan needs to respond to this anticipated expansion in urban development.

Physical Constraints and Building Scale

Glenferrie Road and High Street are located in a highly-established urban area, with small lot sizes and heritage values, which effectively limit re-development opportunities in the centre.

Additionally, the overall height and scale of buildings in the centre remains modest relative to development occurring at nearby centres, including Chapel Street, Caulfield and Elsternwick. Where development above three storeys occurs it is relatively 'hidden' from general view for most shoppers and visitors. Even the dedicated commercial office buildings near Dandenong Road are of modest scale (typically 3-storey) relative to commercial office development in other city fringe markets such as Richmond and Fitzroy.

Generally small lot sizes, heritage values and the low-scale of existing development in the Glenferrie Road and High Street locality will have implications for the type and extent of the future physical scale of development.

Response: The Structure Plan should support planning and built form guidelines that reflect the fine grained nature of development. The Plan should identify and promote the development of large sites where these are available. Where appropriate the Plan should support site consolidation.

6.2 Opportunities

The following opportunities have been identified for Glenferrie Road-High Street that will require an appropriate response in the Structure Plan.

New Forms of Niche Retailing

The number of traders operating in particular market niches (eg, bridal boutiques and antique shops) that are usually not present at other similar centres means that High Street attracts customers from across Melbourne. This has positive impacts on the trading levels of the more traditional retailers in the centre as shoppers attracted to (say) the antique shops can also direct expenditures to other retailers (eg, newsagents and books, cafes and coffee shops, etc). According to the High Street Shopper Survey 2012, the unique/speciality shops, fashion boutiques, and the quality of shops, were mentioned in the top 5 factors that respondents like about High Street.

Recently however, these types of niche traders have been declining in numbers, and this has had implications on the number of 'visitors' (ie residents from beyond the Main Trade Area) attracted to the centre. According to the High Street 2008 Shopper Survey, approximately 65% of shoppers in High Street were 'visitors' to the centre (living beyond local and surrounding postcodes); in 2012, only 47% of shoppers in High Street were 'visitors' to the centre, according to the 2012 survey.

Response: Potential exists to build on the history of High Street and Glenferrie Road as a unique Melbourne shopping destination that does things differently and better than other shopping centres. This means finding new niches in the retail sector and attracting supporting activities (such as urban manufacturing, professional services), providing attractive amenities, and developing an enhanced sense of 'place' for the centre.

Celebrate Trader Diversity

A very diverse range of traders are located across Glenferrie Road and High Street that is comparable to only a small number of locations in Melbourne, such as the CBD. For example, the David Jones mini-department store trades in proximity to an opportunity shop and cluster of medical consulting rooms, while High Street is characterised by bridal boutiques and high-end fashion, antique shops and high-end food retailing. The centre therefore captures a broad range of shoppers and other visitors.

Response: The Structure Plan needs to create greater awareness of the diversity which exists in High Street-Glenferrie Road in order to generate higher levels of multi-purpose shopping trips.

Street-Based Atmosphere

Glenferrie Road-High Street is overwhelmingly street-based, with the only enclosed components being the Malvern Central Shopping Centre and a number of arcades primarily linking to rear carparking. It is the unique street-based atmosphere which has historically earned community support.

Response: The Structure Plan must recognise the importance of retaining the centre's popularity with residents, despite the need to accommodate change over time. Retaining the popularity of Glenferrie Road-High Street with shoppers is essential in the face of increased competitive pressure from other centres.

Transport Rich

The centre is 'transport rich' in terms of access to road, rail and tram facilities and services, and active forms of access to the centre in walking and cycling. Within the centre a generally high standard of pedestrian activity and amenity is evident and this benefits both shoppers and traders. In particular, trams support the linear main street format and are a key component in the integration of centre.

Response: Growth opportunities in residential and commercial office development have the potential to leverage off these high levels of access to road, rail and tram facilities and services.

Improve Linkages with Anchors

A number of key activity generators are located within or on the periphery of Glenferrie Road-High Street, including:

- **Malvern Central** is a sub-regional centre containing approximately 15,000m² of retail floorspace and generating approximately \$ 120 million in turnover per year, and it attracts some 2.9 million visitors per year, according to the Property Council of Australia. Vehicular access to Malvern Central is via Glenferrie Road and Wattleree Road, while pedestrian access is only available via Wattleree Road.

- **Cabrini Hospital** is located a short distance east of the Glenferrie Road strip and has a relatively strong influence on the centre. The hospital, with 508 beds and a diverse range of medical and para-medical services located on-site, attracts significant numbers of staff, patients and visitors, a share of whom also visit the nearby activity centre who may not otherwise visit the centre as they do not live locally. The Cabrini Hospital also supports a large number of consulting rooms and medical services that are located within or on the fringe of the activity centre, and these medical facilities also attract people to the retail and other services in the activity centre.
- **Malvern Town Hall, City of Stonnington Offices and Police Station** which are located in the civic precinct in the north-east corner of the study area. Additionally, Council offices are expanding to the old Transfield Services Building at 311 Glenferrie Road which will further increase the exposure and influence of the precinct on the balance of the centre.
- **Other (non-Retail) activity generators** located within or on the periphery of Glenferrie Road, include the Malvern Tram Depot, Malvern and Armadale Train Stations, De La Salle College, and St Joseph's Primary School.

***Response:** Opportunities may exist to improve integration of key anchors the balance of the activity centre. For example, through increased development of medical and para-medical uses adjacent to Cabrini Hospital, and improved access links near the intersection of High Street and Glenferrie Road.*

7 ACTION PLAN

This Chapter presents an action plan comprising a range of strategies that aim to achieve the objectives of the Structure Plan.

7.1 Centre Heart

- 1 **Develop a strategy for the intersection of High Street and Glenferrie Road** to become the symbolic ‘heart’ of the centre.

7.2 Retail Objectives

- 2 **To support more-intensive retail activity** by encouraging the redevelopment of the sites and tenancies in the centre. In particular we advocate an expansion of the existing Coles store and the potential for Malvern Central to expand in a way that improves integration with the surrounding activity centre.
- 3 **To support comparison shopping in Glenferrie Road-High Street**, with a particular emphasis on niche, independent and other non-chain stores (as appropriate, and not undermining the important role of major chain stores) by supporting appropriate retail floorspace growth, and ensuring these traders are well-represented and involved in marketing activities.
- 4 **To continue to work on improving the streetscape on Glenferrie Road-High Street** to ensure that the centre continues to be seen as ‘best practice’ in Melbourne as a suburban street-based shopping and commercial precinct.
- 5 **To create an evening shopping environment which is inviting, comfortable and safe.** Ensure that businesses seeking to trade in the evening are supported.

7.3 Other Objectives

- 6 **To allow flexibility for the provision of additional commercial floorspace** should demand/interest eventuate.
- 7 **To work with property owners to unlock the potential for higher density-housing and other above-shop commercial development.** This includes ensuring that the height limits applying in the centre are clearly understood by property owners and developers.
- 8 **To emphasise to developers the opportunity to incorporate residential and office components into mixed-use development schemes** during pre-application discussions.
- 9 **To review, and if necessary upgrade, cycling access and infrastructure to reflect the growing popularity of this form of transport.** Where appropriate, provide bike racks or storage that mean cyclists can leave their bikes in a location that is out of the way of traders and pedestrians. Support other improvements for public transport users and people walking to and from the centre.
- 10 **To accommodate higher residential densities**, particularly in Princes Highway precinct.
- 11 **To recognise, without precluding retail and commercial development opportunities, the concentration of community and civic functions on the north-eastern fringe of the centre.**

Maximise the opportunities for the cross-use of these community facilities with the balance of the centre located to the south and west.

12 **To implement a set of performance benchmarks for marketing and promotional activities** based on the following four themes:

- a) The activity will increase the amount of consumer spending in Glenferrie Road-High Street
- b) The activity will increase spending by existing customers, and/or generate sales from new customers
- c) The activity will generate ongoing visitation into the future, either directly or indirectly.

8 CRITICAL SUCCESS FACTORS

DOs

- 1 Do engage proactively with developers and property owners so that the development aspirations and objectives for the centre are clearly understood
- 2 Do encourage more intensive residential development in the centre of an appropriate scale
- 3 Do improve the pedestrian accessibility and amenity throughout the centre, including permeability at cross streets and across Glenferrie Road and High Street
- 4 Do encourage a strong evening and weekend economy
- 5 Do recognise the functional differences between High Street, Glenferrie Road, Malvern Central and the balance of other precincts located in the activity centre
- 6 Do celebrate and highlight the special attributes Glenferrie Road-High Street enjoy over other shopping destinations in Melbourne.

DON'Ts

- 7 Don't forget to engage with traders and other stakeholders
- 8 Don't take the success of Glenferrie Road-High Street for granted, as retail is one of most dynamic and competitive sectors of the economy and **every** centre needs to evolve over time
- 9 Don't under-estimate the ability of centre revitalisation to create a wide range of economic and social benefits throughout the community
- 10 Don't waste time and energy on things you can't influence, so focus on practical and real-world outcomes.

City of Stonnington

**Glenferrie Road-High Street Structure Plan
Social Impact Assessment**

Report prepared by
ASR Research Pty Ltd
Social and Leisure Planners
November 2014

Executive Summary

Aims of Assessment

The aims of the Social Impact Assessment (SIA) are to:

- Identify the negative and positive social impacts - including an assessment of the severity/significance of these impacts - that could arise from the implementation of the *final draft Glenferrie Rd-High St Structure Plan*.
- Identify the actions that could be taken to mitigate/moderate the negative impacts and guarantee and potentially augment the positive impacts.

The findings/recommendations of the Assessment will inform the *final Glenferrie Rd-High St Structure Plan*.

Focus of SIA

The primary focus of the SIA is on the development/change that may occur in the geographic area includes the High St, Glenferrie Rd and Dandenong Rd spines (also known as the Glenferrie Rd-High St Activity Centre), the Malvern and Armadale train stations, the Malvern tram depot, the Malvern Civic/Recreation/ Education Precinct and the Wattletree Rd Health Precinct (the Assessment Area).

The SIA also considers the development/change that may occur in the residential areas immediately east and west of the Glenferrie Road Spine (including Union St Reserve) and the Malvern Public Gardens.

Potential change

The potential changes proposed or identified in the draft Local Structure Plan are as follows:

- Modest increase in infill development along High St and Glenferrie Rd
- Additional commercial and residential development to the south of the railway alignment near Dandenong Rd.
- An enhanced walking environment.
- Additional cycling infrastructure.

- The Wattletree Rd spine between Glenferrie Rd and Cabrini developed as a 'medical precinct'.
- An enhanced gateway to Glenferrie Rd from Dandenong Rd and improved presentation of carparking areas and train stations.
- Public realm improvements at strategic locations through the Assessment Area, e.g. at intersections, around the Council Offices/Town Hall, Library, Tram Depot and Cricket Ground.

Impacts

Positive

- Increased local population creating more custom for the retail and hospitality outlets and potentially increasing the number of local small/home based businesses.
- Construction work and the opening of new retail/commercial outlets creating employment opportunities for local residents, particularly local tradespeople, youth and young adults.
- Redevelopment providing an opportunity to remove/redevelop unattractive buildings and replace them with buildings that better suit the visual character of the Activity Centre and Assessment Area.
- Development around Malvern Station improving the appearance of what is a less attractive area than the sections of Glenferrie Rd to the immediate north.
- Redevelopment expanding the dining and entertainment options of residents, workers and visitors.
- Redevelopment providing the opportunity for older residents living in the Malvern and Armadale to 'age in place' by downsizing but still living in their local area.
- Improved walkability and increased cycling activity:
 - Increasing physical activity levels
 - Making it easier and safer to get to destinations around the Assessment Area
 - Generally enhancing the attractiveness of the Activity Centre and Assessment Area

- Discouraging locals from using their cars to travel to the Activity Centre and as a result reducing traffic congestion and demand for carparking
- Increasing economic activity in the Activity Centre and boosting business profits, employment and local prosperity
- Improving safety and surveillance.
- Construction work and the physical changes made to improve walkability and encourage cycling slowing down the traffic flow through the Activity Centre. This will be positive for the people living, working and visiting the Activity Centre – less traffic noise, safer streets and less through traffic as drivers take alternative routes.
- The definition and promotion of the Wattletree Rd Medical Precinct growing the profile of medical services in the Precinct and improving access by concentrating and containing medical facilities in a defined area (note: the containment of facilities will be of particular benefit to residents living near the Cabrini Hospital who are concerned about the creep of medical facilities into the residential areas).
- Public realm improvements making the Activity Centre more visually appealing, increasing activity in some areas and helping to activate other areas that are seldom used. These improvements will draw more people into the outdoor spaces in the Activity Centre. This will have a positive impact on community pride and foster increased social interaction.

