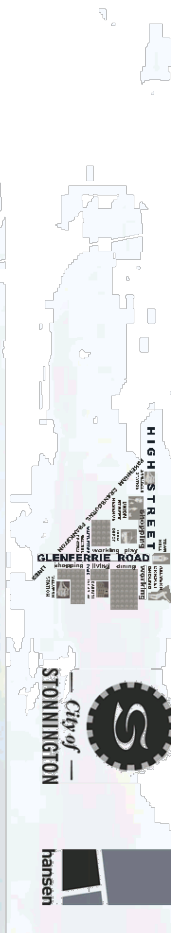


Item 13

Attachment 8 GRHS - Structure Plan - Attach 8 of 9

STRUCTURE PLAN

November 2015



2015 GLENFERRIE ROAD HIGH STREET ACTIVITY CENTRE STRUCTURE PLAN



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Council Project Team

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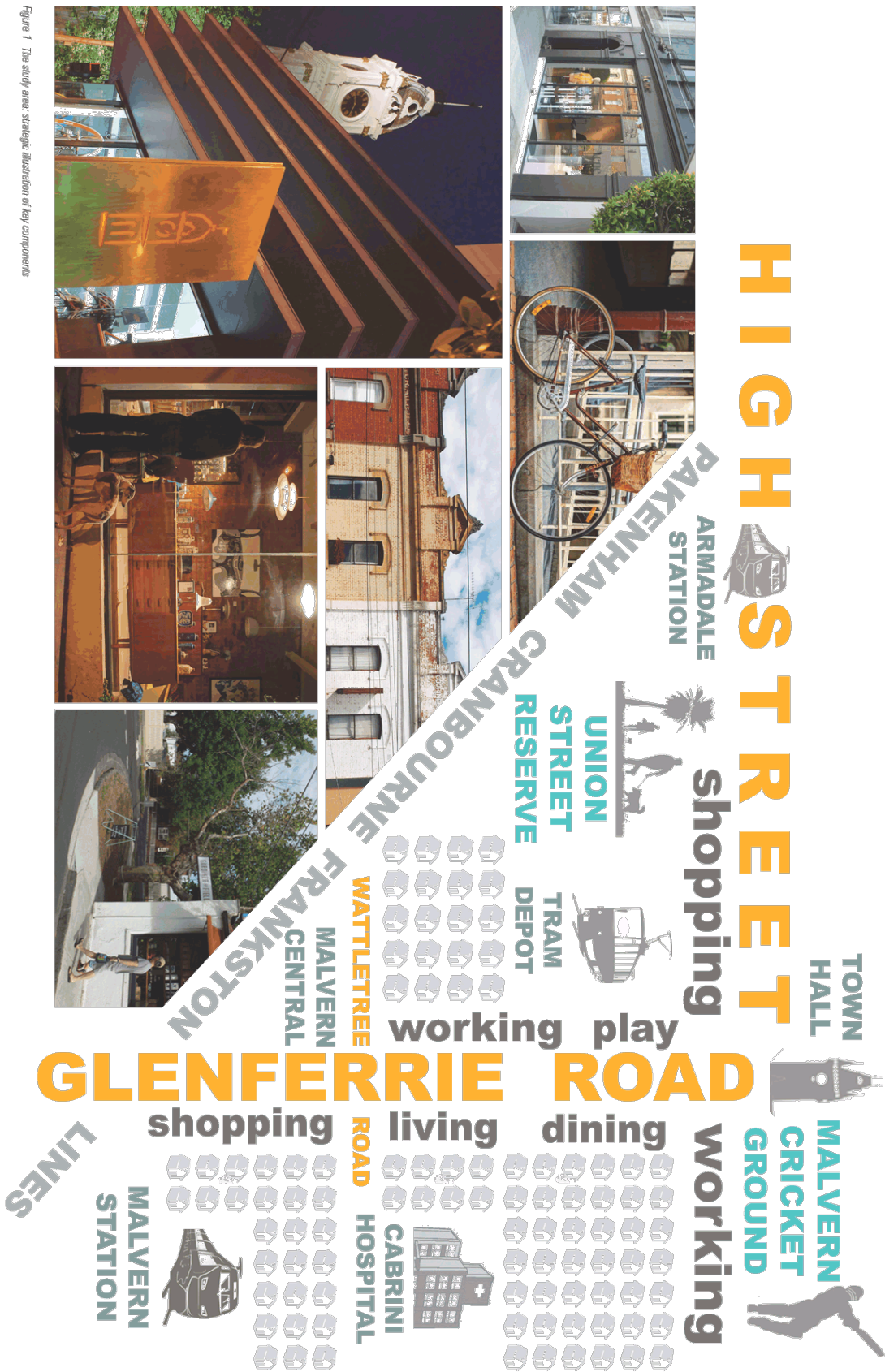
- Simon Beal: Project Director
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Stonnington City Council Officers

Invaluable contributions have also been provided by a range of Council officers within their areas of expertise.



1 Introduction

What is a Structure Plan?

A Structure Plan is a strategic planning tool which sets out a shared long-term vision for the future growth and change of a defined area. The Structure Plan aims to give effect to the policies and objectives set out in the State Planning Policy Framework to provide effectively for community needs. The Structure Plan will guide the major change to land use, built form and public spaces that together aim to achieve economic, social and environmental objectives for the place.

A coordinated plan allows a wide range of stakeholders, service authorities, State Government departments and internal Stonnington Council departments to all read from the same page to produce a coherent outcome and provide certainty for both residents and the Council.

A Structure Plan also provides the framework for statutory planning controls which are implemented through the Stonnington Planning Scheme. Structure Plans are informed by extensive public consultation and discussion. While everyone may not agree on every aspect of the plan, it should reflect broad community consensus.

How will the Structure Plan be used?

The aim of the Structure Plan for the Glenferrie Road / High Street Activity Centre is to guide future land use and development in a coordinated manner and provide greater certainty for all stakeholders to deliver a preferred future. A vision for the centre for a period of 10 - 25 years is established. Key policy directions, and important physical outcomes as they relate to the Glenferrie Road / High Street Activity Centre's built form and activity, landscape and environment and access and movement are identified. The Structure Plan will be used by:

Stonnington City Council:

- As a basis for introducing new planning policies, zones and overlay controls into its planning scheme (as appropriate);
- In assessing planning permit applications;
- In assessing requests to rezone land;
- In guiding non-statutory initiatives, arrangements or partnerships to assist in realising potential future opportunities within the centre;
- In preparing capital works budgets to implement public works; and
- In delivering community services.

The Community:

- To understand how the area, and specific precincts within the activity centre, are likely to change in the future;
- To assist community groups in prioritising future work and in seeking funding for projects; and
- To provide a framework for community groups to assist in making long term plans.

Existing Business Owners:

- To create greater certainty and appreciation regarding the future direction of the centre.

Developers:

- To understand the development opportunities that exist and the matters that will be taken into account in assessing development proposals.

Other government agencies:

- In coordinating infrastructure improvements with work undertaken by Stonnington City Council and other agencies including public transport, roads and water.

The Structure Plan must be regularly reviewed to ensure its directions and ambitions continue to be relevant to the centre as it changes over time.

Glenferrie Road/ High Street Activity Centre Structure Plan

This Structure Plan builds upon the earlier analysis and findings documented within the following documents:

- Glenferrie Road/ High Street Activity Centre Structure Plan Background Report: June 2014.
- Glenferrie Road/ High Street Activity Centre Structure Plan issues and opportunities paper: August, 2014.
- Economic Technical Report, Essential Economics, 2014.

This document does not seek to replicate the detailed content of above documents; rather they form necessary background information which is relevant to the detail contained within this Structure Plan.

The combined retail areas of Glenferrie Road and High Street form an important, and historical, retail precinct that is popular within the local catchment primarily for its convenience, as well as containing a range of retailing activity which has a wider regional catchment. As a combined centre the Glenferrie Road / High Street Activity Centre is an important hub in Melbourne's inner south east. It provides a range of retail and hospitality services and is well located within the Melbourne metropolitan context to capture future development, both residential and non-residential.

Stonnington City Council have engaged Hansen Partnership, a planning and design consultancy, to work with key stakeholders in preparing the Structure Plan for the Glenferrie Road / High Street Activity Centre. Hansen Partnership have been assisted with a supporting sub-consultant's team of O'Brien Traffic (transportation) and Essential Economics (economic analysis).

The Structure Plan constitutes a strategic document that sets out a long term planning and design vision for the evolution of the precinct and is intended to set a 10-25 year strategic framework to manage and guide the future development the centre.

Project Stages

This project is being undertaken in a four stage process. The first stage of the project involved the collation of range of background materials, whilst the second stage involved the preparation of an issues and opportunities paper including consultation with the local community. The current third stage involves the preparation the Structure Plan itself including further community consultation and, following on from this the final stage will involve the implementation of the Structure Plan.

2 Vision Statement

The Vision Statement which underpins the Glenferrie Road/ High Street Activity Centre Structure Plan is as follows:

In 2040 Glenferrie Road and High Street Activity Centre will be:

- *A thriving shopping centre, serving the day to day needs of the local community, coupled with a range of specialist retailers serving a wider regional area.*
- *A vibrant, inviting, attractive and clean centre with well-designed, 'green' streetscapes, pedestrian connections and community gathering nodes.*
- *A centre where new development respects and enhances the form and scale of heritage buildings, which retain prominence.*
- *A centre for localised employment and housing within the commercial spine whilst protecting the valued residential hinterland.*

Objectives

The primary objectives for the Structure Plan are:

- To protect and enhance the existing heritage and built form fabric.
- To provide clear urban and built form guidance for new infill development and additions and alterations of existing buildings.
- To strengthen the commercial and retail role of the centres.
- To improve pedestrian and cyclist mobility and infrastructure throughout the centre and minimise pedestrian and vehicular conflict zones.
- To improve the appearance of the public spaces within the centre, including primary streetscapes and secondary areas such as public car parks and laneways.
- Provide additional vegetated corridors and spaces in strategic locations to develop the urban forest and enhance the public realm.
- To provide for pause points at and around important junctions.
- To improve access and connection of Malvern Central with Glenferrie Road.
- To consolidate medical and health related facilities along Watternee Road.
- To consolidate and strengthen the community node around Malvern Library.
- To establish clearly defined redevelopment sites into the centre.
- To provide healthy and diverse vegetated streets and public spaces.
- To provide clear guidance on the inclusion of vegetation into new infill development and additions and alterations of existing buildings



The study area comprises a diverse range of urban environments and experiences from hostile Dandenong Road, to the intimate leafy residential side streets off High Street

3 Study Area

The Glenferrie Road / High Street Activity Centre, as identified by Plan Melbourne (2014), is located within the City of Stonnington and is located approximately 7kms east of the Melbourne CBD, in the south of the Stonnington municipality.

The Glenferrie Road / High Street Study Area comprises an area of approximately 1.1km² and is a generally triangular area of land bound by the rail corridor to the south-west and the buildings on the northern side of High Street to the north. In 2013, the Centre was estimated to provide approximately 70,000m² of retail floor space, making it larger than the centres of St Kilda and Camberwell, yet smaller than Chapel Street and Chadstone Shopping Centre.

Defined by its two intersecting main streets, Glenferrie Road and High Street Activity Centre is an important hub in Melbourne's inner south east. It provides a range of retail and hospitality services to the immediate suburbs as well as specialty retailing with a more regional focus. Within the Melbourne metropolitan context and its inner suburban location the Glenferrie Road / High Street Activity Centre is poised to be the focus of future development, both residential and non-residential. Designated peripheral areas are located adjacent to the primary Activity Centre Boundary which will also evolve over time (refer Figure 3).

The designated Activity Centre Boundary (refer Figure 3) has been established to be reflective of the existing land use patterns and urban form, in addition to the potential to facilitate the future evolution of more intensive land use and built form elements.

The Structure Plan focuses on the commercial zones of the centre, broadly located along the main strip shopping centre frontages of Glenferrie Road and High Street. In addition, the Activity Centre Boundary includes Council buildings, car parks, community uses, child care centres, schools etc., and also the Cabrini Hospital and the commercial and medical related spine located along Wattletree Road.

The Structure Plan is primarily characterised by the following attributes:

- The main street suburban strip shopping centre frontages located along Glenferrie Road and High Street. Both commercial spines Glenferrie Road and High Street have different functions and roles based on established commercial activity and road alignment (north, south and east/ west).
 - A community/ civic precinct centred around Malvern Town Hall located on the corner of Glenferrie Road/ High Streets and extending to the east to incorporate a number of schools and community based uses.
 - A public transport node around Armadale Station to the west of the study area, which centres on the Kingsway Arcade heritage precinct.
 - A public transport infrastructure node focused on the Malvern Tram Depot located behind Glenferrie Road to the west.
 - A car parking precinct located behind Glenferrie Road to the east (extending from Winter Street to Wattletree Road), including a decked parking structure.
 - A main commercial node focused on Malvern Central, located to the corner of Glenferrie Road and Wattletree Road.
 - A secondary 'commercial and medical related spine' located along Wattletree Road extending from the railway line to the west to Cabrini Hospital to the east.
 - A public transport node focused around Malvern Station to the south of the study area, which incorporates a small localised commercial precinct centred along Station Street and the intersection with Clarendon Avenue.
 - A service industrial/ commercial precinct focused on land between Dandenong Road and the railway alignment.
 - The railway cutting which bisects the local neighbourhoods extending from Armadale to Malvern Station.
- The Activity Centre Boundary has been determined by the extent of the commercial area, and explicitly excluded surrounding hinterland residential areas which have been recently designated for lower order change with the implementation of the New Residential Zones. This results in a relatively confined boundary which follows High Street, Glenferrie Road, the Town Hall, Armadale Station, Malvern Station and properties between Dandenong Road and the rail alignment. Peripheral areas located adjacent to the Activity Centre boundary includes significant facilities such as: the Civic Precinct, Cabrini Hospital / Wattletree Road, and Coddle Tram Depot.