Negative Impacts

- Changes to the visual appearance of the Activity Centre along High St and Glenferrie Rd and near the Malvern Station with the skyline changing, views potentially being obstructed and new buildings looming over laneways and carparks.
- Construction causing noise and dust disturbance and potentially closing laneways and footpaths, impeding access to shops, offices and dwellings. This may have adverse short and possibly long term economic and health impacts on businesses, workers, customers and residents. Some businesses may be permanently affected by loss in custom.
- Existing retail outlets and dining and entertainment venues facing increased competition from new players and potentially making them less viable.
- Increased activity in the Activity Centre generally increasing the ‘noisiness’ of the Activity Centre, particularly in the evenings.

- Improved walkability making it more difficult for cars and trams and even bikes to travel through and around the Activity Centre and Assessment Area.
- More intense pedestrian and cycling activity increasing the potential for vehicle, cyclist and pedestrian conflict.
- Increased activity at and around late night entertainment venues potentially creating an uncomfortable environment for local residents and people leaving dining venues.
- The infrastructure installed to encourage walking and cycling – seats, bike parking facilities, signs, crossing treatments - adding clutter to the streets. This infrastructure may be visually intrusive, conflict with outdoor dining facilities and impede pedestrian flow during busy periods.
- Some sections of the local community not welcoming the changes and wanting the Area to stay as it is. This may result in the community becoming less cohesive and a divide forming between new and old residents or residents living in the new residential developments along High St and those living in the local streets.

Augmentation/mitigation/moderation strategies

The following actions could potentially augment the positive impacts and mitigate/moderate the negative impacts:

- Extensive and effective community engagement.
- Strict controls over any activity that may have an adverse impact and enforcement of these controls.
- Sensitive and careful urban and building design.
- Promotion of universal and age, youth, child and family friendly design principles.
- Education of builders/developers/proprietors of noisy venues etc.
- Measures that promote social interaction and build relationships.

Other comments

The draft Structure Plan is consistent with and will help to achieve many of the objectives that Council considers to be important with respect to urban design, community health and well-being and environmental sustainability. The Plan also responds in a comprehensive and effective manner to the community’s vision for the Glenferrie Rd-High St Activity Centre.

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1. Aims of Assessment

The aims of the Social Impact Assessment (SIA) are to:

- Identify the negative and positive social impacts - including an assessment of the severity/significance of these impacts - that could arise from the implementation of the *final draft Glenferrie Rd-High St Structure Plan*.
- Identify the actions that could be taken to mitigate/moderate the negative impacts and guarantee and potentially augment the positive impacts.

The Assessment examines:

- The social impacts of the physical, demographic, environmental, visual and economic change that could result from the implementation of the draft Glenferrie Rd-High St Structure Plan.
- The implications that this change will have on the access of local residents to services and facilities located in or near the Structure Plan Area.

The Assessment specifically focuses on the appropriateness of change outcomes pertaining to community use, community connectedness, social inclusion, safety, health, universal access and ageing in place.

The findings/recommendations of the Assessment will inform the *final Glenferrie Rd-High St Structure Plan*.

2. Assessment/Study Areas

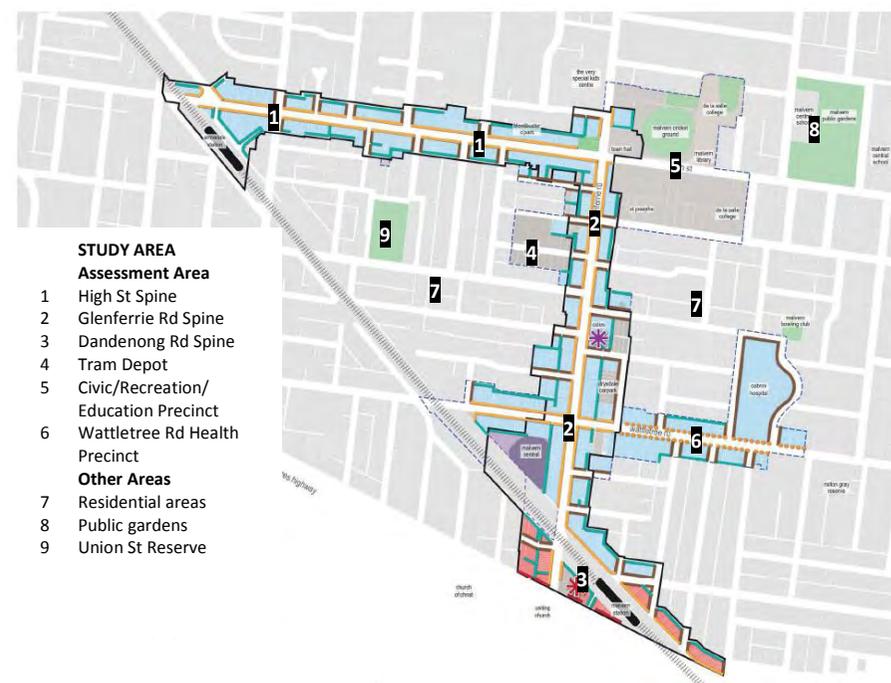
The primary focus of the SIA is on the development/change that may occur in the geographic area shown in Figure 1. This area includes the High St, Glenferrie Rd and Dandenong Rd spines (also known as the Glenferrie Rd-High St Activity Centre), the Malvern and Armadale train stations, the Malvern tram depot, the Malvern Civic/Recreation/Education Precinct and the Wattletree Rd Health Precinct.

The SIA will also consider the development/change that may occur in the residential areas immediately east and west of the Glenferrie Road Spine (including Union St Reserve) and the Malvern Public Gardens.

For the purposes of this report:

- The area which includes the High St, Glenferrie Rd and Dandenong Rd spines is referred to as the Glenferrie Rd-High St Activity Centre.
- The primary focus area is referred to as the Assessment Area.
- The Assessment Area combined with the adjacent residential areas, Union St Reserve and the public gardens are referred to as the Study Area.

Figure 1 – Assessment/Study Areas



3. The Study Area

3.1 Key features/attributes

The Study Area is located about 7kms from the Melbourne CBD. Its significant features/attributes are as follows:

- A commercial/retail activity centre extending along High St, Glenferrie Rd and Dandenong Rd which has approximately 70,000m² of retail floor space and 30,000m² of commercial space. The retail floor space accommodates a diverse range of outlets and hospitality venues that cater for local and regional catchments.
- Four arterial roads traversing the Area – a highway (Dandenong Rd), two primary main roads (High St and Glenferrie Rd) and a secondary main road, Wattletree Rd.
- Tram routes along High St, Wattletree Rd and Glenferrie Rd which extend:
 - From Glen Iris and Malvern to the Melbourne CBD
 - From Kew to St Kilda.
- A large tram depot on Glenferrie Rd.
- The Pakenham Train Line and 2 train stations – Armadale and Malvern.
- A prominent civic/recreation/education precinct comprising Council offices, town hall, sports reserve, secondary college, 2 primary schools, church, police station, retirement living centre and public gardens.
- A local reserve, Union St, which contains play facilities and a public tennis venue.
- A large private hospital on Wattletree Rd with associated medical clinics.
- Built form which has significant heritage values and visual quality.
- Attractive, accessible and relatively quiet residential areas.

3.2 Demographic characteristics

3.2.1 The Study Area

The Study Area falls within the suburbs of Armadale and Malvern. The demographic characteristics of these suburbs are as follows:

- Compared to Greater Melbourne, Armadale and Malvern have:
 - Significantly higher proportions of females than Greater Melbourne and conversely lower proportions of males

- Significantly higher median household incomes.
- Compared to Armadale and Greater Melbourne, Malvern has:
 - A higher proportion of ‘couple with children’ households
 - A higher proportion of Australian born residents.
- Compared to Malvern and Greater Melbourne, Armadale has:
 - Significantly lower proportions of ‘couple with children’ households
 - A significantly higher proportion of young adults
 - A significantly higher proportion of ‘sole person’ households.

Table 1 – Population Characteristics

Characteristic	Armadale	Malvern	Greater Melbourne
Est. population 2011	8,760	9,504	4,000,000
Male	47.0	47.0	49.2
Female as % of total population	53.0	53.0	50.8
Child population (0-9 years) as % of total population	9.5	12.3	12.5
Youth population (10-19 years) as % of total population	7.6	10.8	11.2
Young adult population (20-34 years) as % of total population	30.0	21.0	22.9
Middle adult population (35-49 years) as % of total population	21.0	22.0	22.0
Older middle adult population (50-69 years) as % of total population	21.0	23.0	20.0
Older adult (70+) as % of total population	10.0	10.0	9.2
Australian born	65.0	71.0	64.0
Ave household size	2.1	2.5	2.6
Median weekly household income	\$1,711	\$2,017	\$1,333
Couple family with children as % of all households	36.0	50.0	47.9
Couple family without children as % of all households	49.5	37.0	34.8
One parent families as % of all households	11.0	10.0	15.3
Sole person households	35.0	27.0	23.3

Source – ABS Quickstats 2011

3.2.2 Dwellings in the Activity Centre

In 2011, the Glenferrie Rd-High St Activity Centre contained around 165 dwellings and 270 residents. This is an increase of 55 dwellings and 85 residents since 2006. Some higher density development is now occurring in High St and near the Malvern Station and more development will occur in these areas in the future (source: GR-HS Activity Centre, Economic Technical Report 2014, Essential Economics).

3.2.3 People working, studying, shopping etc in the Study Area

It is estimated that 9,500 people work and 1,700 children study in Study Area. The majority of workers are managers, professionals, clerical staff and sales staff.

The primary catchment area (main trade area) for the Glenferrie Rd-High St Activity Centre includes the suburbs of Malvern, Armadale and Tooronga and parts of East Malvern, Toorak, Caulfield North and Prahran. This area has a population of around 62,000 people (source: GR-HS Activity Centre, Economic Technical Report 2014, Essential Economics).

3.3 Social Infrastructure in the Study Area

The following social infrastructure items are located in the Assessment Area and remainder of the Study Area:

In the Assessment Area

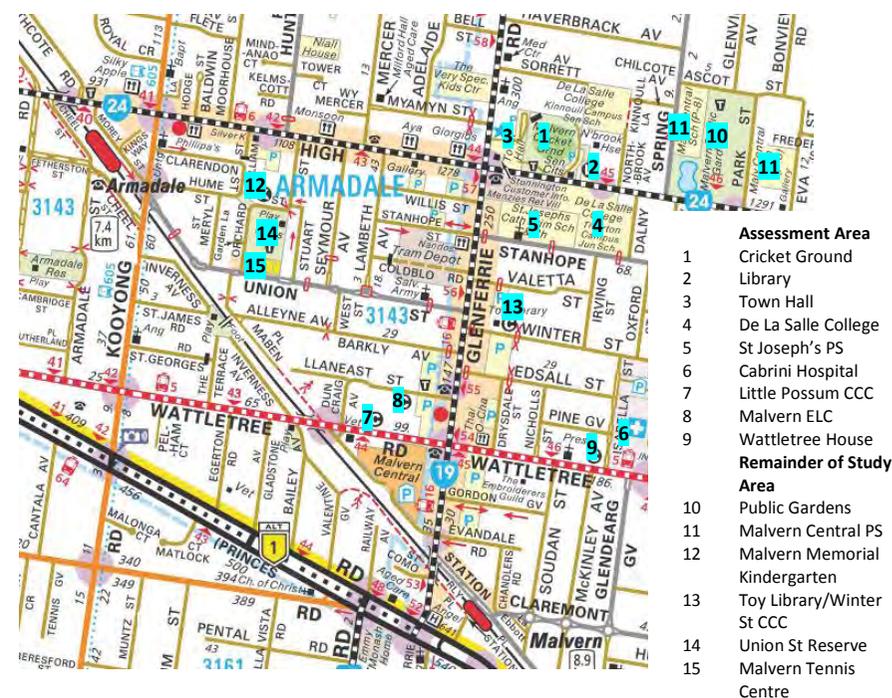
- Malvern Cricket Ground
- Malvern Library
- Malvern Town Hall
- Malvern Square
- De La Salle College
- St Joseph’s Primary School
- Cabrini Hospital
- Little Possums Child Care Centre
- Malvern Early Learning Centre
- Wattletree House Child Care Centre.

In the remainder of the Study Area

- Malvern Central Primary School
- Malvern Public Gardens
- Union St Reserve and Malvern Tennis Facility
- Malvern Memorial Kindergarten
- Stonnington Toy Library/Winter St Child Care Centre

There are also a number of social infrastructure items located close to the Study Area which could be used by residents of the Area (not mapped below). These items include a childcare centre, kindergarten, M&CH centre, a private girls’ school and aged care facilities.

Figure 2 – Social Infrastructure in the Study Area



4. Planning/Policy context

There are a number of Council planning and policy documents that need to be considered when undertaking the social impact assessment. These are:

- Council Plan 2013 – 2017
- Municipal Strategic Statement
- Municipal Public Health and Wellbeing Plan 2013-2017
- Activity Area Economic Analysis 2013

- Building Prosperity - Economic Development Strategy 2012-2016
- Public Realm Strategy 2010
- Sustainable Transport Policy/Walking Policy 2008
- Access and Inclusion Plan – A City for All 2014-2017
- Arts and Cultural Strategy 2011-15
- Youth Strategy 2010-2014
- Older Person’s Strategy 2008-2012
- Early Years Plan 2011-2016
- Cycle Strategy 2013-2018.

These documents indicate that Council encourages or places a high degree of importance on the following activities/facilities/matters with respect to land use planning, urban design and social infrastructure provision:

- Enhanced community health, well-being and safety.
- A community where all people feel connected.
- Appropriate housing available to all residents.
- High quality, accessible social infrastructure that meets needs.
- Sustainable transport including increased use of bikes and public transport and less uses of cars.
- Greening of streets.
- Reduction of carbon footprint.
- Community gathering places.
- Protection/preservation of heritage architecture.
- Protection/preservation/enhancement of landscape character.
- Vibrant retail and commercial sectors.
- Residential development in areas that have good access to public transport.
- Protection of residential amenity.
- Expanded and enhanced public realm.
- Equitable distribution of open space.
- Public places that supports/encourage formal and informal arts activity.
- Safe, walkable and accessible environments.
- Accessible buildings.

- Child friendly spaces.
- Opportunities for youth in the eastern end of Stonnington.
- ‘Main roads with trams’ converted to ‘main streets’ where residents, cyclists and public transport have priority.

5. The community’s views

Council asked the local community’s views to indicate its vision for the future development of the Assessment Area and the potential improvements that could be made to the Area. This two-stage consultation process involved forums with residents, business owners/traders, traders associations, local service providers and other key stakeholders. The responses were as follows:

- The community envisioned that The Assessment Area would be:
 - A thriving shopping precinct which caters in a balanced way for local and specialist retailers
 - A vibrant, inviting, attractive and clean space with well-designed green streetscapes, pedestrian connections and congregation spaces
 - A centre for local employment.
- The community:
 - Wanted more areas converted to green spaces
 - Wanted public spaces (carparks, laneways) activated through improved lighting, way-finding and landscaping
 - Wanted a considered approach to development that balances functional needs and heritage protection
 - Supported additional but ‘delicate’ residential and retail/commercial development as long as it was strictly managed, residential amenity was protected and existing commercial uses were not displaced
 - Supported the notion of a health precinct along Wattle tree Rd.
- With respect to individual sites in the Assessment Area, the community recommended that:
 - The Glenferrie Rd gateway (from Dandenong Rd) be enhanced: aged care, cinema, residential development, possibly late night venues
 - A green corridor be created between the railway line and Malvern Central

- Consideration be given to developing over the Malvern Station and railway line
- Access to the Malvern Station train be improved
- The Armadale Station be activated: improved public realm, shared zones, connections to surrounding areas, carparking, way-finding and landscaping
- The potential for activating the Tram Depot be explored
- The Malvern Town Hall/Civic Precinct be further activated: walking circuit, way-finding, improved landscaping
- Malvern Square be enhanced: more vegetation, congregation areas, better lighting
- Carparks be retained
- Rear carpark be optimised as public realm: more green spaces, lighting, public art
- Streets be considered as community hubs where pedestrians are given priority
- Facilities for cycling be provided: on and off-road routes, bike parking facilities, way-finding
- The second storeys of buildings and shop-tops be used for residential/office space
- Public toilets be provided
- Increased street life, including night life, be encouraged as long as activities are sensitive to surrounding residents.

6. Potential change in the Study Area proposed in the draft Structure Plan

The potential changes proposed or identified in the draft Local Structure Plan are as follows:

6.1 Physical change

- Modest increase in infill development along High St and Glenferrie Rd (mainly above shop residential and commercial development).
- Additional commercial and residential development to the south of the railway alignment near Dandenong Rd.
- An enhanced walking environment through:
 - The addition of pedestrian crossings at strategic locations, e.g. on Glenferrie Rd between High St and the crossing near Edsall St (investigation)
 - Improvements to footpath surfacing along High St

- Erection of way-finding signage
- Development of 'green routes' with high quality walking/cycling surfaces, safe road crossing points, seating and security lighting
- Improvements to arcades and laneways to make them safer and more attractive, such as better paving, lighting, seating etc.
- Additional cycling infrastructure including:
 - Long stay bike parking facilities at Armadale and Malvern stations, community facilities, cafes and parks
 - Bicycle lanes on Glenferrie Rd and High St and bicycle priority markings at signalised crossings
 - Crossing and path treatments/connections that enhance the cycling environment, e.g. connecting Union St Reserve to High St through a cycling bypass, converting off-road pedestrian paths to cycling paths.
- The Wattleree Rd spine between Glenferrie Rd and Cabrini developed as a 'medical precinct'.
- An enhanced gateway to Glenferrie Rd from Dandenong Rd and improved presentation of carparking areas and train stations.
- Public realm improvements at strategic locations through the Study Area, e.g. at intersections, around the Council Offices/Town Hall, Library, Tram Depot and Cricket Ground.

6.2 Visual change

The visual changes will largely result from the physical changes described in Section 6.1. People who live in, work, visit or travel through the Study Area will notice the following visual changes:

- More mixed use, above shop development along High St and Glenferrie Rd.
- Additional commercial/industrial development along Dandenong Rd and around Malvern Station.
- A generally 'greener' environment.
- Busier streets and pavements with more people walking, cycling, eating, shopping and gathering.
- Improved vistas and more attractive public spaces and buildings.

6.3 Demographic change

The Economic Technical Report (2014) suggests that the following dwelling and population growth could occur in or near the Study Area over the next 17 years:

- Around 850 dwellings and 1,500 people in the Glenferrie Rd-High St Activity Centre (based on 50 dwellings and 90 people per annum).
- 1,020 dwellings and around 2,040 people in Malvern and Armadale (based on 60 dwellings and 120 people per annum).
- 6,000 people across the catchment (or trade) area for the Glenferrie Rd-High St Activity Centre.

The Technical Report and draft Structure Plan does not make any comment on the change in the demographic make-up of the Glenferrie Rd-High St Activity Centre or Malvern and Armadale that will result from the population growth outlined above. As the majority of the additional dwellings will be moderately priced apartments (probably 1 and 2 bedroom), it is likely that:

- The demographic characteristics of the Malvern community will remain similar to what they are now.
- The demographic characteristics of the Armadale community will change moderately. With more apartment development, there is likely to be a reduction in the average household size, a slight decline in real terms in median household income and rises in the proportion of sole person households and young adult population as a proportion of total population.

6.4 Economic change

The draft Structure Plan suggests that economic activity in the Assessment Area will increase as a result of growth in the local resident population and the catchment area of the Glenferrie Rd-High St Activity Centre and works which will make the Centre more attractive to shoppers and visitors. The Plan forecasts that, as a result of this increased activity, the amount of retail and commercial floor space in the Glenferrie Rd-High St Activity Centre could rise by between 11,000m² (15%) and 20,000m² (66%) respectively.

7. Impact Assessment

7.1 General impacts

The community that will be impacted by the change in the Assessment Area can be divided into the following segments:

- Current residents, business owners, workers and students in and visitors to the Activity Centre.
- Future residents, workers and students in and visitors to the Activity Centre.
- Current residents of the residential areas in the Assessment Area - east and west of Glenferrie Rd.
- Future residents of the residential areas in the Assessment Area - east and west of Glenferrie Rd.
- People who regularly drive through the Assessment Area.
- People who regularly travel through the Assessment Area on public transport.

The change will impact these segments differently. For example, some existing residents may find the Activity Centre more attractive, lively and safer to move around. They will also have more shopping, eating and entertainment options. They will, most likely, consider these impacts to be positive. Conversely, some existing residents may find the Activity Centre too busy. They may have to constantly evade bicycles, be regularly disturbed by the noise of building work, have their access to their homes temporarily impeded by road and building works, have to compete for carpark spaces, have their views obstructed by new buildings, have to queue for their Saturday morning coffees and be unnerved at night walking down busier streets. They will find these impacts to be negative.

Future residents of the Activity Centre may have an entirely different experience; whether it is a positive or negative experience largely depending on where they have moved from and what expectations that have of their new local area. Some new residents will be pleased by their easy access to public transport and diverse shopping, dining and entertaining opportunities. However, others may be overwhelmed by the 'busyness and loudness' of the Centre. Some business owners may be pleased by the increase in shopper and diner numbers which results in more custom but others may be disheartened if this increased demand results in more competitors being attracted to the Centre.

7.2 Specific Impacts

The positive and negative impacts of the change that could occur in the Assessment Area on the various community segments and the potential severity/significance of these impacts are discussed in Table 2 on the following page.

In undertaking the SIA:

- It was assumed that the strategies outlined in the draft Structure Plan would be fully implemented which, in turn, would cause the changes outlined in Section 6 of this report to occur.
- An evaluation was made of how consistent the changes outlined in Section 6 of this report were with the views expressed by the community (see Section 5) and the matters that Council considers to be important with respect to Activity Centre development, urban design and social infrastructure provision (see Section 4).

In summary, the positive and negative impacts and the actions that could be taken to moderate/mitigate the negative impacts are as follows:

Positive Impacts

- Increased local population creating more custom for the retail and hospitality outlets and potentially increasing the number of local small/home based businesses.
- Construction work and the opening of new retail/commercial outlets creating employment opportunities for local residents, particularly local tradespeople, youth and young adults.
- Redevelopment providing an opportunity to remove/redevelop unattractive buildings and replace them with buildings that better suit the visual character of the Activity Centre and Assessment Area.
- Development around Malvern Station improving the appearance of what is a less attractive area than the sections of Glenferrie Rd to the immediate north.
- Redevelopment expanding the dining and entertainment options of residents, workers and visitors.

- Redevelopment providing the opportunity for older residents living in the Malvern and Armadale to 'age in place' by downsizing but still living in their local area.
- Improved walkability and increased cycling activity:
 - Increasing physical activity levels
 - Making it easier and safer to get to destinations around the Assessment Area
 - Generally enhancing the attractiveness of the Activity Centre and Assessment Area
 - Discouraging locals from using their cars to travel to the Activity Centre and as a result reducing traffic congestion and demand for carparking
 - Increasing economic activity in the Activity Centre and boosting business profits, employment and local prosperity
 - Improving safety and surveillance.
- Construction work and the physical changes made to improve walkability and encourage cycling slowing down the traffic flow through the Activity Centre. This will be positive for the people living, working and visiting the Activity Centre – less traffic noise, safer streets and less through traffic as drivers take alternative routes.
- The definition and promotion of the Wattletree Rd Medical Precinct growing the profile of medical services in the Precinct and improving access by concentrating and containing medical facilities in a defined area (note: the containment of facilities will be of particular benefit to residents living near the Cabrini Hospital who are concerned about the creep of medical facilities into the residential areas).
- Public realm improvements making the Activity Centre more visually appealing, increasing activity in some areas and helping to activate other areas that are seldom used. These improvements will draw more people into the outdoor spaces in the Activity Centre. This will have a positive impact on community pride and foster increased social interaction.

Negative Impacts

- Changes to the visual appearance of the Activity Centre along High St and Glenferrie Rd and near the Malvern Station with the skyline changing, views potentially being obstructed and new buildings looming over laneways and carparks.
- Construction causing noise and dust disturbance and potentially closing laneways and footpaths, impeding access to shops, offices and dwellings. This may have

adverse short and possibly long term economic and health impacts on businesses, workers, customers and residents. Some businesses may be permanently affected by loss in custom.

- Existing retail outlets and dining and entertainment venues facing increased competition from new players and potentially making them less viable.
- Increased activity in the Activity Centre generally increasing the ‘noisiness’ of the Activity Centre, particularly in the evenings.
- Improved walkability making it more difficult for cars and trams and even bikes to travel through and around the Activity Centre and Assessment Area.
- More intense pedestrian and cycling activity increasing the potential for vehicle, cyclist and pedestrian conflict.
- Increased activity at and around late night entertainment venues potentially creating an uncomfortable environment for local residents and people leaving dining venues.
- The infrastructure installed to encourage walking and cycling – seats, bike parking facilities, signs, crossing treatments - adding clutter to the streets. This infrastructure may be visually intrusive, conflict with outdoor dining facilities and impede pedestrian flow during busy periods.
- Some sections of the local community not welcoming the changes and wanting the Area to stay as it is. This may result in the community becoming less cohesive and a divide forming between new and old residents or residents living in the new residential developments along High St and those living in the local streets.