Based on the above-mentioned characteristics and the overall size of the Glenferrie Road and High Street Activity Centre, the Structure Plan is further broken down into framework plan areas, which will allow more specific detail for each of the precincts to be provided within the context of the Structure Plan. Individual framework plan locations are illustrated at Figures 9-16 in the locations shown in Figure 2 including:

1. Civic Centre
2. Armadale Station
3. Malvern Station
4. Malvern Central
5. Coddle Tram Depot
6. Drysdale Street Carpark

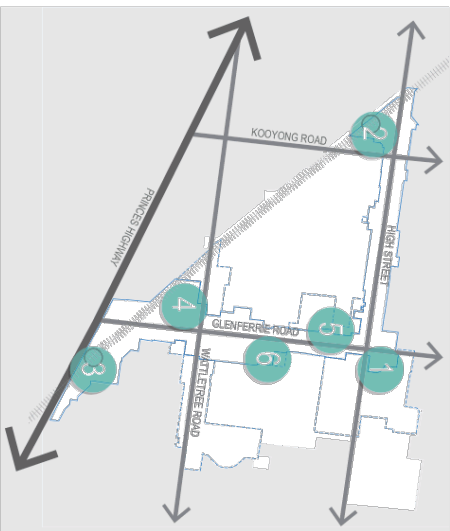


Figure 2 Precinct Framework Plan Locations



4 Background Analysis

In addition to the brief background analysis provided below, further detailed information is provided within the June 2014 Background Report.

History

- The Structure Plan area has a rich history, having originally been occupied by the Woi Woorung (Wurundjari) and Boon Wurrung (Bunurong) people, forming part of the larger language group of the Kulin Nation, with a geographic range extending across the majority of metropolitan Melbourne down to Gippsland in the east.
- Post-settlement, the land was initially used for grazing and a number of stock routes which connected to Dandenong, Gippsland and Western Port Bay. Further subdivision and development of the land, in the form of a hotel, dwellings and shops commenced in the 1850s.
- The construction of the Oakleigh Rail line in 1879 led to the development of Toorak, Hawthorn, Arncliffe and Malvern Stations. As is seen elsewhere in 19th century Melbourne, the suburban rail stations established the nodes from which growth radiated, and the clusters of commercial development at Malvern and Arncliffe Station particularly exemplify this.
- Other historical events of note within the Structure Plan area include:
 - The establishment of the Civic Precinct of Malvern Council in 1886.
 - The construction of the Coldbly Tram Depot, 1910, leading to the creation of an L shaped tram route along this portion Glenferrie Road and High Street, stimulating the development of a combined Glenferrie Road and High Street commercial spine.
 - The development of the majority of the residential hinterland by the 1930s; resulting in a primarily Victorian through Edwardian to Interwar residential building stock characterising these residential areas.
 - The redefining of the role of the High Street commercial spine to specialise in high end retail, including wedding attire, serving a wider, regional, catchment in the 1980s.

The People

The demographic and socio-economic profile of residents can be summarised as follows:

- Income levels: are significantly higher than the Greater Melbourne average, with median individual incomes +50% above and median household incomes +35% above the Greater Melbourne median.
- Age Profile: is similar to the Greater Melbourne profile, with a median age of 37 years compared to a 36 years median for Melbourne. However, there is a higher proportion 'young professional' residents compared to Melbourne. Approximately 27% of residents are aged between 20 and 34 years compared to the Greater Melbourne average 23%.

- Family Composition and Household Size: a higher proportion of families are couples without children, compared to Greater Melbourne (42% of 35%). There is a lower proportion of one parent families (11% of 15%). As such, the average household size of 2.3 persons per household is significantly lower than the average of 2.6 persons per household for Greater Melbourne.
- Cultural background: there is a relatively low degree of ethnic diversity compared to Greater Melbourne. 70% of residents were born in Australia, with approximately 80% speaking English only at home. These percentages compare to approximately 67% and 70% for Greater Melbourne respectively.
- Dwelling Type: Compared to Melbourne, the wider study area is characterised by a high number of apartments (39% of housing stock, cf 18% of housing stock) and semi-detached dwellings and terraces (18% of housing stock of 12%).
- Tenure: the share of dwellings rented (37%) is significantly higher compared to Greater Melbourne (28%), while the share of homes that are owned with a mortgage is significantly lower (26%) compared to Greater Melbourne (36%).
- Housing Costs: housing costs in terms of both average rents (\$350/week) and mortgage repayments (\$2,510/month) are significantly higher than the Greater Melbourne averages (\$310/week) and (\$1,840/month).
- Internet, Car Ownership: levels of car ownership are slightly lower compared to Greater Melbourne (69% of 91%), while a higher share of households in the MTA are connected to the internet (87% cf 82%).
- Employment: unemployment (4.3%) is lower than the Greater Melbourne average (5.5%), with a high labour force participation rate (69%) relative to Greater Melbourne (65%).
- Occupation: approximately 90% of the labour force are employed in white-collar occupations (Managers & Professionals, and Clerical & Sales workers), compared to Greater Melbourne (72%).

Population Growth

Analysis of ABS Census data shows recent changes in population and housing in the Glenferrie Road-High Street Activity Centre has occurred over the period 2006 to 2011. In 2011 the Centre contained an estimated 165 dwellings accommodating approximately 270 residents, representing an increase of +55 dwellings (+50%) and +65 residents (+46%) over the census period. Overall, the average household size in the Centre is 1.6 persons, which is in line with household sizes found in Activity Centres across Melbourne. In comparison, the broader study area has an average household size of 2.0 persons per household which is indicative of the larger detached dwellings and family homes in the centre's residential areas. Given the proximity of Glenferrie Road-High to central Melbourne and its desirable neighbourhood location, the centre can be expected to experience increasing pressure for additional residential development.

Metropolitan Context

A Structure Plan seeks to influence land use, transport networks and development and this is guided primarily by a State Planning Policy Framework and a Local Planning Policy Framework. The Local Planning Policy Framework must support the State Planning Policy Framework. The Glenferrie Road, High Street Activity Centre Structure Plan Background Report, June 2014 provides a detailed review of applicable policy of the Stonnington Planning Scheme.

The Strategic Framework Plan at Clause 21.03 of the Stonnington Planning Scheme nominates the Glenferrie Road and High Street centre as a Major Activity Centre. Within the Stonnington Activity Centre Hierarchy, a Major Activity Centres is situated below the Principal Activity Centres of Chapel Street and Chadstone.

The Stonnington Planning Scheme provides policies, objectives and strategies that apply to development within the Structure Plan Area, however these are not specific to the Glenferrie Road/ High Street Activity Centre. This Structure Plan will form the detailed policy and strategy basis relating to the Glenferrie Road/ High Street Activity Centre.

In addition to the above, the current State Government's Metropolitan Planning Strategy *Plan Melbourne* was released in May 2014, which sets a new planning vision for the future development of Metropolitan Melbourne. Whilst Melbourne 2030 previously classified the Glenferrie Road/ High Street Activity Centre as a Major Activity Centre (MAC), it is noted that in accordance with Plan Melbourne, it is now referred to as simply an Activity Centre which is equally applied to both Principal and Major Activity Centres.



Figure 4 Metropolitan Context

5 Community Views

Meaningful engagement with the community has been a key focus of this project. A series of events have been held to ensure that the community have had the opportunity to identify issues that currently affect the centre and to actively participate in the generation of ideas and concepts to guide the future direction for the centre.

Community consultation was undertaken in two stages, with Stage 1 occurring at the commencement of the Structure Plan process in March 2014 and Stage 2 in August to September 2014. The following is a summary of the main consultation events undertaken and the feedback received from the community.

Vision

The community raised the following comments in relation to the nominated Vision Statement for the Glenferrie Road / High Street Structure Plan:

- It is deemed beneficial to have a mixture of regional and local functions. There was a strong view that it would be beneficial for the centre to have a greater balance of uses to achieve its vibrancy and long term resilience by finding its niche and own unique offering.
- There was support for converting more areas to green spaces, retaining street trees and incorporating environmentally sustainable initiatives such as solar panels and green roofs. Amenity enhancements could include activating public spaces (car parks), improved lighting, public toilets, way-finding, landscaping and management of litter and graffiti.
- Heritage and its maintenance is deemed to be a very important element of the activity centre. There was a desire for a reasonable approach to be taken to development and protection that balances the amenity of heritage with functional needs.
- Support for mixture of residential and retail / commercial use while balancing the impact on existing residential amenity and not displacing commercial uses. The scale of development should be identified and strictly managed.

Significant sites

- A number of significant sites have been identified in the study area, where a summary of the main comments relating to the opportunities at these sites include:
 - Creation of a 'strategic redevelopment site' at the Dandenong Road entry to the centre which protects existing heritage buildings and provides setbacks to allow for greenery on Glenferrie Road.
 - An improved interface way-finding, amenity and vibrancy of Malvern Central at all access points, and improved links with the surrounding street network.
 - A potential increase in the size of Malvern Central, including the provision of additional car parking and introduction of entertainment uses.
 - The creation of a green corridor between the railway line and Malvern Central.
 - The redevelopment of the Malvern Train Station which protects existing heritage elements, and increases accessibility through the station.

- Creating a more vibrant and activated Armadale Train Station through a higher quality public realm, the introduction of shared zones and the possible building over the railway corridor.

- Support for the presence of a health precinct along Wartle Road associated with Cabrini hospital, provided it does not adversely impact on the adjacent residential and heritage areas. Car parking was identified as a key issue in this precinct.

- Redevelopment of the Tram Depot precinct which is sensitive to the existing heritage buildings.

- The activation of the Malvern Town Hall / civic precinct more as a civic space with increased way-finding pedestrian access to and through the site and improved landscaping.

- Potential reinterpretation of Malvern Square with increased landscaping and its use for markets and other activities, making the site more level so that it can be used as more of a congregation space, and increasing public seating.

- Retention and possible expansion of car parking areas.

Pedestrian, cycle, public transport friendly environment

- Strategies for creating a pedestrian, cycling and public transport friendly environment were sought from residents and traders, where the following suggestions were made: High Street and Glenferrie road act as community hubs where pedestrian access should be prioritised and the street spaces should serve as community spaces.

- The potential reducing of speed limits, limiting turning options and creating one-way areas in and around side streets.

- Streetscape improvements (e.g. widening footpaths, kerb 'outstands' with shelters and seating at side street junctions) to enhance pedestrian amenity and connections.

- A better link the Kooyong Road bus route and increase in public transport services in the centre.

- Improved bicycle routes through the centre, including safe alternatives off Glenferrie Road and High Street, as well as improved way-finding and bicycle parking facilities.

Open / public space

- There was a view that there should be an increase in the amount of open space and green spaces within the centre.

- A general view was expressed for the need for general streetscape improvements.

- The vacant King David School site * was noted as an opportunity for creating additional public open space (* site noted to be located outside of the study area).

Built form

- The encouragement of shop top development (residential and office), and development of infill sites with 'delicate' multi-storey buildings including the protection of heritage buildings and amenity from the street face (i.e. through the use of set-backs, no overlooking or overshadowing).

- Shop top development was generally supported at appropriate sites. It was recognised that the greatest opportunity for development in the activity centre is infill at individual sites – which should occur at a scale that reflects surrounding buildings.

- Better utilising second storeys of buildings and shop tops for residential / office spaces was suggested.

Land use

- A focus on the local catchment needs in terms of the retail sector.

- The consideration of alternative purposes for shops as a response to the changing retail sector.

- The need for development and land use to build a sense of community through street life and activity.

- Support to build night time activity in the centre, providing it is appropriately located and sensitive to surrounding residences.

- The need for additional public toilets throughout the activity centre.

A more detailed summary of the Community Consultation Stages 1 and 2 can be found at Appendix A.

Social Impact Assessment

A Social Impact Assessment (SIA) on the Structure Plan has been prepared examining the social impacts of the physical, demographic, environmental, visual and economic change that could result from the implementation of the Structure Plan. The SIA also considers the implications that this change will have on the access of local resident to services and facilities located in or near the Structure Plan area. Potential negative and positive social impacts have been identified as well as actions that could be taken to mitigate the negative impacts. Key strategies have been incorporated into the Structure Plan. The SIA is included in full at Appendix C: Social Impact Assessment.

6 Structure Plan Framework

As overarching strategic documents Structure Plans contain a range of strategies and objectives, some of which are more important to the broad aspirations of the Structure Plan, whilst others are more relevant to specific precincts and locations. During the background investigations and preparation of the Structure Plan a series of 'key directions' were identified. These 'key directions' represent the 'grand gestures' of the plan and should influence thinking on the range of smaller matters which will be needed to implement the preferred future for the Glenferrie Road / High Street Activity Centre. Furthermore there are more specific strategies and objectives relating to individually designated precincts which are addressed separately within the relevant neighbourhood framework plans.

Nominated key directions focus on five key areas including:

- Contribute to the local economy
- Facilitate place making
- Allow for mobility and safe access
- Celebrate heritage and embrace new development
- Green streets for everyone

Keeping the focus on these five key focal areas is intended to ensure that the key messages of the project are not lost within the detail. These key directions should be considered in all decision making within the centre.