Augmentation/mitigation/moderation strategies

The following actions could potentially augment the positive impacts and mitigate/moderate the negative impacts (also see table 3):

- Extensive and effective community engagement.
- Strict controls over any activity that may have an adverse impact and enforcement of these controls.
- Sensitive and careful urban and building design.
- Promotion of universal and age, youth, child and family friendly design principles.
- Education of builders/developers/proprietors of noisy venues etc.

- Measures that promote social interaction and build relationships.

7.4 Achieving Council’s objectives

The draft Structure Plan is consistent with and will help to achieve many of the objectives that Council considers to be important (see Section 4 of this report). The implementation of the Plan will:

- Enhance community health, well-being and safety.
- Foster sustainable transport including increased use of bikes and public transport and less uses of cars.
- Green the streets.
- Provide more community gathering places.
- Protect/preserve heritage architecture.
- Enhance landscape character.
- Create vibrant retail and commercial sectors.
- Create residential development in areas that have good access to public transport.
- Protect residential amenity.
- Expand and enhance public realm.
- Create a safe, walkable and accessible environment.
- Convert Glenferrie Rd and High St from ‘main roads with trams’ to ‘main streets’ where residents, cyclists and public transport have priority.

7.3 Responding to the community’s vision

The draft Structure Plan responds in a comprehensive and effective manner to the community’s vision for the Glenferrie Rd – High St Activity Centre (see Section 5 of this report). All the views suggested in the consultation process have been covered in the Plan.

An area that is not mentioned in the draft Structure Plan but should be considered for inclusion is the provision of accommodation options for the elderly. More emphasis could be given in the Plan to providing local elderly residents with the opportunity to ‘age in place’.

Table 2 – Social Impact Assessment

CHANGE	IMPACTS	TYPE OF IMPACT			POTENTIAL DEGREE OF IMPACT
		POSITIVE	NEGATIVE	NEUTRAL	
Modest increase in infill development along High St and Glenferrie Rd (mainly residential and commercial development above shops).	<p>The draft Local Structure Plan anticipates that infill residential development over the next 17 years will generate an additional 800-900 dwellings and 1,500-1600 people in the Glenferrie Rd-High St Activity Centre and 1,020 dwellings and 2,040 people in Malvern and Armadale. It is anticipated that the dwelling and population growth will be gradual and relatively even over the 17 years.</p> <p>Most of the population increase will occur in Armadale and cause a moderate change to its demographics, i.e.</p> <ul style="list-style-type: none"> - A slight reduction in the average household size and decline in real terms in median household income - Increase in sole person households as a proportion of all households - Increase in the young adult population as a proportion of total population. <p>The demographic characteristics of the Malvern community will be largely unaltered by population growth. Its community will continue to age, but otherwise there will be little change.</p> <p>Therefore, from a social cohesion perspective, population growth will have little impact. The residents living in the area will notice little change, other than Armadale residents potentially discerning that their community is becoming younger.</p>			√	
	Although reasonably modest, the increase in population in Glenferrie Rd-High St Activity Centre and across Malvern and Armadale will lead to increased activity in the Activity Centre and more custom for the retail and hospitality outlets. This will have a positive impact on economic activity and local prosperity.	√			Moderate
	The increased population will place additional demand on local services and facilities – local kindergartens, childcare centres, schools, tennis centres, sports field, local fitness centres etc. However, it is anticipated that the additional demand will be minimal due to the size of the increase and the demographic characteristics of the new residents. The bulk of new population is likely to be single young adults and couples without children. Therefore, there will be little extra demand for early years services or schools and some minor additional demand for fitness centres and casual tennis.	√			Minor

CHANGE	IMPACTS	TYPE OF IMPACT			POTENTIAL DEGREE OF IMPACT
		POSITIVE	NEGATIVE	NEUTRAL	
	The draft Structure Plan anticipates that infill commercial development over the next 17 years will generate an additional 11,000m ² and 20,000m ² of retail and commercial floor space respectively. This extra floorspace will support additional employment and, potentially, provide more job opportunities for local residents, particularly local youth and young adults.	√			Moderate
	Infill development will change the visual appearance of the Activity Centre with some new buildings reaching as high as 3-4 storeys. Although development will be set back, it may obstruct the views for residents or workers who already occupy the second storey areas of existing buildings and loom over people using the back laneways and carparks.		√		Moderate but has the potential to be significant if not managed sensitively.
	Construction will cause noise and dust disturbance and potentially close laneways and footpaths, impeding access to shops, offices and dwellings. This may have adverse economic and health impacts on businesses, workers, customers and residents. Some businesses may be permanently affected by loss in custom.		√		Moderate but has the potential to be significant if multiple development occurs simultaneously and/or the disruption lasts for long periods.
	Construction may provide an opportunity to remove/redevelop unattractive buildings and replace them with buildings that better suit the visual character of the Activity Centre and Assessment Area.	√			Moderate but could be significant. It will depend on which sites are chosen for redevelopment.
	Development is likely to include the provision of new dining/café outlets and entertainment venues. This could have a positive impact in that it will expand the dining and entertainment options of residents and visitors. It could also have a negative impact in that it may make some existing venues less viable and generally increase the 'noisiness' of the Activity Centre, particularly in the evenings.	√	√		Positive – moderate Negative – minor but has the potential to be major if not managed carefully, particularly for businesses that have been at the Centre for a long time
	Construction work and the opening of new retail/commercial outlets may create employment opportunities for local residents/tradespeople.	√			Minor-moderate depending on the number of jobs and the take up/success rate of local residents and tradespeople in securing these jobs.
	The residential development will mainly be apartment buildings. This will provide opportunities for local older residents to downsize and 'age in place'.	√			Moderate

CHANGE	IMPACTS	TYPE OF IMPACT			POTENTIAL DEGREE OF IMPACT
		POSITIVE	NEGATIVE	NEUTRAL	
Additional commercial and residential development to the south of the railway alignment near Dandenong Rd.	Development in this location could have similar positive impacts to those outlined above but to a greater extent, particularly the negative impacts. The scale of development is likely to be more substantial than development in High St and Glenferrie Rd. Therefore, it could be noisier, more visually arresting (particularly as it may not be setback) and significantly change the character of the Area. It could also be beneficial. It could improve the vibrancy and the appearance of the area and create a gateway to the Activity Centre.	√	√		Positive – moderate Negative - moderate but could be significant if not carefully and sensitively managed.
An enhanced walking environment through: <ul style="list-style-type: none"> – The addition of pedestrian crossings at strategic locations, e.g. on Glenferrie Rd between High St and the crossing near Edsall St (investigation) – Improvements to footpath surfacing along High St – Erection of way finding signage – Development of ‘green routes’ with high quality walking/cycling surfaces, safe road crossing points, seating and security lighting – Improvements to arcades and laneways to make them safer and more attractive, such as better paving, lighting, seating etc. 	These changes will improve ‘walkability’. This will have significant benefits for people living, working and visiting the area. It will increase physical activity levels, make it easier and safer to get to destinations around and beyond the assessment area and generally enhance the attractiveness and vibrancy of the Activity Centre and Assessment Area. It may also discourage locals from using their cars to travel to the Activity Centre and as result reduce traffic congestion. The increased vibrancy of the Activity Centre will in turn increase economic activity in the Centre which will boost business profits, employment and prosperity.	√			Significant
	The improved walkability, however, may make it more difficult for cars and trams and even bikes to travel through and around the Activity Centre and Assessment Area. Some shoppers and workers cannot avoid using their cars and may feel aggrieved that their shopping or work experiences are made more difficult by these walkability enhancements. Also more intense pedestrian activity may increase the potential for vehicle and pedestrian conflict with pedestrians taking less care when they cross roads or walk down laneways etc.		√		Moderate
	Generally, increased activity in the Activity Centre should improve safety and surveillance. However, increased activity at or around late night entertainment venues may have the opposite effect with local residents and people leaving dining venues feeling threatened by unruly patrons leaving existing and potentially new late night venues.	√	√		Positive – moderate Negative – moderate but potentially significant if not managed carefully

CHANGE	IMPACTS	TYPE OF IMPACT			POTENTIAL DEGREE OF IMPACT
		POSITIVE	NEGATIVE	NEUTRAL	
<p>Additional cycling infrastructure including:</p> <ul style="list-style-type: none"> - Long stay bike parking facilities at Armadale and Malvern stations, community facilities, cafes and parks - Bicycle lanes on Glenferrie Rd and High St and bicycle priority markings at signalised crossings - Crossing and path treatments/connections that enhance the cycling environment, e.g. connecting Union St Reserve to High St through a cycling bypass, converting off-road pedestrian paths to cycling paths. 	<p>These changes should encourage more people to cycle to, around and from the Activity Centre and Assessment Area. This increased cycling activity could have significant benefits for people living, working and visiting the area – more physical activity, less traffic, less noise, less pressure on carparks, easier to get to destinations. Like ‘improved walkability’, it should enhance the vibrancy of the Activity Centre and Assessment Area which in turn will boost increase economic activity and local prosperity.</p>	√			Significant
	<p>Again like ‘improved walkability’, greater bicycle activity in the Activity Centre may increase the potential for cyclist, vehicle and pedestrian conflict which could result in disharmony and possible serious injuries.</p>		√		Moderate
	<p>The infrastructure proposed to be installed to encourage walking and cycling – seats, bike parking facilities, signs, crossing treatments - could add clutter to the streets. This infrastructure may conflict with outdoor dining facilities and impede pedestrian flow during busy periods. It may also be visually obtrusive.</p>		√		Moderate
	<p>The construction work and the physical changes made to improve walkability and encourage cycling will slow down the traffic flowing through the Activity Centre. This will be positive for the people living, working and visiting the Activity Centre – less traffic noise, safer streets etc, less through traffic as drivers take alternative routes – but it will be negative and frustrating for tram users and people driving through to get to further destinations.</p>	√			Positive for local residents, visitors and workers - moderate
<p>The Wattletree Rd spine between Glenferrie Rd and Cabrini developed as a ‘medical precinct’.</p>	<p>This strategy will concentrate medical activity in a defined precinct. In time, this will enhance the profile of the precinct, help to contain the spread of medical facilities and hopefully improve the accessibility of the services with people being able to access multiple services in the one location.</p> <p>The containment of facilities to a defined area will be of particular benefit to residents living near the Cabrini Hospital who are concerned about the creep of medical facilities into the residential areas.</p>	√			Moderate
<p>An enhanced gateway to Glenferrie Rd from Dandenong Rd and improved presentation of carparking areas and train stations.</p>	<p>An enhanced gateway will make the area more welcoming and visually appealing. It will have little social impact other than making people living and working in the area more proud of their local environment.</p>			√	

CHANGE	IMPACTS	TYPE OF IMPACT			POTENTIAL DEGREE OF IMPACT
		POSITIVE	NEGATIVE	NEUTRAL	
More gathering points and public realm improvements at strategic locations through the Study Area, e.g. at intersections, around the Council Offices/Town Hall, Library, Tram Depot, Cricket Ground, laneways, carparks etc.	Public realm improvements will make the Activity Centre more visually appealing. It will increase activity in some areas and help to activate other areas that are seldom used. It will draw more people into the outdoor spaces Activity Centre. It will have positive impact on community pride and foster increased community interaction	√			Significant
	Some sections of the local community, however, may not welcome the changes described above, particularly those living in the surrounding residential areas. They may prefer their local area to stay as it is. They may not want 'green routes' or to have queue for a coffee. As a result, the community may become less cohesive with a divide occurring between some new and old residents or between some residents living in the new residential developments along High St and those living in the local streets.		√		Moderate