Establish a clearly defined entry into the centre from the south

Key Directions

Contribute to the local economy

- Acknowledge High Street as a regional commercial and retail destination.
- Strengthen local and retail role of Glenferrie Road.
- Provide guidance to possible future expansion of Malvern Central.
- Consolidate medical and health facilities along Wattlegrave Road.
- Encourage entrepreneurship, creative industry and special focal points (i.e. market, events etc.) in the Centre.

Facilitate place making

- Strengthen and Establish clearly defined entries into the Centre.
- Establish diverse experiences along different streets within the Centre.
- Establish active and attractive street presentations along High Street.
- Establish clear urban form and design guidance throughout key strategic development areas.
- Consolidate and strengthen community node around Malvern Library.
- Establish clear urban form and design guidance throughout key strategic development areas.

Allow for mobility and safe access

- Establish clear streets hierarchy and users' priorities.
- Encourage and facilitate more sustainable modes of travel within and around the Centre.
- Establish clearly defined pedestrian linkages and manage road crossing points.

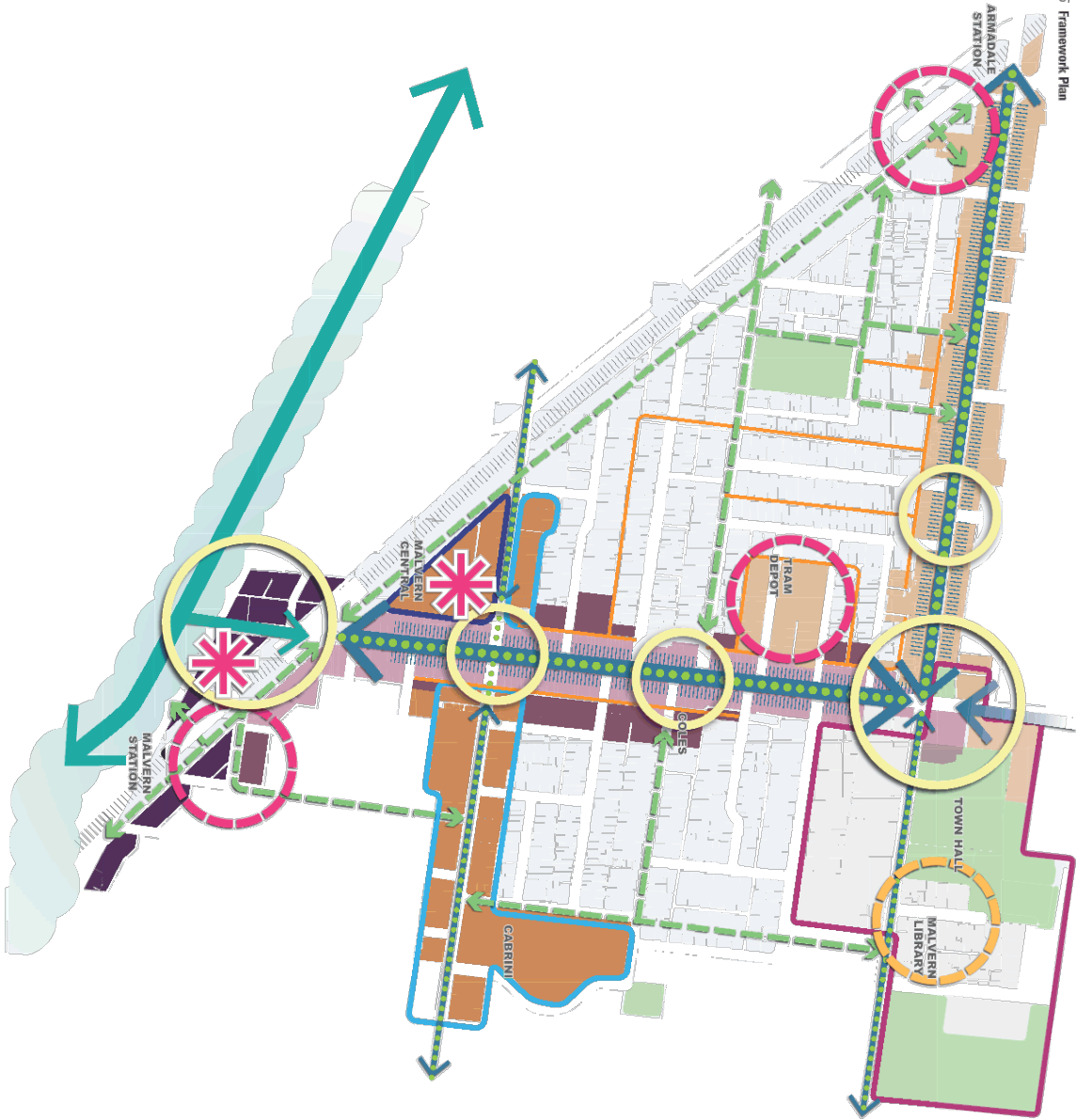
Celebrate heritage and embrace new development

- Protect and enhance significant heritage fabric along Glenferrie Road and High Street.
- Protect and enhance the laneway network within the Centre.
- Encourage development to provide a secondary address to rear lanes behind Glenferrie Road.
- Encourage development to provide a secondary address to the rear at Commercial interfaces.

Green streets for everyone

- Strengthen public meeting space at and around important junctions.
- Establish equitable access to all existing open spaces.
- Establish potential event spaces at key sites (market, special event etc).
- Establish additional vegetation in the public realm to contribute to the urban forest.

Figure 5 Framework Plan



Great Streets:

1. Contribute to Local Economy

- Recognize High Street as a regional commercial and retail destination
- Strengthen local and retail role of Glenferrie Road
- Provide guidance to possible future resequencing of Malvern Central
- Consolidate medical and health facilities along Watteles Road
- Encourage enterprising, creative industry & special local points in the Centre

2. Facilitate Place Making

- Clearly define strategic redevelopment sites into the Centre
- Establish diverse experiences along different streets within the Centre
- Establish active and attractive street presentations along Glenferrie Road and High Street
- Establish clear urban form and design guidance throughout key strategic development areas
- Consolidate and strengthen community nodes around Malvern Library

3. Allow for Mobility & Safe Access

- Establish clear streets hierarchy and user priorities
- Encourage and facilitate more sustainable modes of travel within and around the Centre
- Establish clearly defined pedestrian linkages and manage road crossing points

4. Celebrate its Heritage & Embrace the Future

- Protect and enhance significant heritage fabric along Glenferrie Road and High Street
- Protect and enhance the historic Malvern within the Centre
- Encourage development to provide a secondary address to rear lanes behind Glenferrie Road
- Encourage development to provide a secondary address to the rear at commercial interfaces.

5. Green & For Everyone

- Consolidate and strengthen the community node around civic spaces
- Strengthen public meeting space and vegetational and screen important junctions
- Establish equitable access to all existing open spaces
- Establish potential new access at key sites (market, school, sport, etc.)
- Establish additional vegetation in the public realm to contribute to the urban forest.

Land Use

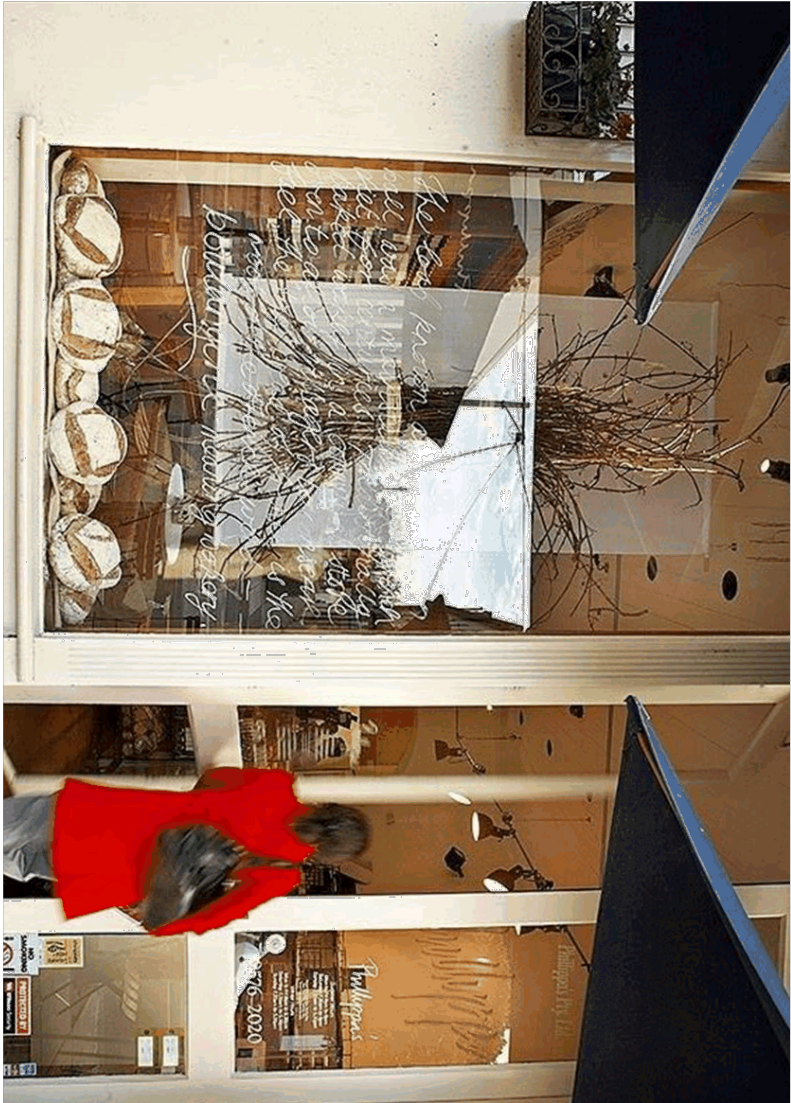
This Structure Plan does not seek to implement significant and wholesale changes to the current land use patterns within the Centre. Instead, it seeks to enhance and improve the existing distribution of land uses, attract supporting services and retain the competitive position of the Centre.

As identified in the background economic analysis the High Street and Glenferrie Road Activity Centre “manages to retain a sense of vibrancy and activity throughout virtually all of the centre, despite its relative size and extensive length” and has an “extremely diverse range of traders that is comparable to only a small number of locations in Melbourne”.

While there are particular uses which would be welcomed by the local community (such as a cinema, bars, a bowling alley and music venues) the planning system has limited ability to guide the establishment of specific businesses. What it can do is continue to support the existing situation while making minor changes to the planning controls to ensure that, as development pressures increase, land uses reflect the best outcome for the Centre.

Given the projected continued successful functional commercial operation of the Centre, the Structure Plan focuses on, and seeks to encourage the expansion, consolidation and augmentation of key places, including Malvern Central, the medical and health facilities along Wattletree Road and the community node around Malvern Library. Aspirations for new localised hubs are encouraged through the Structure Plan (such as creative industry etc) and particularly including Malvern and Armadale Train Stations.

Also by moderately increasing the amount of people living within the Structure Plan area, as well as increasing populations in the surrounding residential areas, will be of benefit to the Activity Centre by increasing the hours of activity and offer further custom for local businesses. Given the constraints of the Heritage Overlay and Neighbourhood Residential Zone, which apply to much of the residential hinterland, it is anticipated that the majority of the increases in residential population are likely to be accommodated by suitably integrated ‘above-shop’ infill development, however noting that the such increased above street level activity may accommodate both residential and office uses.



An increased residential population within the Activity Centre will support existing and new businesses

Economic Activity

Glenferrie Road-High Street Today

- Glenferrie Road-High Street serves a wider role than is typical for similar-sized centres and provides a diverse range of fresh food, café/restaurant, apparel and companion shopping, retail and professional services.
- A total 617 shopfront tenancies were identified in Glenferrie-High Street Activity Centre, comprising 489 retail tenancies, 76 other commercial tenancies and 52 vacancies.
- The 52 vacant shopfront tenancies account for 8.4% of total shopfront tenancies and is higher than the typical range of vacancies for a well-performing street-based centre of between 4% and 8%. A higher share of vacant tenancies are present in the High Street component of the centre.

Glenferrie Road-High Street Main Trade Area

- Glenferrie Road-High Street serves a main trade area (MTA) which reflects the geographic region within which residents are likely to regularly use the centre for day-to-day retail and other needs. The boundaries of the MTA are based on factors such as strength of retail offering in Glenferrie Road-High Street, the location and performance of competing centres, access via main road routes and via public transport, and existing travel to work patterns.
- The MTA is located entirely to the south of the Monash Freeway and is generally bounded by the East Malvern Train Station, Waverly Oval and Darling Road to the east, Dandenong Road and Balclutha Road to the south, and Orrong Road, Chomley Street and Toorak Village to the west.
- The MTA has a socio-economic profile which is significantly more affluent than the metropolitan Melbourne average. This aspect is reflected in the higher levels of retail spending by MTA residents compared with the metropolitan area. The population of the MTA served by the centre is estimated at 62,190 persons in 2014 and is expected to increase by approximately 6,000 persons, to 68,190 persons in 2031.

Floorspace Assessment

- It is forecast that the Glenferrie Road-High Street centre could accommodate additional retail floorspace of up to 11,500m² by 2031. Retail floorspace growth could be accommodated through initiatives such as expansion of the existing Malvern Central Shopping Centre, supermarket expansion on Glenferrie Road, and/or the redevelopment and intensification of the existing built form in the centre, particularly along High Street and around Malvern Station/Dandenong Road and the Armadale Station.
- An aspirational goal to encourage the development of up to 20,000m² of commercial floorspace, focused around the Dandenong Road entry. Such an outcome is likely to be achieved over the longer-term in view of current economic conditions which do not support strong levels of office development.