Table 3 – Mitigation/moderation strategies

Impact	Strategies
Changes to the visual appearance of the Activity Centre along High St and Glenferrie Rd and near the Malvern Station with the skyline changing, views being obstructed and new buildings looming over laneways and carparks.	<ul style="list-style-type: none"> - Enforcing the building setbacks where necessary in the draft Local Structure Plan - Ensuring building design is consistent with local architectural character - Ensuring local residents, workers and business owners are appropriately informed about new development works
Construction causing noise and dust disturbance and potentially closing laneways and footpaths, impeding access to shops, offices and dwellings. This may have adverse economic and health impacts on businesses, workers, customers and residents. Some businesses may be permanently affected by loss in custom.	<ul style="list-style-type: none"> - Placing controls which will minimise the adverse impacts - Strictly enforcing these controls - Educating builders/developers - Ensuring local residents, workers and business owners are appropriately informed about any works etc that may impact on them - Investigating compensation measures for businesses that are adversely impacted
Existing retail outlets and dining and entertainment venues facing increased competition from new players and potentially making them less viable.	<ul style="list-style-type: none"> - There are no obvious mitigation strategies. Outlets and venues will have to develop their own strategies to deal with this competition - Council may be able to provide some support through its marketing and economic development units
More activity in the Activity Centre generally increasing the 'noisiness' of the Activity Centre, particularly in the evenings.	<ul style="list-style-type: none"> - Placing controls over 'noisy' venues and strictly enforcing these controls - Educating proprietors of venues
Increased activity at and around late night entertainment venues creating an uncomfortable environment for local residents and people leaving dining venues.	<ul style="list-style-type: none"> - Limiting the number of late night venues - Imposing liquor licensing controls - Conducting SIAs on late night venue applications - Placing appropriate controls over their activities and enforcing these controls - Educating licensees and staff - Installing CCTV at appropriate locations - Installing suitable lighting - Removing 'dark spots'
Improved walkability making it more difficult for cars and trams and even bikes to travel through and around the Activity Centre and Assessment Area. People who rely on cars may be upset by the changes.	<ul style="list-style-type: none"> - Ensuring that an appropriate balance is struck in the redesign and redevelopment of the Activity Centre and Assessment Area between the providing access to cars, improving walkability and encouraging cycling - Involving the community in this process
More intense pedestrian and cycling activity increasing the potential for vehicle, cyclist and pedestrian conflict.	<ul style="list-style-type: none"> - Ensuring that in the redesign and redevelopment of the Activity Centre and Assessment Area, potential conflict points between cyclists, pedestrians and vehicles are minimised. - Involving the community (residents and traders) in this process - Conducting education campaigns - Installing appropriate road treatments and signage
Infrastructure installed to encourage walking and cycling – seats, bike parking facilities, signs, crossing treatments - adding clutter to the streets. This infrastructure may be visually intrusive, conflict with outdoor dining facilities and impede pedestrian flow during busy periods.	<ul style="list-style-type: none"> - Ensuring these problem do not occur through sensitive and careful design - Involving the community (residents and traders) in this process
Some sections of the local community not welcoming the changes and wanting the Area to stay as it is.	<ul style="list-style-type: none"> - Consulting with the community (residents and traders) about the changes that will occur - Listening and being responsive to the views of existing residents - Implementing activities that will increase social interaction and build relationships between old and new residents

Glenferrie Road – High Street Structure Plan

Background Transportation Report

November 2014

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1. INTRODUCTION

O'Brien Traffic has been engaged by the City of Stonnington to provide transportation advice in relation to the development of a Structure Plan for the Glenferrie Road - High Street (GRHS) Activity Centre.

This advice was mainly provided within two reports issued to Council in August 2014. These reports were an *Issues and Opportunity Report* dated 12 August 2014 and a *Background Transport Report* also dated 14 August 2014. These two reports have now been combined into this *Background Transportation Report* so that it can be provided as background information to accompany the Draft Structure Plan during the consultation phase.

The first part of this transportation report provides an analysis of the access and movement issues within the Activity Centre and identifies potential opportunities to be investigated as part of the structure planning process.

The second part of this report provides base of information on the policy context of transport within the activity centre and scopes existing conditions to assist in the creation of the Structure Plan.

2. ACCESS AND MOVEMENT ISSUES

The following issues have been identified for Glenferrie Road-High Street Activity Centre that will require an appropriate response in the Structure Plan.

2.1 Focus on Vehicle Movement within the Centre

Both Glenferrie Road and High Street are designated by VicRoads as having pedestrian, cycling and public transport priority. However, historically focus has been on planning for vehicle movement and on-street car parking. High Street also is designated as a 'traffic priority' route with associated clearways which has limited the improvement opportunities for other modes. This has resulted in a situation where the walking environment along Glenferrie Road is generally of a higher quality than High Street.

Response: The Structure Plan should support Council's Sustainable Transport Policy priority list of modes.

2.2 Traffic and Parking Congestion around Schools

The existence of schools within the study area particularly St Josephs and De La Salle adjacent to the Civic Precinct and Lauriston Girls School further north from the study area provides a significant attractor for vehicular traffic during pick-up and drop off times, which correspond with the morning peak, and bring forward the evening peak. Whilst it contributes to the Glenferrie Road local economy spine, this heavy traffic hinders safe and efficient movement of vehicles, cyclists and pedestrians.

Response: Opportunities to encourage greater utilisation of sustainable transport for the schools within the study area should be explored, through providing a high quality walking environment, Council supported in-school programs and designated safe routes. Increases in sustainable transport usage by students will naturally decrease the number of vehicle movements. Similarly, improved cycling facilities along Glenferrie Road may offer a better economic contribution than car based journeys to and from school.

2.3 Inconsistent Pedestrian Environment

A number of recent initiatives have prioritised pedestrian movement along Glenferrie Road with the implementation of 'continuous path crossings' and kerb build outs. Some of these measures are also present along High Street, although surfaces and intersection treatments are generally not of the same standard. Furthermore, there are no formal pedestrian crossing opportunities within 500 metres of the intersection of Glenferrie Road and High Street, requiring some visitors to the Centre to walk considerable distances to controlled crossing locations.

Response: The pedestrian infrastructure implemented on both streets should continue to be of a high standard, with a consistent approach to both High Street and Glenferrie Road adopted. Additionally, surveys of pedestrian crossing movements should be considered to determine whether additional signalised pedestrian crossings are warranted within the Centre.

2.4 High Street Speed Limit Implications

The speed environment (and posted limit) of 60 km/h reduces pedestrian amenity and makes informal crossing more difficult. Additionally, a 60 km/h speed limit increases the risks to cyclists and discourages less confident riders from using this mode of transport to access the Centre.

Response: The Structure Plan should strongly support the current proposal to reduce the speed limit on High Street to 40 km/h, which will support pedestrian and cycling activity within the Centre.

2.5 Lack of Cycling Infrastructure

Both Glenferrie Road and High Street form part of VicRoads' Principal Bicycle Network (PBN), but do not incorporate any dedicated cycling lanes. It is likely that the lack of these facilities discourages potential bike riders from cycling to the Centre, and is inconsistent with Council's Cycling Strategy and VicRoads' designation of both roads as priority cycling routes. External funding for these VicRoads-responsible projects is required to implement cycle facilities on these roads, and VicRoads currently do not have a budget for improvements for cyclists.

Response: Council should continue to lobby VicRoads for cycling infrastructure. As suggested by Council's draft Cycling Strategy, Council could fund some studies into potential facilities on Glenferrie Road and High Street; so that these projects are able to be implemented within a short time frame should funding become available.

2.6 Delays to Trams

The Glenferrie Road/ High Street Activity Centre is well served by public transport, particularly in relation to fixed rail. There are three separate tram routes operating along High Street, Glenferrie Road and Wattletree Road. Whilst providing access to the city, these tram services also provide access to and from local and nearby centres, such as Hawthorn, Prahran and St Kilda. Trams from these adjoining areas suffer significant delays at peak times due to the traffic congestion on those routes, making travel by tram less attractive to users.

Response: The Structure Plan should advocate the use of tram priority measures at traffic signals to reduce delays to tram users. Furthermore, the benefits of an eastbound part-time tram lane on High Street, operating during the evening peak hours should be investigated.

New and improved infrastructure appropriate to the Centre should be explored to provide a safer and highly accessible public transport network.

2.7 Demand for Parking

Provision of convenient car parking is of key concern to visitors to the Centre. Whilst there is generally sufficient short-term parking for shoppers, at times parking opportunities become limited. Surveys also reveal that shoppers on High Street who travel to the speciality shops may be seeking longer term car parking than is currently available. At times when there are limited parking opportunities available, the number of drivers looking for car parks and circulating also increases, this leads to increased congestion on the local road network.

Regarding increasing parking space demand there appears to be no opportunity for the Council to significantly increase parking supply without the construction of costly decked or underground parking structures. There is also limited opportunity to provide for additional customer parking on development sites (due to the fine grained nature of most sites).

Response: Given future development and increased activity in the Centre, planning for transport to and from the Centre must focus on more space efficient and sustainable modes of travel, as outlined in Council's Draft Integrated Transport Plan. Cars and commercial vehicles will need to be acknowledged and accommodated as part of a balanced approach to movement in the area, but should not be the main focus.

The Structure Plan should also support a parking system which maximises the use of existing car parking facilities. This could include changes to existing parking

restrictions to maximise the availability of car parking for visitors to the Centre. These changes could include the conversion of all day car parking to shorter term parking, a greater offering of 'car share' alternatives and potentially demand management. For shorter local trips and for worker parking, changes to car parking may influence a shift towards more sustainable modes of transport.

Alternatively, opportunities exist to explore well integrated development of existing surface parking areas into decked or underground parking. It is noted however that there may be significant cost and community barriers to this.

3. OPPORTUNITIES

The following opportunities have been identified for Glenferrie Road-High Street Activity Centre that will require an appropriate response in the Structure Plan.

3.1 Enhanced Walkability

Irrespective of how visitors or residents to the Centre arrive, at some point they are all pedestrians. The provision of a good quality walking environment can add to the attractiveness of the Centre and encourage new visitors, whilst ensuring that existing users stay longer, whether for social interaction, shopping or eating. Whilst some public realm works have been undertaken already within the Study Area to improve the pedestrian realm, additional measures need to be explored to further enhance the walkability of the Centre.

Response: There are a number of measures which could further enhance walkability. There is little in the way of pedestrian way finding signage within the Centre or to connect to key nodes within the residential hinterland such as open space. If this were to be implemented it would assist pedestrians in finding their way to important destinations, and highlight walking times to train stations.

Measures such as ensuring developments fronting High Street and Glenferrie Road provide weather protection in the form of canopies would improve walkability during periods of inclement weather. The provision of more pause points through seats within the public realm for pedestrians to wait or rest also greatly enhances the quality of the pedestrian experience, and increases the desirability of walking as a transport mode.

3.2 Utility Trips by Bicycle

The relative density of existing neighbourhoods in the vicinity of the activity Centre and the close proximity of key attractions and services (particularly in relation to the services offered on Glenferrie Road) are ideally suited to travel by bicycle. Given the potential growth in the number of people living and working in the Centre and the surrounding areas, the demand for travel is likely to increase in the future. Enabling and encouraging access to the Centre by bicycle has a number of benefits, including less vehicular congestion and reduced demand for car parking.

One type of bicycle trip that is not currently catered for is utility trips. These trips are generally for specific purposes such as to visit shops and services, and are shorter (up to 2 km), often at slower speeds. The typical characteristics of cyclists undertaking these trips are that they prefer low-stress routes and cycle in 'everyday' clothing. By providing cycling infrastructure aimed at this user type, demand could be reduced for other modes of transport.

Response: In addition to addressing the on-road cycling infrastructure issues, measures to encourage cycling for utility trips would include alternative designated routes on quieter streets through the activity centre, review of existing cycle parking facilities, the provision of additional parking facilities in areas of high demand (e.g. cafés), improved secure cycle parking at rail stations, and a bicycle wayfinding strategy that guides bike riders to the Centre via low stress routes.

3.3 Armadale Station Precinct Improvements

The area between Armadale Station and High Street comprises an assortment of buildings and infrastructure, including a good quality pedestrian link through Kings Arcade. The road network in this precinct generally consists of one lane streets and parking with narrow or no footpath. Considering the demand for pedestrian access between the station and High Street and the comparatively low number of vehicle movements to the adjoining uses, it is considered that the design of this area has been heavily focussed on providing for vehicle access over pedestrian amenity.

Response: Ability exists to enhance pedestrian access between the Station and High Street through the provision of 'after hours' pedestrian access through Kings Arcade. It is also recommended that a shared space (i.e. areas where cars are slowed to 10 km/h and pedestrians are given the space of the whole road reserve to use) is established along the length of Morey Street and Kingsway. These improvements should include public realm and lighting improvements. The benefits of natural surveillance of this area should also be considered in relation to any adjoining development proposal. New crossovers to Kings Way should be avoided where possible, or well managed to ensure pedestrian prioritisation. All of the above is supported by the existing Council's Walking Policy.

3.4 Encouraging Sustainable Transport in New Developments

Residential development within the Centre is well served by a variety of services and has the benefit of excellent public transport to the City and surrounding areas. Therefore new development in this location is ideally suited to encourage travel by sustainable modes, and limit the impact on the Centre through less demand for car parking and vehicular travel.

Response: The Structure Plan should support development which encourages low car use due to the availability of alternative transport modes and the wide variety of goods and services available in the Centre (and accessible by walking). In enabling

low car use developments, supporting measures such as the provision of cycle parking above the Planning Scheme requirements should be encouraged, in addition to supporting the provision of car sharing schemes and appropriate car parking policy.

4. POLICY CONTEXT

4.1 Background

The Stonnington Planning Scheme designates the commercial centres of Glenferrie Road, Malvern and High Street, Armadale as separate Major Activity Centres. Within the Planning Scheme each of the centres has separate strategic directions, with Glenferrie Road, Malvern's role being to act as a local centre for goods and services, whilst High Street, Armadale's focus is speciality retailing with a regional focus.

The combined activity centre is also identified in the Metropolitan Planning Strategy (Plan Melbourne) as an 'existing activity centre'.

As part of the preparation of the Activity Centre Structure Plan, a primary and peripheral boundary was nominated. The Structure Plan boundaries are shown in **Figure 1**.

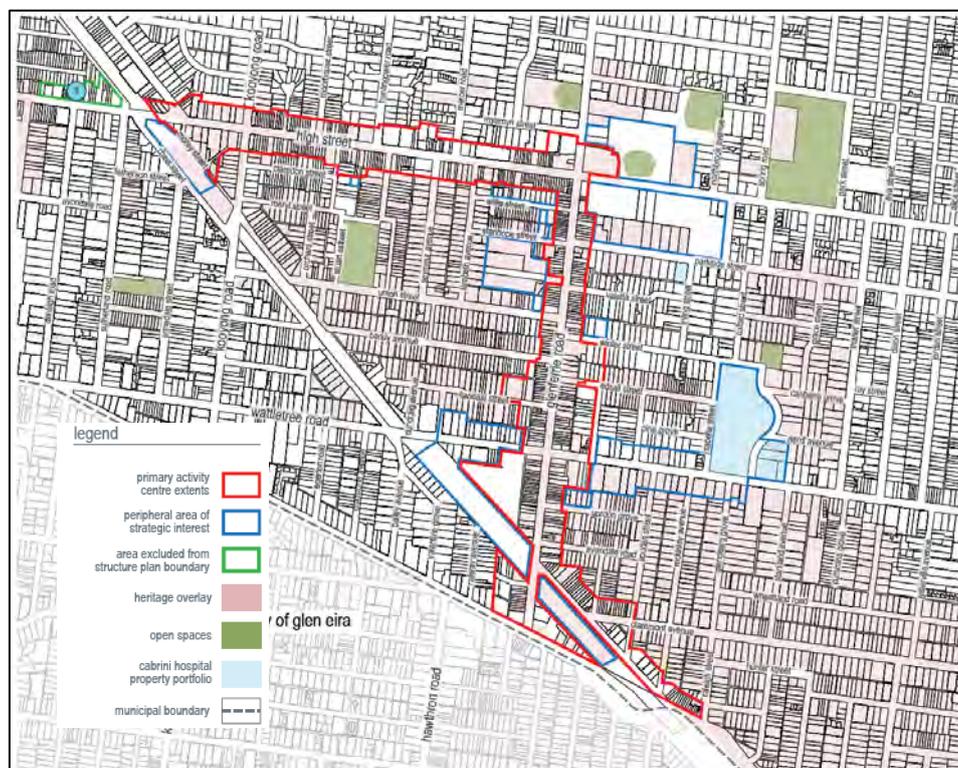


Figure 1: Structure Plan Boundaries

It is intended that the Structure Plan will set out a 10-20 year strategic framework for the future development of the centre. It will guide the current and future form and function of the centre in accordance with a shared vision. It will also provide a framework for the integration of Council's services and programs, existing policies and strategies and will consider the role of the centre in the context of the broader municipality and the region.

Transport is a key component of the structure planning process. Within Stonnington transport priorities are guided by the Sustainable Transport Policy, and supported by the emerging Integrated Transport Plan (ITP) which is a long-term plan integrating land use planning and transport.

4.2 Objectives of the Structure Planning Process

Structure planning is the process of creating a framework for the integrated development of an activity centre.

Key objectives of the structure planning process in relation to transport are outlined as follows:

- Set the framework for the use and development of the land in and around the centre to meet the future needs of the community and provide direction to investors about preferred locations for investments.
- Provide a mobility network and traffic and car parking management (including traffic generation, circulation and access) that encourages and supports sustainable transport mode choices.
- Identify opportunities to promote walkability and enhance pedestrian connectivity through public realm improvements and creative public/private design responses.
- Provide for well-designed and well located public spaces and public open space (including active and passive open space) that serves the needs of the community and visitors to the centre.
- Consider whether there are opportunities to provide for small workspaces/workshops and start-up businesses in laneways, at the rear of shops, in arcades, adjacent to car parking areas.
- Provide greater certainty for residents and developers about appropriate built form outcomes.

4.3 Transport Policy

The Sustainable Transport Policy (2008) guides Council's strategic, statutory, operational and service activities relating to the transport network. It aims to create an integrated, sustainable, safe, convenient, and accessible transport network that responds to the municipality's style and character, minimises impact on the environment and overall amenity, enhances liveability, promotes wellbeing, vitality and prosperity and benefits all users. It proposes this is achieved by a series of policy

principles, including; reducing car dependence; improving pedestrian, cycling and public transport connections and accessibility; and improving safety. In recognising that travel relates to the movement of people (and goods where appropriate), the Policy outlines a delivery priority that preferences sustainable transport modes as outlined in **Figure 2**.

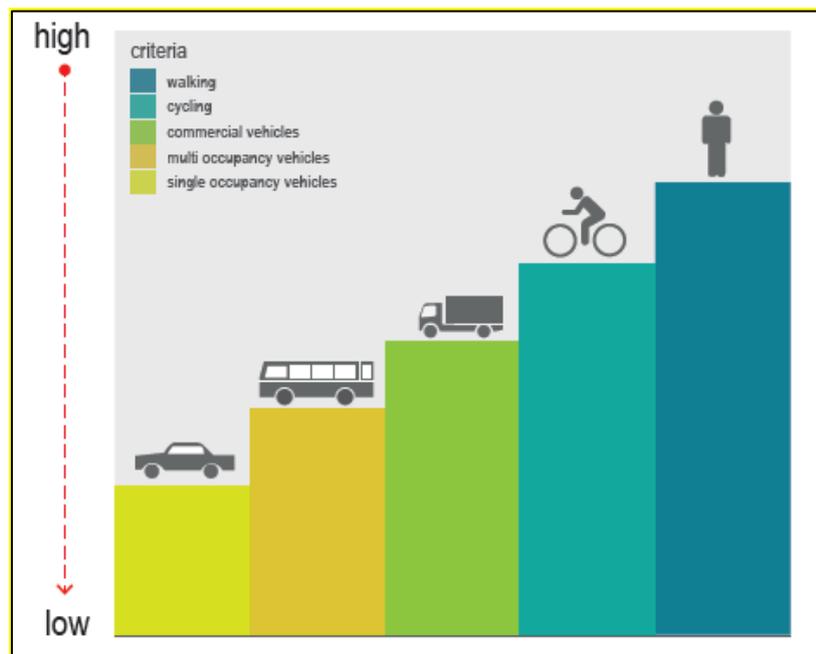


Figure 2: Transport Priority Modes

For reference it is noted that public transport is not included in this hierarchy. Whilst trams could be considered multiple occupancy vehicles, it is likely that public transport would sit above commercial vehicles in such a hierarchy.]

The Sustainable Transport Policy is supported by the Draft Stonnington Integrated Transport Plan (ITP). This is a long term plan with the aim of facilitating increased integration between transport and land use planning.

The objective of the strategy is integrate transport, land use planning and development to maximise accessibility, safety and sustainability of the transport network and the built environment. Central to its vision is the need for communities to be “well connected”, with facilities to encourage safe walking and cycling, the provision of good public transport and easy access to high quality local services. This policy includes a serves of strategies and actions to achieve this outcome.

The draft ITP identifies specific improvement projects to address deficiencies in transport. It recognises that travel relates to the movement of people (and goods where appropriate) and not to the movement of vehicles. Based on this approach, the draft ITP states that priority will be given to transport modes in the order outlined in **Figure 3**.

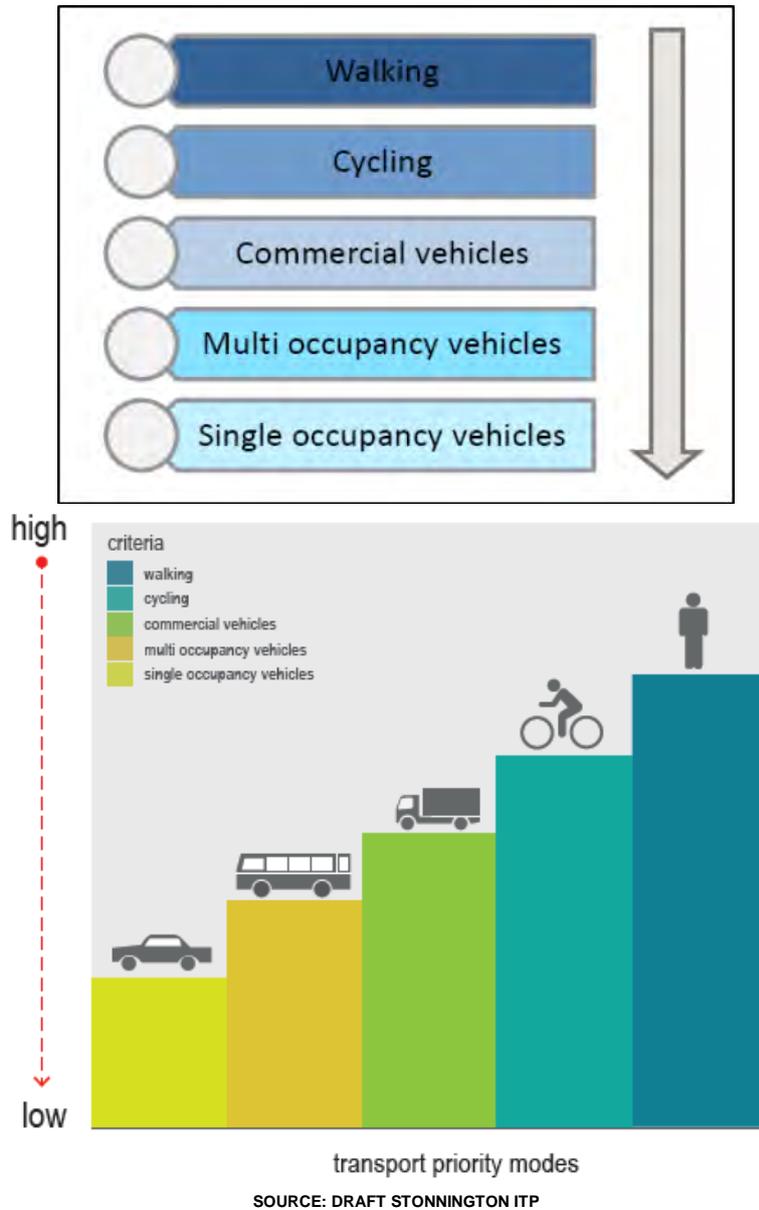


Figure 3: Transport Priority Modes

Note that the highest two priorities are walking and cycling and the lowest is single occupancy car travel.

5. TRANSPORT NETWORK

5.1 Arterial Road Network

Glenferrie Road and High Street are the primary roads within the activity centre, and along with Wattletree Road are all declared state arterial roads, under the control of VicRoads. For these roads, VicRoads has operational responsibility, including responsibility for improvements and maintenance of road infrastructure. Access to these roads (from side streets and developments) is also the responsibility of VicRoads whose agreement is required for any changes. Council is responsible for the footpath areas along these arterial roads and parking.

In recent years, VicRoads has developed SmartRoads. SmartRoads is an approach that manages road space by giving priority use of the road to different transport modes at particular times of the day. This aims to support land use and transport planning and to better consider the effects on the surrounding community, rather than simply speed and ease of movement for motorised traffic. These plans often aim to encourage alternative vehicle routes around activity centres and to prioritise local transport including walking, public transport and cycling.

Within the Activity Centre, under SmartRoads both High Street and Glenferrie Road are designated as pedestrian priority areas. Both roads are also designated as tram and bicycle priority routes. However, High Street is also designated as a traffic route (refer Figure 3).