- It is expected that Glenferrie Road-High Street, as an activity centre, will accommodate in the order of 75%-85% of the projected dwelling demand for Armadale and Malvern South over the period to 2031.
- On this basis it is considered prudent to plan for dwelling growth in the activity centre in the order of +800 to +900 dwellings by 2031, at an average rate in the order of +50 dwellings pa.

Relevant Key Directions

- Acknowledge High Street as a regional commercial and retail destination.
- Strengthen local and retail role of Glenferrie Road.
- Consolidate medical and health facilities along Wattletree Road.
- Encourage entrepreneurship, creative industry and special focal points (i.e. market, events etc.) in the Centre.

Objectives

Retail

- Build on the history of High Street and Glenferrie Road as a unique Melbourne shopping destination that does things differently and better than other shopping centres.
- Provide an integrated approach to the planning and development of Glenferrie Road-High Street, which reflects the various precincts that reflect how customers and visitors use different parts of the centre.
- Ensure that growth and change in the centre occurs in a manner that doesn't negatively impact on the amenity of the centre to visitors and the local community.
- Increase market share in the face of continuing retail competition and generate new uses for existing and new retail floorspace. Retaining the popularity of Glenferrie Road-High Street with shoppers is essential in the face of increased competitive pressure from other centres.
- Accommodate additional retail floorspace through the expansion of the existing Malvern Central Shopping Centre, development of a future supermarket expansion on Glenferrie Road, and/or the redevelopment and intensification of retail activity in the balance of the centre.
- Find new niches in the retail sector and attract supporting activities, provide attractive amenities, and develop an enhanced sense of 'place' for the centre.
- Promote greater awareness of the diversity which exists in High Street-Glenferrie Road in order to generate higher levels of multi-purpose shopping trips.
- Better integration of key anchors the balance of the activity centre. For example, through better pedestrian linkages with Malvern Central, increased development of medical and para-medical uses adjacent to Cabrini Hospital, and higher levels of activity near the intersection of High Street and Glenferrie Road.



Ensure that growth and change doesn't negatively impact the amenity of the centre for residents and visitors

Office

- Ensure that the quality of office stock in the centre remains strong, particularly in the northern and southern parts of the centre where most of the dedicated office space exists.

- Seek more intensive office uses in Glenferrie Road-High Street through small-to-medium sized boutique developments that reflect the well-established built-form within the centre.

Residential

- Evidence across Melbourne shows that demand for higher-density residential development is extending outwards from the inner-city over time. In coming years, Glenferrie Road-High Street is expected to experience increasing pressure to accommodate higher-density residential development. This requires an appropriate response in the Structure Plan to ensure that market demand for additional housing is met without undermining the amenity of the centre for existing users.

Strategies

Important initiatives for future economic development, employment and investment in the Glenferrie Road-High Street activity centre include:

- Council to develop a strategy for the intersection of High Street and Glenferrie Road to become the symbolic 'heart' of the centre
- Support more-intensive retail activity by encouraging the redevelopment of the existing sites and tenancies in the centre
- Support comparison shopping in Glenferrie Road-High Street, with a particular emphasis on niche, independent and other non-chain stores (as appropriate, and not undermining the important role of major chain stores)
- Continue to work on improving the streetscape on Glenferrie Road-High Street to ensure that the centre continues to be seen as 'best practice' in Melbourne as a suburban street-based shopping and commercial precinct
- Allow flexibility for the provision of additional commercial floorspace should demand/interest eventuate.
- Work with property owners to unlock the potential for higher density-housing and other above-shop commercial development. This includes ensuring that the height limits applying in the centre are clearly understood by property owners and developers.
- Emphasise to developers the opportunity to incorporate residential and office components into mixed-use development schemes during pre-application discussions.

- Review, and if necessary upgrade, cycling access and infrastructure to reflect the growing popularity of this form of transport.
- Accommodate higher residential densities, particularly in the Malvern Station Precinct to the south of the rail line.
- Recognise the concentration of community and civic functions on the north-eastern fringe of the centre (without precluding retail and commercial development opportunities)
- Implement a set of performance benchmarks for marketing and promotional activities.

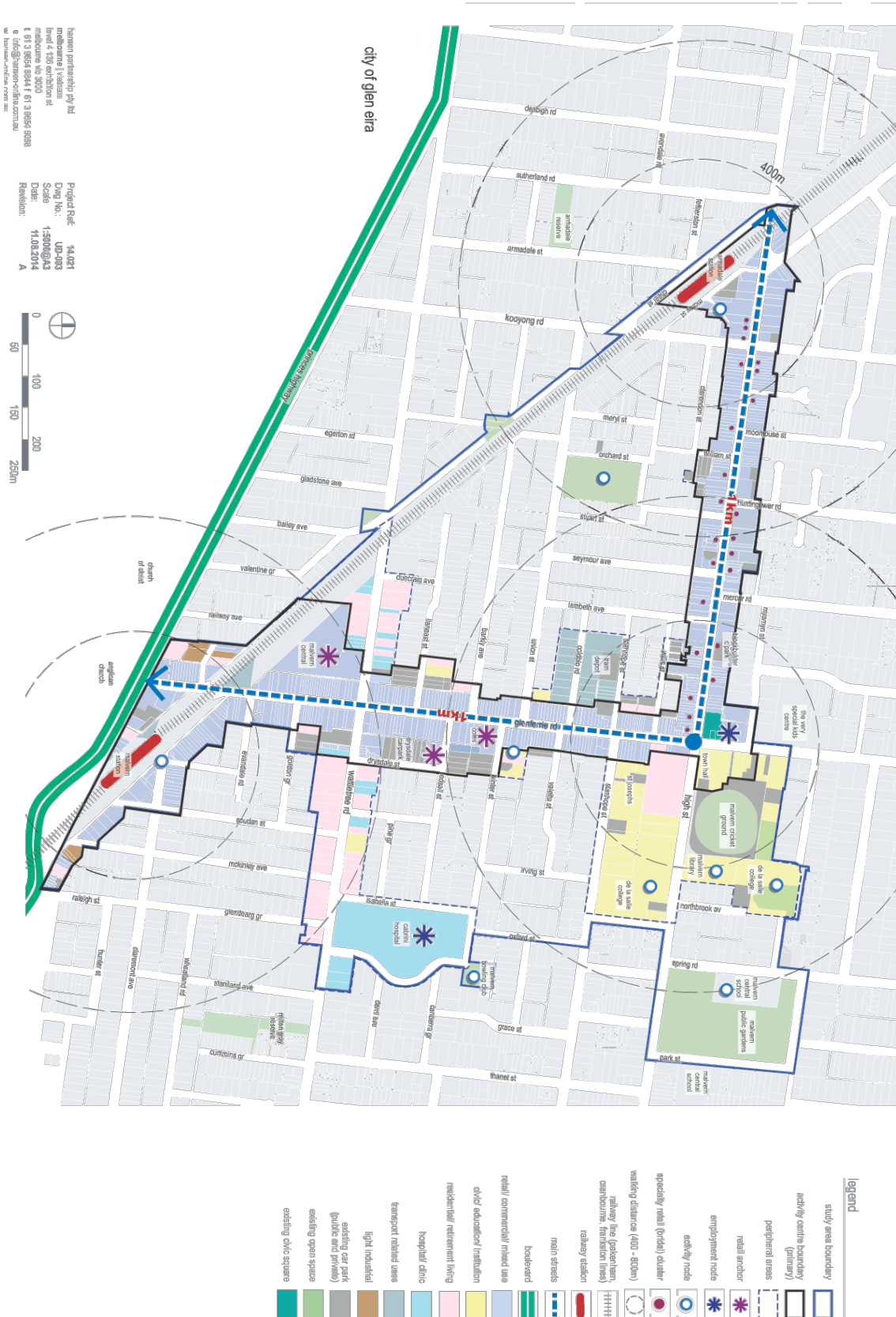
Additional detail on economic analysis and related recommendations is contained within the Economic Technical Report, which can be found at Appendix B.



Figure 6 Accommodate higher residential densities, particularly in the Malvern Station Precinct to the south of the rail line

Figure 7 Land Use and Economic Activity

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Access and Movement

The Glenferrie Road / High Street Activity Centre is anticipated to change over time, with an increase in the number of visitors and residents. This partly reflects the renewed desire of many people to live in close proximity to the range of retail, eating, services, and public transport. People living in the areas surrounding the Centre will also continue to be attracted to the evolving retail offering.

The changes envisaged are not a major readjustment to the nature of the area, however, they are likely to impact on how transport is used. Currently, travel by car to and from the Centre is the predominant mode choice, with congestion occurring at times. In the consultation process, provision of convenient car parking was one of the key issues raised. Whilst there is generally sufficient short-term parking for shoppers, at times parking opportunities become limited. Whilst many options for providing additional parking were put forward and explored as part of this process it was concluded that there is limited opportunity for the Council to significantly increase parking supply without the construction of costly decked or underground parking structures. There is also limited opportunity to provide for additional customer parking on most development sites (as they are not large enough to allow for convenient parking). Clearly, a new approach is necessary.

Council's Sustainable Transport Policy and supporting documents recognise that travel relates to the movements of people (and goods where appropriate) and not the movement of vehicles, thereby it prioritises transport modes in the following order:

- Walking
- Cycling
- Public Transport
- Commercial vehicles serving local businesses and institutions
- Multiple-occupancy vehicles
- Single-occupancy vehicles

This Structure Plan recognises that the continued dominance of the motor vehicle in the Centre would lead to a decline in its attractiveness as a place to live, shop and do business. By increasing the proportion of people using sustainable modes of travel the Centre will be more able to cater to any growth in the number of residents and visitors.

The Centre is well served by public transport with two rail stations (both on the Cranbourne, Frankston and Pakenham lines), and three tram routes providing easy access to a wide range of destinations. There are some good quality pedestrian facilities and urban realm treatments, and quiet local streets. However, a significant proportion of those living within close proximity still choose to drive. By enhancing existing pedestrian and cycling networks including the provision of Green Routes' around the Centre, it is these visitors and residents that the Structure Plan aims to attract away from the car.

Through these actions both road and parking capacity will also be freed up to allow those travelling from further afield, and those unable to use alternative modes to continue to drive to the Centre.

Among the outcomes pursued, a recasting of the area surrounding Armadale Station, investigate a 'shared space', where pedestrians, vehicles and cyclists can safely share the same road surface. These improvements will enhance the existing urban realm associated with Kingsway. Additional improvements for those walking in and through the area are also proposed, with measures to bring the walking environment on High Street more in line to that offered by Glenferrie Road. These are further supported by a network of Green Routes' linking routes along quiet streets to the centre where improved facilities are proposed.

The relative density of existing neighbourhoods in the vicinity of the Centre and the proximity of key attractions and services are ideally suited to travel by bicycle. One type of bicycle trip that is not currently catered for is utility trips. These trips are generally for specific purposes such as to visit shops and services, and are shorter (up to 2 km), often at slower speeds than longer commuter cycling trips. By providing cycling infrastructure aimed at this user type, demand could be reduced for other modes of transport. Therefore, cycling related improvements are proposed along the Green Routes' in addition to improved end of trip facilities (cycle parking) at key destinations.

A number of changes to the VicRoads controlled roads of High Street and Glenferrie Road are proposed, including a new signalised pedestrian crossing across Glenferrie Road in the northern end of the centre, to address the lack of pedestrian crossing opportunities in the area. Additionally, the establishment of bicycle lanes on Glenferrie Road is considered important to encourage users to cycle to the centre.

The Structure Plan will guide development within the Centre, and the transport aspects of development are no different with measures proposed to encourage low car use developments and to ensure that developments do not adversely impact their environs. An example of this is the strategy of preference of vehicle access by laneway (where appropriate) to ensure that main street frontages remain intact and that vehicle accesses are not scattered along the major pedestrian thoroughfares.

Relevant Key Directions

- Establish clear streets hierarchy and users' priorities
- Encourage and facilitate more sustainable modes of travel within and around the Centre
- Establish clearly defined pedestrian linkages and manage road crossing points
- Protect and enhance the laneway network within the Centre
- Encourage development to provide a secondary address to rear lanes behind Glenferrie Road.
- Encourage development to provide a secondary address to the rear at Commercial interfaces.



Despite the excellent level of public transport, cars will continue to have a significant ongoing role



Access to existing transport infrastructure increases the opportunity for sustainable transit uptake

Objectives

- To improve pedestrian amenity thereby encouraging visitors to walk to and around the centre.
- To provide improved connections to and through the centre and end of trip facilities for cyclists to encourage travel by bicycle.
- To maximise the use of existing car parking facilities given the limited opportunities to provide additional car parking.
- To improve pedestrian crossing opportunities across the arterial roads in the centre.
- To address areas of pedestrian/cyclist/tram/vehicular conflict.
- To reduce the dominance of motor vehicles in predominantly pedestrian areas (e.g. Kingsway, Armadale).
- To integrate and enhance public transport infrastructure appropriate to the Centre.
- To guide the design of new developments to support low car use.

Strategies

A range of transport related strategies are proposed, which individually addresses various modes of transport, including: walking, cycling, public transport, private vehicles and new development, each of which are addressed in turn below.

Walking

- Investigate the provision of a pedestrian crossing facility at a suitable location on Glenferrie Road between High Street and the signalised crossing south of Edsall Street to improve pedestrian crossing opportunities within this strip.
- Enhance the High Street walking environment through improved footpath surfacing and side road intersection upgrades (e.g. as per Glenferrie Road).
- Provide kerb buildouts to assist pedestrians (by reducing the width of road to be crossed) where formal pedestrian crossings facilities are not warranted.
- Provide pause points including the provision of seating at key locations.
- Enhance existing pedestrian and cyclist connections parallel to the rail line, including investigating a new link along the northern side of the railway reserve adjacent Malvern Central.
- Encourage safety and public realm improvements at the intersection of Glenferrie Road and Cobble Road through discussions with tram operators.
- Introduce enhancements to the walking environment along designated "green routes" through ensuring high quality walking surfaces, provision of pedestrian refuges to assist in the crossing of roads, additional seating for pedestrians (particularly older pedestrians), and adequate lighting for security.

- Improve pedestrian wayfinding signage to key destinations within the Centre, including showing walking times on signs.
- Support programmes that encourage students of schools within the Study Area to walk to and from schools, such as those that support "safe routes".

Cycling

- Liaise with VicRoads to advocate for the establishment of bicycle lanes on Glenferrie Road and High Street (part-time only) given that this route is identified as a part of VicRoads Principal Bicycle Network.
- Encourage VicRoads for the provision of bicycle priority road marking at the signalised intersections within the centre to protect cyclists.
- Review the bicycle wayfinding strategy that guides cyclists around the Centre via safe and convenient routes.
- Support programmes that encourage school students within the area to participate in cycling, such as the Cycle with Confidence programme and Ride 2 School programme.

- Encourage Metro Trains to install secure long-stay bicycle parking facilities at Armadale and Malvern rail stations to encourage mixed mode trips.
- Provide additional bicycle parking facilities in areas of high demand, including at community facilities, parks, and in the vicinity of cafes that integrate into surrounds.
- Improve connections between Union Street Reserve and High Street through cyclist bypasses (Orchard Street and William Street), and contra-flow cycling lanes along one-way streets (Clarendon Street).

- Enhance the cycling environment along the designated "green routes" through additional cycle parking, converting the existing pedestrian operated traffic signals on Kooyong Road at the intersection of Morey Street to a pedestrian and cyclist (Toucan) crossing, and converting off-road pedestrian paths to paths (where appropriate).

Public transport

- In conjunction with Tram Operators and VicRoads investigate additional tram priority measures at signalised intersections and pedestrian operated signals.
- Investigate the benefits of an eastbound evening peak hour tram lane on High Street to coincide with the existing clearway hours of operation.
- Encourage PTV to provide pedestrian wayfinding signage to Armadale Station from High Street (via Morey Street).



Lobby for the establishment of bicycle lanes on the main road network



Provide for designated bicycle facilities near to public transport modes

Access and Movement (continued)

Private vehicles

- Investigate the implementation of a 40 km/h speed limit through all streets within the Centre.
- Support a system of providing parking which maximises the use of existing car parking facilities, including making changes to parking restrictions to maximise the availability of car parking for visitors to the Centre (where justified).
- Investigate implementing a 'shared space' where all road users utilise the same area to reduce the dominance of motor vehicles in the predominantly pedestrian area of Morey Street and Kingsway in the vicinity of Armadale Station.
- Investigate implementing a shared space treatment along Station Street and Clarendon Avenue;
- Consider altering vehicle priorities at the intersection of Station Street and Soudan Street to discourage through traffic of Station Street (should the shared space proposal on Station Street be pursued).
- Rationalise the vehicle movements at the intersection of Drysdale Street, Edsall Street, and the Winter Street car park through an intersection redesign with the aim of reducing user confusion and improving safety (potentially by relocating the access into and out of the car park).
- Redesign the intersection between Llanest Street car park and Wattleree Road to address road safety issues (potentially by converting the Llanest car park access to entry only and providing a footpath).
- Seek to slow vehicles using the laneway parallel to Glenferrie Road adjacent to the Drysdale Street car park through the introduction of traffic calming.
- Ensure that rear lane widths are sufficiently wide to accommodate building servicing and car park access, so as to preserve the primary active frontage to the main street (High Street, Glenferrie Road and Wattleree Road).
- Where a laneway width is insufficient for efficient vehicle movements require a minimum 1.5 metre setback.



The implementation of a 40km/h limit through the centre would increase safety for pedestrians and cyclists



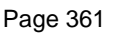
Avoid crossovers within Kings Way, to maintain a pleasant, pedestrian focussed environment

New developments

- Encourage developments which support low car use through generous provision of cycle parking (above Planning Scheme Requirements) and those that facilitate car share schemes.
- Require developments to utilise laneways for vehicle access to retain active street frontages along main streets.
- Seek to upgrade the walking experience along laneways with a pedestrian function through the provision of low level lighting and through opportunities associated with new developments (increasing laneway widths).
- Encourage the design of developments to incorporate passive surveillance of rear laneways and pedestrian paths.
- Ensure that development of sites adjoining the intersection of laneways and streets address pedestrian visibility issues.
- Ensure developments fronting High Street and Glenferrie Road provide weather protection in the form of canopies to improve walkability during periods of inclement weather.
- Explore opportunities for new pedestrian connections between Malvern Central and Malvern Station via Glenferrie Road.
- Ensure that future developments at the Cobble Tram Depot consider the access to and from the site by all modes.
- Avoid new crossovers to Kingsway, or manage these to ensure pedestrian prioritisation.

Additional detail on traffic and integrated transport and related recommendations are contained within the Background Transportation Report, which can be found at Appendix D.

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Public Realm and Landscape

The urban structure of the study area provides an opportunity for street based connections between the main streets, including access to open space assets. However, these routes are often convoluted, and lack the legibility to function as important pedestrian links. There is an excellent opportunity to enhance these existing links through improvements to residential streets which connect between the Glenferrie Road, High Street and Wattletree Road spines. These works should be underpinned by a focus on green infrastructure, comprising new soft landscaping, Water Sensitive Urban Design (WSUD) treatments, public realm fixtures and high quality way-finding. It will be important that the footpath connections and street crossings provide for Disability Discrimination Act (DDA) compliant movement to ensure equitable access. Given new opportunities for green space are limited, the ability to better utilise and connect to existing open space such as Malvern Cricket Ground and Union Street Park will be imperative to the management of a high quality environment for a growing localised population of workers and residents.

The diversity of experience between Glenferrie Road and High Street is a positive attribute which should continue to be built upon, both in built form and public realm terms. Whilst Glenferrie Road has a high quality pedestrian experience, with slower vehicle movements, the hostility of vehicle movements along Wattletree Road and High Street however will warrant some further consideration of footpath layering devices such as planters or other public realm fixtures to improve the sense of refuge for pedestrians.

The Town Hall and Library are significant anchors within the centre, but lack connectivity to one another, with poorly considered 'in between' spaces around the edges of Malvern Cricket Ground. An excellent opportunity exists to rework existing accessways, parking areas and residual garden space to provide for informal recreation opportunities and enhance the role of Malvern Cricket Ground as a significant open space asset. This treatment should also seek to connect through to and complement Malvern Square and new Council Offices across Glenferrie Road to the west.

Existing kerb build-outs occur throughout the centre and in combination with priority surface treatments at crossings achieve a pleasant pedestrian environment. This strategy should be extended along High Street and Glenferrie Road and combined with new public realm fixtures integrated in an overall 'kit of parts' to create a unique sense of identity for the centre. These stopping spaces are an important part of the sequence of movement along the main streets, and provides a valuable opportunity for canopy tree planting.

A number of significant spaces are noted within the study area which represent an excellent potential opportunity for temporary or more permanent event spaces. Such temporary uses are a proven method of resolving conflicts with vehicular requirements, whilst also allowing the 'chaining' of additional space for community use. More specific opportunities exist to leverage off the distinct spaces at Clarendon Avenue / Station Street (Malvern Station), Kingsway/ Arcade (Armadae Station) and Coddle Tram Depot, where street based festivals, community arts and cultural events or markets could be accommodated.

Permanent public realm improvements within these precincts should have regard to the potential function of these event spaces, and allow for a suitable flexible condition, through the use of kerb less streets, and limited or movable physical barriers between vehicle and pedestrian movement.

Relevant Key Directions

- Strengthen and Establish clearly defined strategic redevelopment sites into the Centre.
- Establish diverse experiences along different streets within the Centre
- Establish clearly defined pedestrian linkages and manage road crossing points
- Strengthen public meeting space at and around important junctions
- Establish equitable access to all existing open spaces
- Establish potential event spaces at key sites (market, special event etc.)
- Establish additional vegetation in the public realm to contribute to the urban forest.

Objectives

- Provide for a strong way-finding and branding strategy for the centre, to better unify disparate precincts within the broader study area.
- To provide public realm treatments at key entries into the study area which respond to both the pedestrian and vehicle scale of movement.
- Strengthen the legibility of existing street based connections between the main streets, public open spaces and local streets within the study area.
- Incorporate green infrastructure initiatives as an integral component of any new public realm works through management of stormwater runoff, canopy planting and soft landscaping works.
- Encourage the potential use of special places within the Activity Centre for temporary public use, including Coddle Tram Depot, Clarendon Avenue and Kingsway to increase public appreciation of existing 'gems' within the study area.
- Provide opportunity for residents and visitors to linger and meet without having to spend money within the public realm of Glenferrie Road, High Street and Wattletree Road.
- Improve the public realm experience within the Civic Precinct, and legibility of connections between Malvern Square, the Town Hall, Library, Police Station and Malvern Cricket Ground.
- Encourage greater use of Malvern Cricket Ground and its edges for informal public recreation use.
- Ensure that intensive redevelopment of the alignments fronting Dandenong Road to contribute to public realm enhancements along the rail alignment and adjacent to Malvern Station.

- Improve the legibility of the Malvern Station, Clarendon Avenue and Station Street connections to Glenferrie Road.
- Enhance the existing qualities of Kingsway/ Arcade and Armadae Station public realm, whilst increasing the emphasis on priority pedestrian movement.

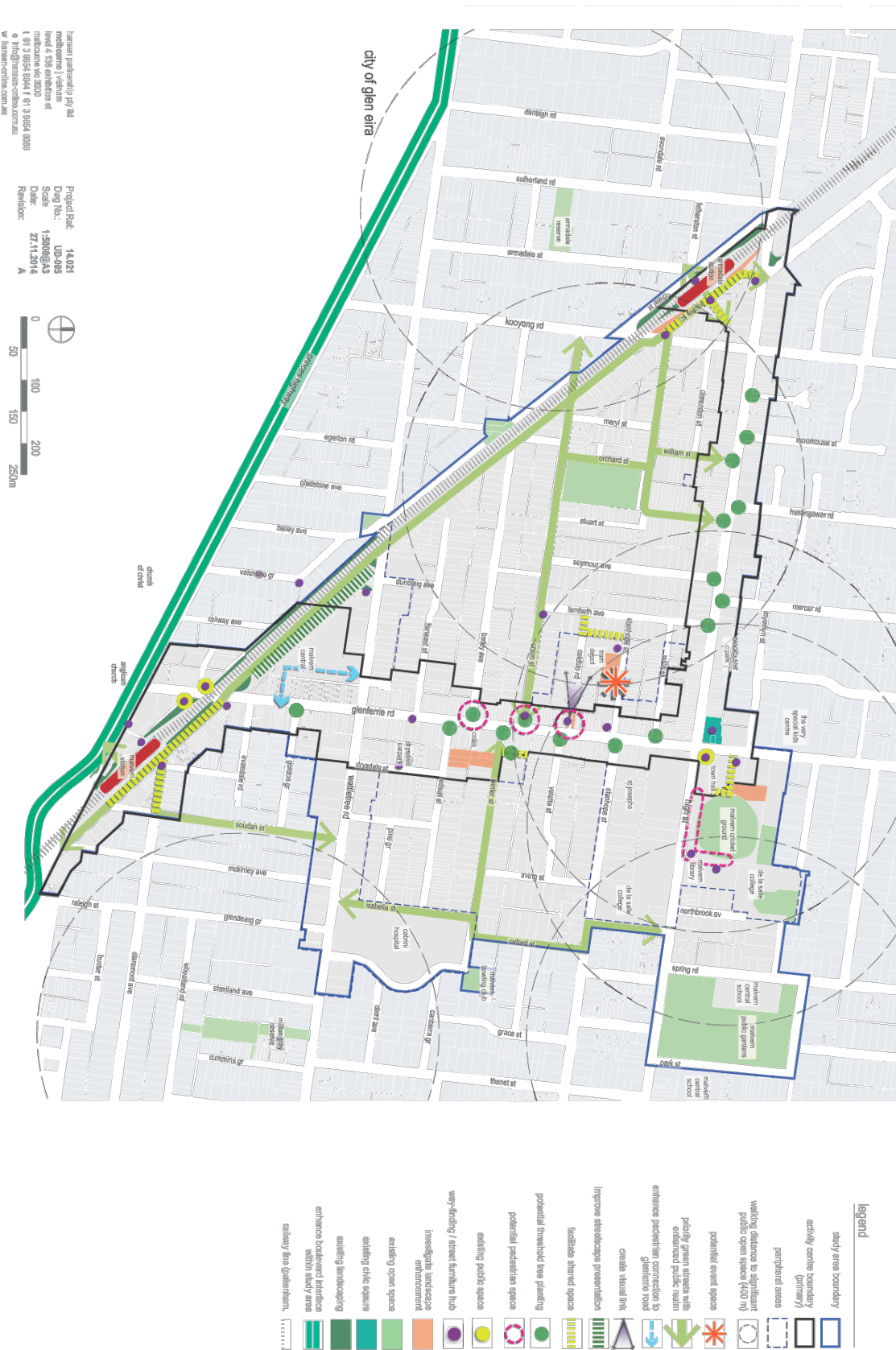
- Encourage the potential provision of a pedestrian connection between Malvern Central and Glenferrie Road, so as to increase connectivity between Malvern Station and Malvern Central.
- Reconfigure back of house surface parking areas with increased emphasis on public realm treatment, and the creation of new safe and high quality pedestrian connections.
- Encourage the retention of arcades between Glenferrie Road and back of house parking with greater emphasis on the creation of safe, well lit and pleasant pedestrian environments with all-hours access.
- Encourage the retention of existing significant trees and street trees which contribute an important amenity to the public realm, whilst forming an important part of Stonnington's urban forest.