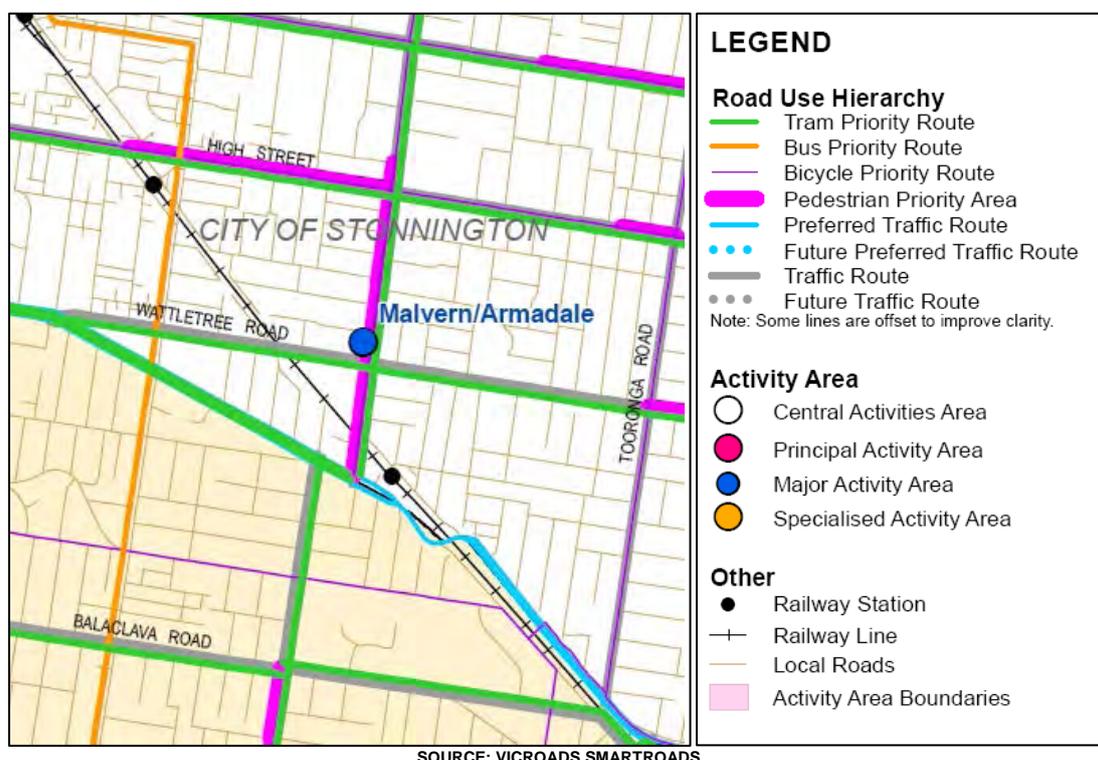


Figure 4: Smart Roads Hierarchy

5.2 Surrounding Streets

All non-arterial streets within the Structure Plan area are under the control of Council. An aim of Stonnington's ITP is to make Stonnington more sustainable by having more people in the municipality choosing to walk, cycle and to use public transport more often and driving their cars less.

Design of the public realm is important in structure plans, due to the importance of promoting pedestrian access within activity centres to increase walkability, access to public transport, the viability of activity centres, and in providing opportunities for social interaction.

The Structure Plan is to take into account opportunities to encourage increased walkability and connectivity and provide for sustainable transport options such as bicycle and car share schemes on the surrounding streets.

6. WALKING

6.1 Policy

The draft ITP ranks walking as its top priority mode and notes that pedestrian accessibility is an important element of the transport system for the many short trips, including trips to public transport stops. It notes that walkability of areas can be affected by a perceived or actual lack of connectivity, safety concerns, attractiveness of streets, public realm, and inadequate weather protection. Glenferrie Road and High Street are also designated pedestrian priority areas in VicRoads' SmartRoads system.

The draft ITP lists key attributes of the public realm which assist in increasing walkability and connectivity as the:

- *Width of streets (including areas free of obstructions)*
- *Quality of paving and way finding signage;*
- *Adequate sight lines, lighting and other safety measures built into design (i.e. passive surveillance etc);*
- *Adequate weather protection;*
- *Amenity (i.e. street trees and visual outlook); and*
- *Location and type of pedestrian crossing points*

Stonnington also has Walking Policy, adopted in April 2011. The vision of the policy is:

"To progressively make Stonnington's suburbs and retail precincts world-class walking areas, where it is safe and convenient to walk to destinations, where people are actively encouraged and enabled to walk, and where key community stakeholder groups and the community at large support Council's efforts to increase the amount of walking."

The policy target is to:

“increase the share of walking trips made by Stonnington residents in Stonnington by 50% by 2020.”

The policy proposes a three pronged approach to achieving this target, through programs and events, information (including wayfinding signage), and good quality walking environments.

In regard to the provision of good quality walking areas, the policy highlights; flat footpaths with low gradients, pram ramps and tactile pavers; a high level of amenity with seating, lighting and landscaping; and a good ‘on-road’ environment with ample crossings, low speed limits and traffic management devices. The Policy recommends the *Pedestrian Planning and Design Guide* (December 2007) produced by the New Zealand Transport Agency as the preferred manual for providing good practice pedestrian infrastructure.

6.2 Existing Conditions

The walking environment along Glenferrie Road and High Street is generally of a high quality. In line with the level of activity on both these streets, pedestrian volumes are higher on Glenferrie Road than on High Street.

Footpaths on both streets are generally of a high quality, and are generally flat and even, and well maintained (although the surfaces are generally newer and of better quality on Glenferrie Road). ‘Continuous path of travel’ crossings have been installed over side roads all along Glenferrie Road, and on most side road crossings of High Street. In addition, kerb buildouts have been constructed on Glenferrie Road to provide improved visibility for exiting vehicles, reduced crossing widths, and additional space for seating or footpath trading. Footpath trading boundary indicators are installed and generally provide sufficient space, although at some locations along Glenferrie Road (where cafes operate) there can be localised pedestrian congestion at times.

A timed 40km/h speed limit operates along Glenferrie Road throughout the shopping centre, and while the 50 km/h default limit applies to Council Roads in the area, a 60 km/h posted speed limit is in place on High Street.

There are a number of signalised pedestrian crossings of Glenferrie Road and High Street, with both pedestrian operated signals and signalised intersections, and all with yellow ‘walksafe’ treatments. Some parts of these key streets are served well, however, the spacing between crossings west along High Street, and south along Glenferrie Road from the intersection of these two roads is 500 metres in each direction (refer **Figure 5**).

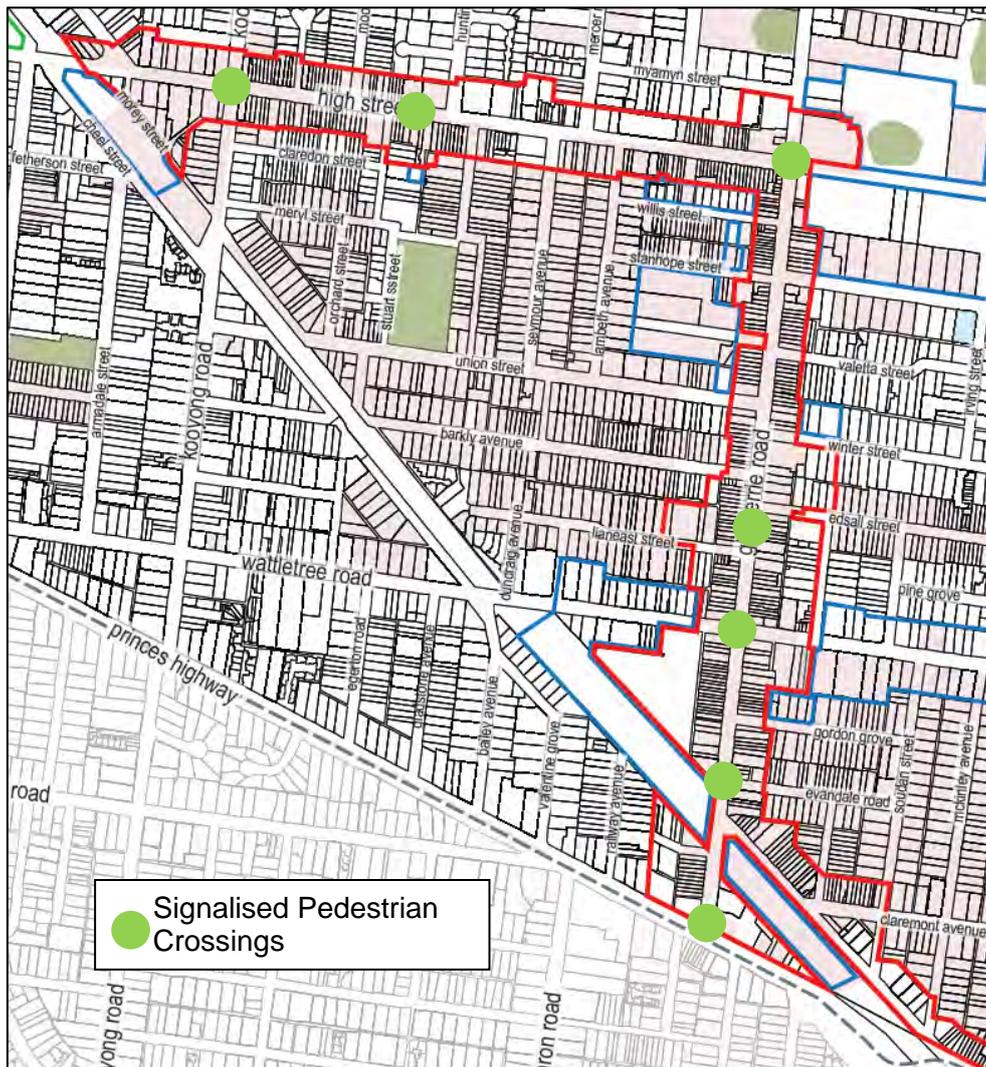


Figure 5: Signalled Pedestrian Crossings

On the side roads adjoining Glenferrie Road and High Street the footpath width and design is generally acceptable, with adequate provision of pram crossings. Street name signage is adequate, but there is little in the way of pedestrian wayfinding signage.

6.3 Current Proposals

VicRoads are currently investigating the implementation of a 24 hr 40 km/hr speed zone along High Street from St Kilda Road to Maitland Street, which would include all of High Street within the Centre. As part of this review they are also investigating the extension of the Glenferrie Road 40 km/h zone north from the intersection with High Street, and changes to its hours of operation.

7. CYCLING

7.1 Policy

The Sustainable Transport Policy ranks cycling as its second to top priority, and Council are currently in the process of updating their Cycling Strategy. The key aim of the draft Cycling Strategy is to:

“enhance the physical and social environments in order to support more people to cycle, and people to cycle more often.”

One of the core focuses of the strategy is ensuring good access to key local destinations, such as activity centres. The draft strategy includes a number of emerging strategic directions, these include:

- Continuing to develop and improve the cycling network and associated facilities to provide access to local places of congregation and prioritising the VicRoads designated Priority Bicycle Corridors;
- Supporting people at their destinations through enhancing the end-of-trip experience;
- Having safety as a paramount consideration, and to influence a culture of sharing of road space; and
- Reviewing all way-finding signage.

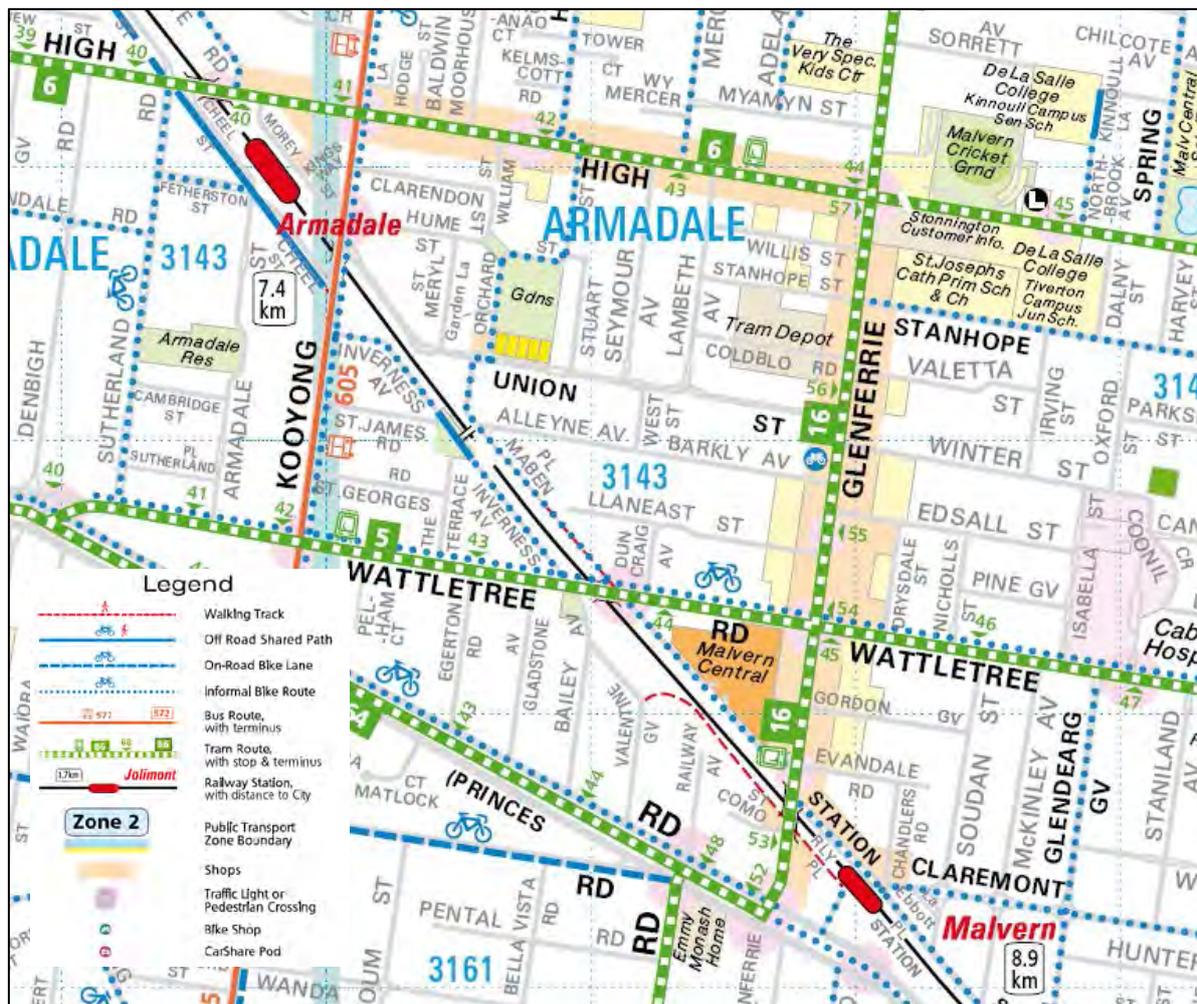
Glenferrie Road and High Street also form part of VicRoads Principal Bicycle Network (PBN), and are listed as bicycle priority routes in VicRoads SmartRoads system.