Strategies

- Develop a strong wayfinding and branding strategy for the centre, comprising a 'kit of parts' for public realm fixtures with a high level of design quality reflective of its time and place. This should be distinctive for the study area rather than one which applies throughout Stonnington.
- Prepare a landscape masterplan for the Civic Precinct encompassing new shared and pedestrian links, passive open space and informal gathering spaces.
- Prepare a landscape masterplan for the Dymallye Street car park to incorporate new public realm treatments and enhanced north-south pedestrian connections through the space. Ensure works complement future development opportunities and ambitions of adjacent key tenants.
- Provide for increased function for existing and future kerb build outs comprising seating, planting and other public realm fixtures to enhance their role as pause points, and to create subtle differences between each location.
- Develop a typical street section treatment for 'green streets' which connect the main roads through the residential hinterland with integral green infrastructure, way finding and public realm fixtures.
- Facilitate or engage with local community groups, within Council and private operators to explore opportunities for the establishment of temporary or more regular event spaces within Clarendon Avenue, Station Street, Kingsway and Coddle Tram Depot.
- Engage with the owners of Malvern Central to explore opportunities for a new pedestrian connection to Glenferrie Road.

Figure 9 Public Realm and Landscape

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Built Form and Heritage

Given the relatively modest level of growth anticipated both in residential and commercial development within the Study Area, the area as a whole is not anticipated to change substantially from its current image when compared to the substantial growth anticipated for other Activity Centres such as Chapel Street Activity Centre. Accordingly, the proposed built form and heritage guidance seeks to reinforce the existing qualities and difference between sub precincts within the Study Area, whilst ensuring that new insertions or precincts of greater change are carefully managed carefully within the overall framework.

Based on current growth trajectories, it is estimated that 80% of the houses that exist today will still be standing in 50 years (Trina Day, 2012). Further to this, additional housing growth within the Neighbourhood Residential Zone areas and Heritage Overlay precincts proximate to the Activity Centre Boundary will be minimal, emphasising the need for growth to be contained within the Activity Centre Boundary. This suggests that the existing, positive abrupt transition between commercial and low rise residential building stock in the hinterland will be maintained and emphasised in some instances. In this sense, rather than promoting a typical wedding cake or terracing of building format, that it may be acceptable to have a more sheer profile of 3-4 storeys, with a single setback above a single storey form. Through a uniform requirement for a 4.5m laneway offset, privacy mitigation can be more readily dealt with by measures other than screening, whilst rear facades at night take on equivalent significance through exposure to the street frontage and improved building composition.

From yield and typology testing, the built form scale proposed by the Structure Plan has been devised to comfortably enable the housing growth projections anticipated by the Economic Technical Report. This is to be realised through a combination of selective infill, as well as the concentration of higher order growth to large lots within the Dandenong Road Precinct, as well as along High Street and Wattletree Road. The general scale of 4 storeys with some potential for a 5th level in designated areas on larger than typical allotments is sufficient to encourage investment whilst not representing a significant departure from the existing scale of built form. Conversely, the heights proposed for Dandenong Road and Malvern Central represent a concerted ambition to transform and strengthen the image of these precincts, based on their ability to accommodate more robust built form without negative impacts on surrounds.

The method of built form and heritage management is broken into a series of elements, comprising generic streetscape conditions and framework plan areas. The generic streetscape condition principle assumes that there are a series of common allotment types within High Street, Glenferrie Road and Wattletree Road, within which a range of small, medium, large and corner allotments will occur, for which built form modelling has been prepared.

With respect to areas of specific interest, the approach slightly differs, with an ‘acupuncture’ approach to new built form initiatives, to strengthen interfaces, preserve heritage values and enhance the intensity and activity of the space. These framework plans have been prepared for 5 specific precincts including: Armadale Station, the Civic Precinct, Drysdale Street Carpark, Coldbath Tram Depot and Malvern Station. Additional attention is given to Dandenong Road, where the area suffers from a poor built form image, and naturally invites a higher order of infill development, in order to position a greater population directly adjacent to Malvern Station.

A common challenge for higher intensity infill development within period tram-street shopping centres is the desire to preserve the heritage built form which makes the places unique and valued by the community. The desire in this instance is to ensure that this valuing of heritage extends beyond the immediate facade to some greater portion of the fabric of the building (including the retention of original roof forms where possible). Whilst the preferred setback of upper level form is 3m for non-contributory built form, the response to heritage conditions will defer to the requirements of the Heritage Overlay and existing policies at Clause 22.04, as distinct from the generic outcomes sought in the built form precinct diagrams.

The retention of heritage commercial forms, will also warrant some greater consideration of rear interfaces, which will have an increased role as a secondary address as well as the primary location for servicing and vehicular access. These rear areas will require further consideration of lighting, tactile materiality and wayfinding signage. Widening of laneways will be necessary to ensure their ongoing function, in order to maintain a primary active commercial frontage to High Street, Glenferrie Road and Wattletree Road.



Existing mixed use development within Study Area

Relevant Key Directions

- Provide guidance to possible future expansion of Malvern Central.
- Strengthen and Establish clearly defined strategic redevelopment sites into the Centre.
- Establish clear urban form and design guidance throughout key strategic development areas.
- Protect and enhance significant heritage fabric along Glenferrie Road and High Street.
- Encourage development to provide a secondary address to rear lanes behind Glenferrie Road.
- Encourage development to provide a secondary address to the rear at Commercial interfaces.

Objectives

- To ensure that individually significant heritage forms within the centre are given adequate regard in terms of retention of substantial fabric in any infill development scenario.
- To maintain a modest scale of infill development within High Street and Glenferrie Road, which complements the existing heritage fabric.
- To ensure that new insertions along High Street are clearly distinguished as high quality contemporary elements adding a layer of diversity to the streetwall.
- Ensure that new buildings along High Street enhances the image of the street as a boutique retail destination through high quality contemporary architecture, integrating branding and signage where appropriate.
- To ensure that new insertions within Glenferrie Road are more recessive and neutral in profile and presentation to draw attention to the primary order of existing heritage built form.
- To facilitate and focus use and development of medical aligned activities along Westliff Road to better stitch Medical uses back to the Glenferrie Road spine.
- To establish substantial growth opportunity south of the railway alignment within the Dandenong Road precinct, to enhance the image and sense of arrival to the Activity Centre at the entrance to Glenferrie Road
- Carefully manage change within significant character precincts of Claremont Avenue and Kingsway with a greater emphasis on heritage over new development opportunities.
- Ensure that development of Malvern Central is carefully managed within a vertical footprint with improved public realm interfaces.

- Should Malvern Central seek to expand over the rail alignment, ensure that this provides improved interface and connection conditions to Glenferrie Road and positive presentation from the rail corridor.

- Should the Cobble Team Depot become redundant for public transport requirements in future, seek to ensure that the significant cultural and architectural value is preserved. Integration with the surrounding street network is improved and interfaces with lower scale residential neighbours is carefully managed.

- Ensure that new built form orientated onto rear carpark provides high levels of surveillance from upper level uses, and an improved presentation.

Strategies

- Draft a local planning policy and Design and Development Overlay for land within the Activity Centre Boundary and implement through a formal Planning Scheme Amendment, so as to give effect to the findings and recommendations of the Structure Plan.

Additional Interface Design Guidance

Highway/Railway Reserve Interface

Proposed typical building profile for the 'highway/ railway reserve' interface is applicable for designated sites, away from the traditional Glenferrie Road and High Street spines. The recommended building profile acknowledges the precinct's renewal potential. For properties without an immediate sensitive residential abutment, lots with primary frontage to Dandenong Road and the railway reserve have the capacity to establish a more robust presentation of up to 4 storey streetwall.

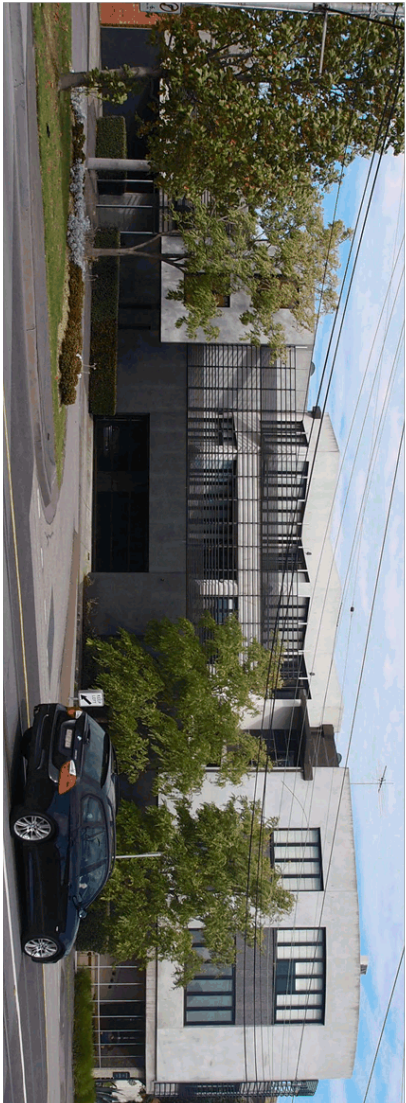
Local street interface

Side street interface is applicable for designated corner sites located throughout the Structure Plan area. The anticipated building profile acknowledges the streetwall requirements stipulated within the Glenferrie Road/ High Street interface conditions and the visual exposure along the side streets. Active frontages should turn the corner with a sense of address to the side streets. In absence of immediate sensitive residential abutment, there is capacity for upper levels to be built to boundaries where it demonstrates no adverse amenity impact onto the public realm.

Laneway/ Rear Interface

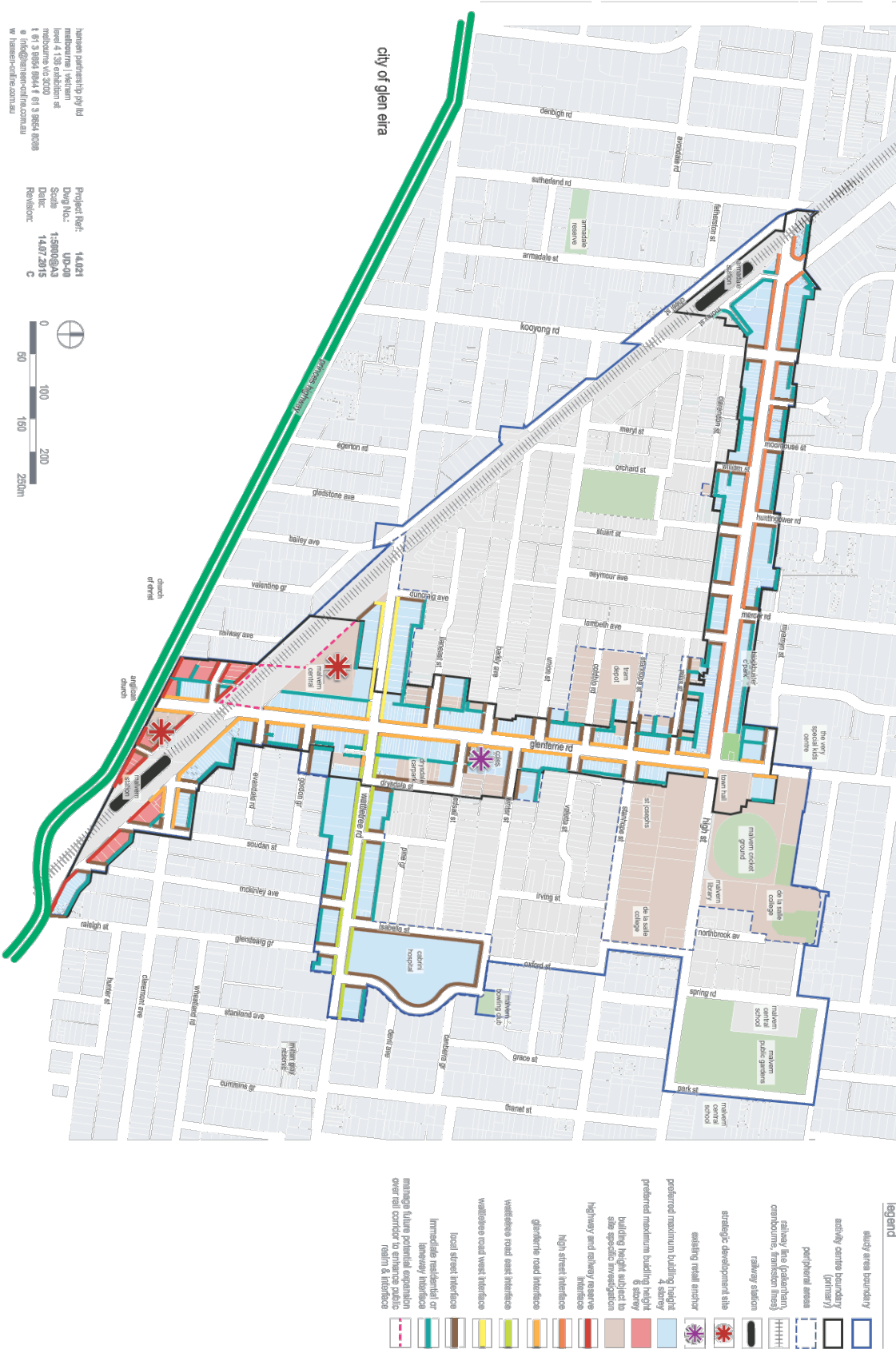
For 'rear interface' to laneway (residential/ sensitive interface), the recommended building profile incorporates a mid-block setback at the top level to allow greater separation and mitigate overwhelming sense of visual bulk. This mid-block setback does not apply to the 'rear interface' to laneway (non-residential).

Where future private open space (balconies) have a direct interface to the rear, it must not encroach beyond the recommended building envelope/ profile to maintain the integrity of building separation and minimise potential overlooking.



Existing residential development within Study Area

Figure 10 Built Form and Heritage

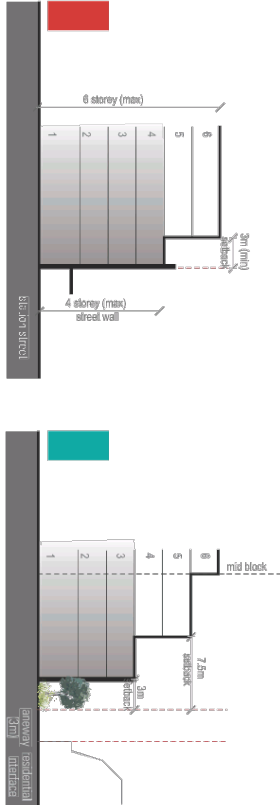


Typical Edge Condition

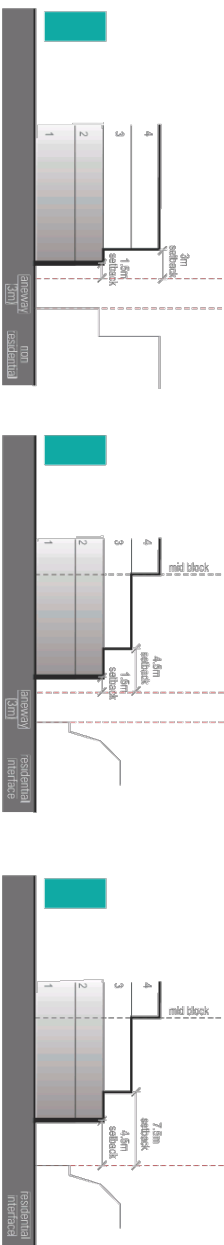
Main Spine Interfaces



Highway / Rail reserve interface



Rear/Laneway Interface



Note:

Sensitive Residential Interface: sites with an immediate abutment to a residential property (or adjacent along the rear laneway interface).

Non-Sensitive Interface: sites with an immediate abutment non-residential property (or adjacent along the rear laneway interface).

Strategic development Site: Malvern Central

The preferred building form on the Malvern Central Strategic Development Site includes the following elements:

- 1. Maximum scale of 10 to 12 storeys at the north eastern corner, stepping down to 8 storeys to the south and west (when measured from the Wattletree Road frontage).
- 2. Separated slender tower forms with a maximum width of 25 metres.
- 3. Provide separation greater than 10 metres between potential tower forms on or off site.
- 4. A 2/3 storey street wall/ podium (equivalent to 3/4 storey residential) along Wattletree Road to maintain a consistent parapet line.
- 5. Preferred setback of 12 metres from the Wattletree Road parapet line to maintain Wattletree Road streetscape presentation.
- 6. Preferred setback to other external boundaries of 5 metres (minimum).
- 7. Any development of Vic Track land to match the future parapet line of Malvern Central's rear elevation.
- 8. No overshadowing of residential allotments to the south side of the railway line, or the footpath on the eastern side of Glenferrie Road should be caused at the equinox (22 September) between 9am and 3pm.

Building scale is required to be confirmed and determined through further site specific investigations and its positive net contribution to its context. Site specific investigations will require the preparation of a detailed urban form and public realm/ urban landscape analysis to determine an appropriate built form scale and public contribution.

Recommendations regarding built form scale for Malvern Central should clearly demonstrate how proposed built form is moderated by and respects the existing and proposed built form within the surrounding locality. Site specific investigations to inform built form scale must consider:

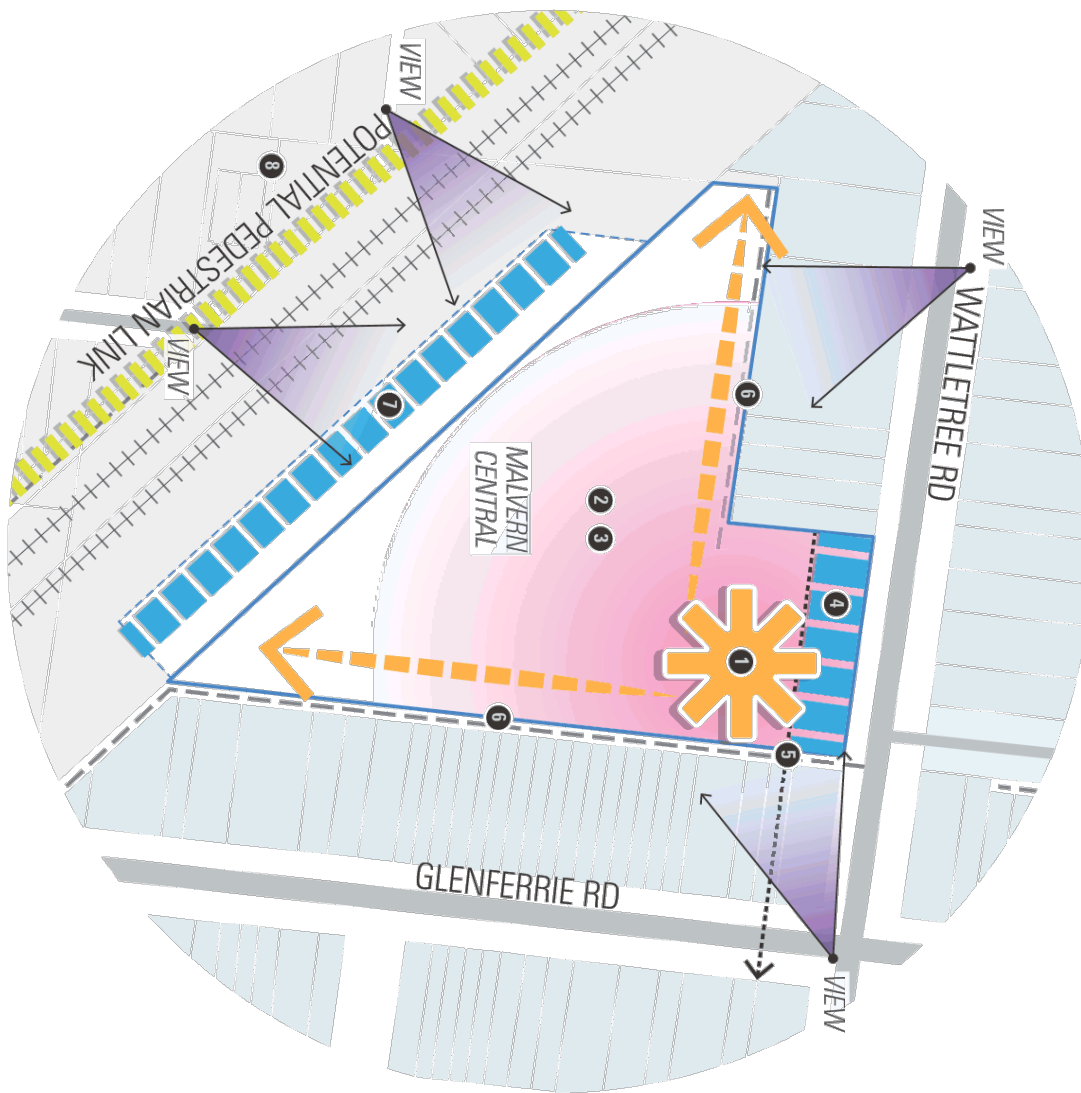
- Current built form (typology, height datum, grain and aspects) within 500 metres of the site.
- Built form recommendations as outlined within this Structure Plan, with particular reference to the 4 storey scale along Glenferrie Road and Wattletree Road.
- Built form scale associated with the preferred number of storeys to be guided by 4.0 m at ground level and 3.5 m for upper levels.

- The siting and orientation of taller built form to be centralised within the site to maintain prominence of heritage built form, particularly when viewed from Glenferrie Road.
- Immediate and longer range views of the site from public vantage points, including Glenferrie Road, Wattletree Road and the rail corridor.
- Mitigation of potential visual bulk through separated towers with varied building heights, rather than horizontal setbacks alone.
- Opportunities to improve the southern elevation of Malvern Central and ensuring façade treatments address frontage interfaces and views to the site.
- Opportunities to improve pedestrian connectivity to Glenferrie Rd and between Malvern Central and Malvern Station.
- The utilisation of roof decks of lower forms to provide communal gathering space, urban landscape and a high quality outlook from upper level commercial or residential uses.
- Initiatives and objectives associated the Malvern Central Precinct Framework Plan (Figure 21).
- Development equity for abutting/ adjacent sites to the north and east through sufficient building separation (i.e. to maximise daylight access to lower floors, managing outlooks and external aspects, and privacy matter).
- An exemplary standard of internal amenity and avoiding reliance on privacy screening, as strategic development sites should accommodate amenity matters within its own site.
- Promotion of sustainability measures including: sustainable design, sustainable transport, minimising heat island effect etc.



Malvern Central from Glenferrie Road

Malvern Central built form diagram



Strategic Development Site: Dandenong Road Gateway

The preferred building form on the Dandenong Road Gateway Strategic Development Site includes the following elements:

1. The retention of the corner heritage form (Angel Tavern) in situ, without any upper level additions.
2. Preferred maximum building height of 8 storeys (when measured from the Dandenong Road frontage) including with appropriate design and management of built form at the interface the retained corner heritage form (Angel Tavern).
3. Ability to increase building height to 10 storeys with the provision of a 3 metres ground level setback of new buildings along north and south boundaries (so as to facilitate more generous ground level pedestrian space), 9th and 10th storey required to be setback to minimise overshadowing of Dandenong Road.
4. Preferred setback for upper levels above the podium is 5 metres (minimum).
5. Provide separation greater than 10 metres between potential tower forms on or off site.

Building scale is required to be confirmed and determined through further site specific investigations and its positive net contribution to its context. Site specific investigations will require the preparation of a detailed urban form and public realm/urban landscape analysis to determine an appropriate built form scale and public contribution.

Recommendations regarding built form scale for Dandenong Road Gateway site should clearly demonstrate how proposed built form is moderated by and respects the existing and proposed built form within the surrounding locality. Site specific investigations to inform built form scale must consider:

- Current built form (typology, height datum, grain and aspects) within 500 metres of the site.
- Built form recommendations as outlined within this Structure Plan, with particular reference to the 4 storey scale along Glenferrie Road.
- Built form scale associated with the preferred number of storeys to be guided by 4.0 m at ground level and 3.5 m for upper levels.
- The siting and orientation of a slender vertical projection to be located away from the Dandenong Road and frontage to maintain prominence of the former Railway Hotel (Heritage Overlay Schedule 403).
- Potential taller form/s on this site should contribute positively to the skyline interest.
- A potential plot-ratio arrangement which balances additional floor space with ground level publicly accessible open space.
- Immediate and longer range views of the site from public vantage points, including: Dandenong Road, Glenferrie Road and Station Street.

- Ensuring potential facade treatments address frontage interfaces and views of the site, including opportunities to activate the northern facade along the pedestrian walkway to improve passive surveillance of the station environs and pedestrian connectivity to Malvern Station.
- Initiatives and objectives associated the Malvern Station Precinct Framework Plan (Figure 18).
- Development equity for abutting sites through sufficient building separation (i.e. to maximise daylight access to lower floors, managing outlooks and external aspects, and privacy matter).
- An exemplary standard of internal amenity and avoiding reliance on privacy screening, as strategic development sites should accommodate amenity matters within its own site.
- Promotion of sustainability measures including: sustainable design, sustainable transport, minimising heat island effect etc.

Strategic Development Site: Glenferrie Road Gateway

The preferred building form on the Glenferrie Road Gateway Strategic Development Site includes the following elements:

6. A podium form with a 3-4 storey base and preferred maximum building height of 8 storeys (when measured from the Glenferrie Road frontage).
7. Setback rising form behind the podium parapet line by at least 3 metres to the north and west, and at least by 5 metres to the south and east.
8. Position tallest form centrally within the site away from Glenferrie Road Railway Reserve junction and the adjacent heritage form (Angel Tavern).
9. Provide separation greater than 10 metres between potential tower forms on or off site.