7.2 Existing Conditions

The medium density nature of residential development in the vicinity of the activity centre and the close proximity of key attractions and services support travel by bicycle. The layout of the street network in the surrounding area also provides for permeable travel north-south and east-west. However, the most direct roads are state arterials such as High Street and Glenferrie Road, which have heavy traffic at times, and limited space available for cycling.

As noted previously High Street and Glenferrie Road form part of VicRoads Principal Bicycle Network (PBN). This is a network of bicycle routes providing access to major destinations in the Melbourne metropolitan area. Whilst this designation highlights the importance of these roads for cycling, and raises the potential for inclusion of cycling facilities in the long-term, there are currently no bicycle facilities (for example cycle lanes), currently provided.

There is also an absence of alternative designated routes through the activity centre, although there are some signed informal bicycle routes on local streets with lower traffic volumes (refer **Figure 6**). However, whilst these are suitable for less confident cyclists travelling short distances (for example undertaking local utility trips to shops and schools), they are less suitable for long distance travel (such as cycle to work commuters).



SOURCE: TRAVELSMART

Figure 6: Bicycle Routes around the Activity Centre

There are a number of on-street bicycle parking hoops provided within the Activity Centre. Some of these are heavily utilised, which result in cyclists locking bicycles to less secure street furniture such as sign posts.

It is recommended that bicycle parking is provided at regular locations within the Activity Centre and reviewed at a regular basis (parking patterns can alter as businesses and community facilities change).

It is likely that the number of people living locally and cycling to rail stations as part of the commute may increase in the future. Currently, a small number of bicycle spaces are provided at Malvern and Armadale Rail Stations within the Activity Centre.

Wayfinding for cyclists is especially important for new cyclists as the most suitable routes may not coincide with the routes generally taken when walking or travelling by motor-vehicles. There are currently no bicycle wayfinding signs within the Activity Centre, apart from those indicating informal bicycle routes (these signs do not include destinations or distances).

7.3 Current Proposals

A number of bicycle infrastructure improvements have been identified in Council's emerging Bicycle Strategy. These include a review of bicycle wayfinding across the municipality, cycle lanes along High Street and Glenferrie Road within the Activity Centre, and cycle storage boxes on the approaches to the signalised intersections along these two arterial roads.

The proposed cycle lanes in High Street and Glenferrie Road include a shared parking and cycle lane in Glenferrie Road, and a peak hour cycle lane in High Street (similar to Sydney Road, Brunswick). However, external funding for these VicRoads-responsible projects is required for these VicRoads responsible roads, and VicRoads currently do not have a budget for improvements for cyclists.

8. PUBLIC TRANSPORT

8.1 Policy

The emerging Integrated Transport Plan lists the following objective in relation to public transport in Stonnington:

'To facilitate an increase in the use of public transport'

Within the Activity Centre, there are two rail stations, three tram routes and one bus route. These services are the responsibility of Public Transport Victoria (PTV), who are the statutory authority responsible for providing, coordinating and promoting public transport in Victoria. PTV contracts these services out to companies providing services such as Metro Trains, and Yarra Trams.

The biggest announcement by the Victorian Government in relation to services within the Activity Centre is the upgrade of the Dandenong Rail corridor (served by the two stations within the Centre) including new signalling and trains. These proposals are intended to improve passenger carrying capacity, and would impact positively on the ability of the existing services to cater for future population growth and mode shift within the Activity Centre. This upgrade requires additional investigation to understand the implication of freight route designation on passenger capacity.

In relation to trams, one legislative requirement will continue to impact on the design of stops and vehicles. This is the Disability Discrimination Act (DDA). The DDA requires that services are accessible to those with disabilities. For example, Yarra Trams in their Accessibility Action Plan outlines improving access to stops as an improvement priority. This has led to new accessible tram stops being implemented and new styles of tram stops being provided in some locations (for example the 'easy access' stop trial on Bridge Road, Richmond).

8.2 Existing Conditions

The Activity Centre is well served by public transport, particularly in relation to rail, with two stations being located in the Centre (Armadale, and Malvern). These stations are on the Frankston, Pakenham and Cranbourne lines, and within Zone 1. In addition there are three separate tram routes operating along High Street, Glenferrie Road and Wattletree Road. The route 605 bus also operates within the Centre along Kooyong Road. These services are shown in **Figure 7**.

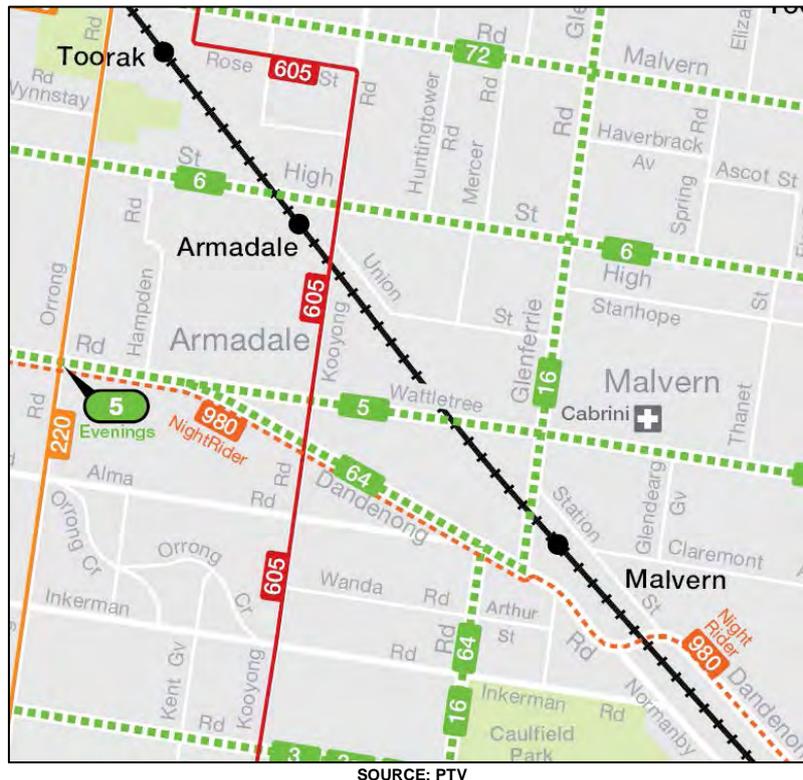


Figure 7: Public Transport around the Activity Centre

Malvern Rail Station is approximately 150 metres walk from Glenferrie Road. It is accessible from both sides of the rail corridor, and includes prominent wayfinding signage. The station includes step free access from street to platform and is staffed in the morning peak period.

Armadale Station is of identical design, and is also staffed. However, the pedestrian route from the Activity Centre (High Street) is less obvious (although well signed), and is along a generally poor quality pedestrian environment along Morey Street, or alternatively through the privately operated Kings Arcade (limited opening hours) through to the intersection of High Street and Kooyong Road.

The numbers of passengers using Malvern Station is higher than those using Armadale Station.

All tram stops within the Centre are kerb access stops where passengers wait on the footpath, crossing the road when the tram arrives, whilst all traffic stops. These stops are not accessible for passengers with mobility impairments. Other criticisms include longer dwell times and higher safety risks than other stop types. Busier stops in the area include shelters and seating.

Tram priority is included at signalised intersections within the Centre, and a westbound morning peak tram only lane operates along High Street for the Route 6 tram. This provides quicker journey times than the adjacent traffic lane.

A tram depot is also located on Glenferrie Road within the Centre accessed via an unsignalised intersection in the vicinity of Valletta Street.

8.3 Current Proposals

As noted above the State Government has announced capacity improvements for the rail corridor.

9. MOTOR VEHICLES

9.1 Policy

Private vehicles (particularly single occupant vehicles) are the lowest priority under the emerging Integrated Transport Strategy. However, the predominant mode of transport within Stonnington is currently the motor vehicle. Although areas with higher residential density, a wide range of land uses and public transport services such as the Activity Centre have a lower proportion of the population travelling by motor vehicle, particularly to places of employment.

High reliance on private motor vehicles results in numerous problems, including congestion, road safety problems, and a less pleasant environment for other users (such as pedestrians and cyclists). VicRoads SmartRoads system sets out a network of 'primary traffic routes' for through traffic away from activity centres. In the vicinity of the Activity Centre, these routes are the Princes Highway for east-west travel and Williams Road for north-south travel.

Nearly all reported injury crashes occurring on the road involve at least one motor-vehicle. Council's Road Safety Policy (2008-2017) aims to reduce the road related fatalities and serious injuries in the Municipality by 30% over the ten year life of the Policy. It is noted that achieving this goal would save one life and prevent 47 serious injuries.

The Road Safety Policy includes the concept of 'Vision Zero', which is the philosophy of road safety that states that eventually no one will be killed or seriously injured within the road transport system. Its premise is that it can never be ethically acceptable that people are killed or seriously injured when moving within the road transport system.

The Vision Zero approach includes the following strategies:

- Aligning vehicle speed to the inherent safety of the system;
- Improving vehicles; and
- Stimulating the community to use the system in a safer way.

As part of the Policy, Council committed to investigating and applying innovative and current approaches to road and roadside road safety treatments. One new approach highlighted was the 'shared spaces principles' of road design. Shared spaces is a philosophy of road design that aims to reduce crash rates, particularly for pedestrians and cyclists. This approach includes shared spaces used by vehicles, pedestrians and cyclists with a minimum of signs and traffic controls and where decision-making is returned to users. This type of road space is engineered to naturally encourage lower speeds.

9.2 Existing Conditions – Movement

The main corridors for movement within the Centre are the arterial roads of Glenferrie Road (12,300 vehicles per day – 2012), High Street (16,600 vehicles per day – 2012) and Wattletree Road (13,100 vehicles per day – 2012). All other roads adjoining the Centre are local roads with the exception of Stanhope Street and Union Street, which are all under the control of Council. At peak times, the arterial roads in the area can become congested, with slow vehicle speeds and delays at intersections.

High Street and Glenferrie Road are approximately 14 metres wide through most of the centre, and generally consist of one traffic lane in each direction and parking on both sides. In peak periods, clearways operate on High Street and parking and stopping is restricted. In the morning peak, the clearway operates on the southern (westbound side), and on the opposite side and direction in the evening peak. In the morning peak one of the two lanes operates as a tram only lane providing tram priority.

9.3 Existing Conditions – Parking

There is a mix of on-street, off-street public, and off-street private parking within the centre. Parking restrictions apply to the on-street parking with typically a 1P restriction on Glenferrie Road, 2P restriction on High Street, and 2P restriction on one side of the side roads. Resident permit parking is in place on streets adjoining Glenferrie Road and High Street where there is predominantly residential development.

The largest off-street public parking facilities are the multi-level car park in Drysdale Street (2P restriction, except for the upper level which is unrestricted), and the at-grade car park on Winter Street (2P restriction). The largest private car park is the multi-level Malvern Central car park (690 spaces) accessed from Glenferrie Road.



Car park utilisation surveys have not been carried out for the centre in recent years, but anecdotal evidence is that whilst the available parking is highly utilised (especially the premium spaces on Glenferrie Road), there are generally convenient car parking spaces available with a 2P restriction.

Long term (all-day) parking is generally available in private car parks, or on the periphery of the centre (for example Glenferrie Road, north of High Street and in some local streets).

9.4 Current Proposals

VicRoads are currently investigating a 40 km/h speed limit for High Street (refer **section 6.3** of this report).

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