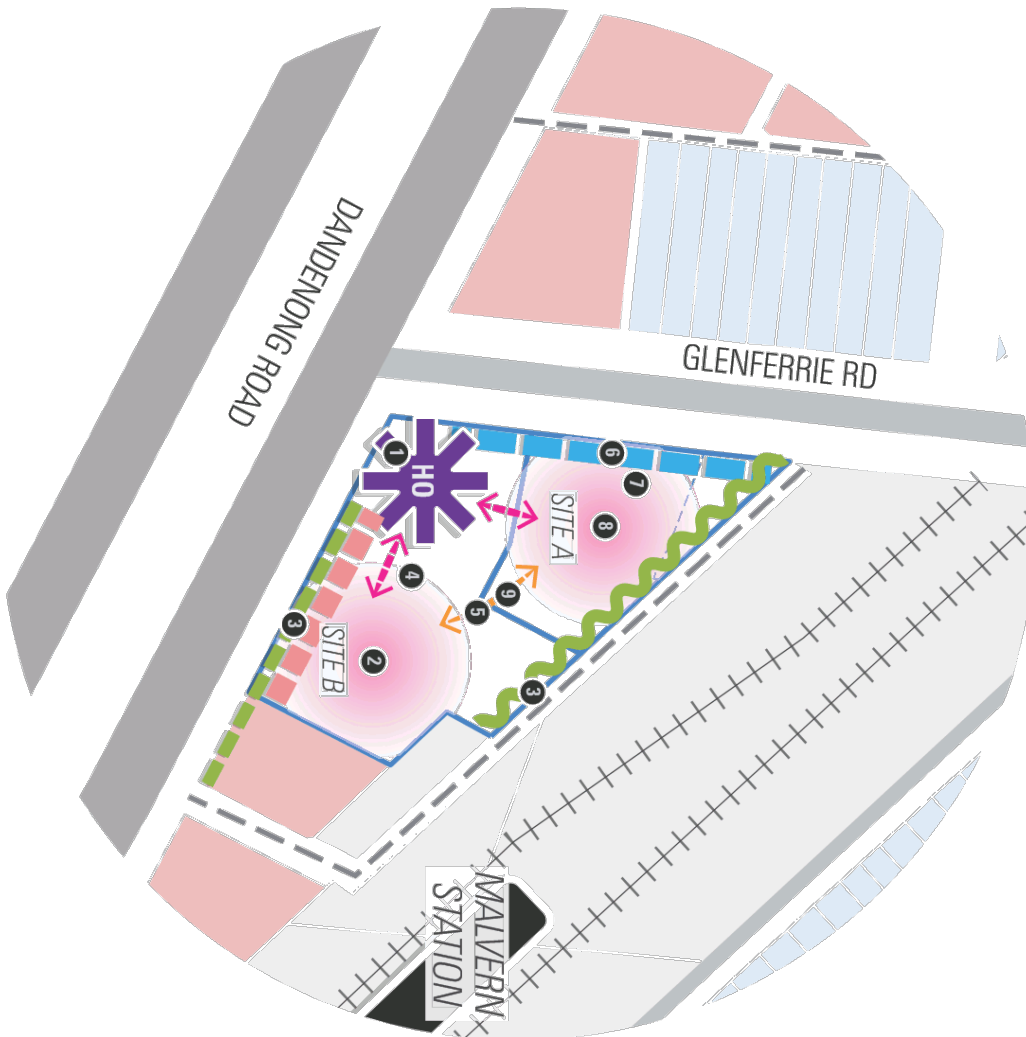
Building scale is required to be confirmed and determined through further site specific investigations and its positive net contribution to its context. Site specific investigations will require the preparation of a detailed urban form and public realm/urban landscape analysis to determine an appropriate built form scale and public contribution.

Recommendations regarding built form scale for Glenferrie Road Gateway site should clearly demonstrate how proposed built form is moderated by and respects the existing and proposed built form within the surrounding locality. Site specific investigations to inform built form scale must consider:

- Current built form (typology, height datum, grain and aspects) within 500 metres of the site.
- Built form recommendations as outlined within this Structure Plan, with particular reference to the 4 storey scale along Glenferrie Road.

- Built form scale associated with the preferred number of storeys to be guided by 4.0 m at ground level and 3.5 m for upper levels.
- The siting and orientation of a slender vertical projection to be located centrally on the site to maintain prominence of the former Railway Hotel (Heritage Overlay Schedule 403).
- Potential taller form/s on this site should contribute positively to the skyline interest.
- A potential plot-ratio arrangement which balances additional floor space with ground level publicly accessible open space.
- Immediate and longer range views of the site from public vantage points, including: Dandenong Road, Glenferrie Road and Station Street.
- Ensuring potential facade treatments address frontage interfaces and views of the site, including opportunities to activate the northern facade along the pedestrian walkway to improve passive surveillance of the station environs and pedestrian connectivity to Malvern Station.
- Initiatives and objectives associated the Malvern Station Precinct Framework Plan (Figure 18).
- Development equity for abutting sites through sufficient building separation (i.e. to maximise daylight access to lower floors, managing outlooks and external aspects, and privacy matter).
- An exemplary standard of internal amenity and avoiding reliance on privacy screening, as strategic development sites should accommodate amenity matters within its own site.
- Promotion of sustainability measures including: sustainable design, sustainable transport, minimising heat island effect etc.

Dandenong Road and Glenferrie Road Gateway built form diagram



Glenferrie Road - High Street Structure Plan | Hansen Partnership Pty Ltd

GLENFERRIE ROAD

S

ATTRIBUTES

(s) small sites are sites which have 6m street frontage (width), or less.

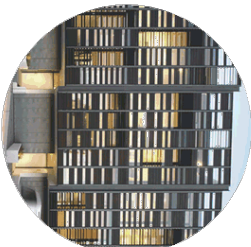
Glenferrie Road exhibits the highest level of consistency of small, fine-grained allotments due to its continued economic success since settlement. The spine between the rail cutting to the south and the Civic Precinct is perceived as a continuous set of 2 storey forms with pronounced parapets. As opposed to High Street where a greater degree of infill is anticipated, Glenferrie Road will emphasise the existing streetwall condition with any new built form set behind the parapet. New built form should seek to provide surveillance of rear laneways and incorporate softening of the laneway with tactile materiality and landscape elements where possible. Larger non-contiguous forms within the street are to be carefully managed to complement existing built form in scale and vertical rhythm whilst avoiding heritage mimicry.

GUIDELINES

- 1 maximum built form height of 4 storeys with 3 storey streetwall based on an appreciation of the established and predominantly street wall/heritage fabric, key views from the public realm within Glenferrie Road and side streets.
- 2 the preferred streetwall height of 3 storey (built to boundary) with top level setback minimum 3 m to ensure required visual openness, whilst accepting a degree of visibility for an existing building of heritage significance, refer to Clause 22.04 Heritage Policy of the Stonnington Planning Scheme (and associated reference documents) for further guidance.
- 3 primary active frontages be maintained at the ground level of Glenferrie Road with attractive frontages to the rear lane incorporating tactile materiality, signage, lighting and low level softening vegetation in planter box format to provide a secondary address where possible.
- 4 building setbacks should be simplified to provide for good building composition and avoid a 'wedding cake' profile, whilst party walling should be maintained to at least 3 levels to encourage a strong street edge profile.
- 5 a 1.5 m minimum rear lane offset is required at the ground level and first level to facilitate the ongoing function of the laneway with all vehicle access from the rear lane.
- 6 rear building profile should incorporate sufficient setback to achieve the required building separation between building lines and primary outlooks.
- 7 rear elevations should step down mid-block from 4 to 3 storeys where a sensitive interface occurs beyond the ROW (this does not apply to rear non-residential interfaces).
- 8 above the first level, rear elevations should be setback a minimum of 3m from the rear boundary for non-residential interfaces and 4.5 m from the rear boundary for residential interfaces. This is to manage building bulk, avoid a 'wedding cake' profile, and provide sufficient spatial separation to ensure good management of privacy outcomes. Balconies must be contained within their respective setbacks.



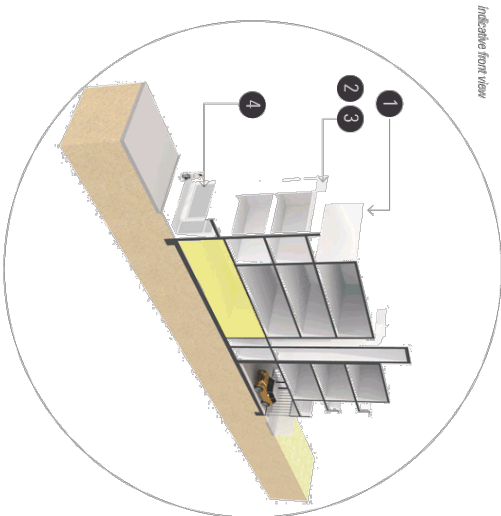
attractive rear interfaces with adequate design effects



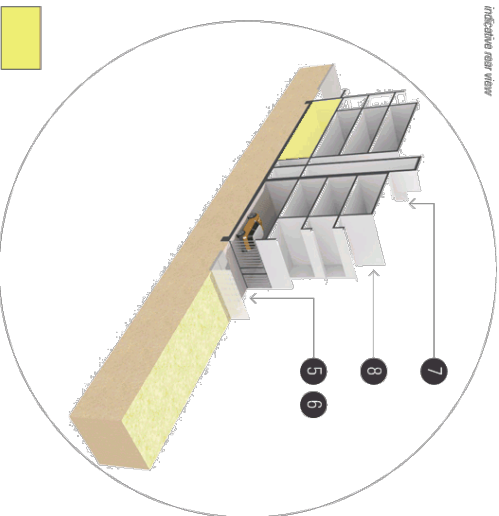
new infill building should reflect the existing parapet line



indicative front view



indicative rear view



encourage active uses at the ground level



GLENFERRIE ROAD

M/L

ATTRIBUTES

(m) medium sites are sites which have 6m-18m street frontage (width),
(l) large sites are sites which have street frontage (width) greater than 18m.

Whilst Glenferrie Road is dominated by fine-grained allotments a handful of medium or large allotments are noted which constitute medium or large allotments. These allotments include supermarkets, and mini-malls with arcades, and require positive management in terms of opportunity. As opposed to High Street where a departure in character on these larger sites is anticipated, new infill of larger allotments along Glenferrie Road will continue to emphasise the existing streetwall condition with any new built form set behind the parapet. New built form on larger sites should seek to provide a secondary address, with surveillance of rear laneways and incorporate softening of the laneway with tactile materiality and landscape elements where possible. Larger allotments should complement or instate an appropriate sense of vertical rhythm in ground and upper level treatments, whilst heritage mimicry should be avoided.

GUIDELINES

- 1 maximum built form height of 4 storeys with 3 storey streetwall based on an appreciation of the established and predominantly street wall/heritage fabric, key views from the public realm within Glenferrie Road and side streets.
- 2 the preferred streetwall height of 3 storey (front to boundary) with top level setback minimum 3m to ensure reduced visual prominence, whilst accepting a degree of flexibility for an existing building of heritage significance (refer to Clause 22.04 Heritage Policy of the Stonnington Planning Scheme (and associated reference documents)) for further guidance.
- 3 primary active frontages be maintained at the ground level of Glenferrie Road with attractive frontages to the rear lane incorporating tactile materiality, signage, lighting and low level softening vegetation in planter box format to provide a secondary address where possible.
- 4 building setbacks should be simplified to provide for good building composition and avoid a 'wedding cake' profile, whilst party walling should be maintained to at least 3 levels to encourage a strong street edge profile.
- 5 a 1.5m minimum rear lane offset is required at the ground level and first level to facilitate the ongoing function of the laneway with all vehicle access from the rear lane.

- 6 rear building profile should incorporate sufficient setback to achieve the required building separation between building lines and primary outlooks.
- 7 rear elevations should step down mid-block from 4 to 3 storeys where a sensitive interface occurs beyond the ROW (this does not apply to rear non-residential interfaces).
- 8 above the first level, rear elevations should be setback a minimum of 3m from the rear boundary for non-residential interfaces and 4.5m from the rear boundary for residential interfaces. This is to manage building bulk, avoid a 'wedding cake' profile, and provide sufficient spatial separation to ensure good management of privacy/outcomes. Balconies must be contained within their respective setbacks.



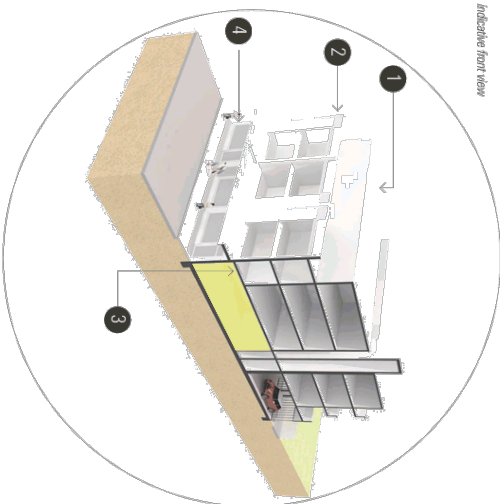
minimum typical site dimensions for a MEDIUM to LARGE allotment on Glenferrie Road



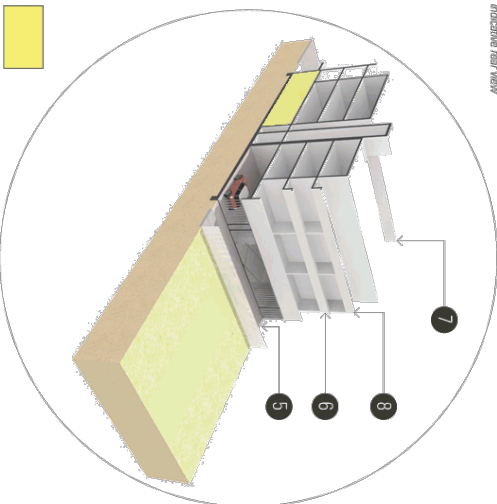
setback new built form beyond laneway fabric



contemporary infill should achieve high standards of design



indicative front view



indicative rear view

encourage active uses at the ground level

HIGH ST

S

ATTRIBUTES

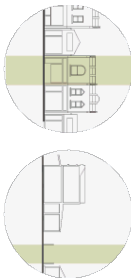
(s) small sites are sites which have 6m street frontage (width), or less.

High Street has a more varied character, influenced by a gently sloping street profile, varied parapet height and non-contiguous buildings interspersed with valued period stock. New buildings should maintain and enhance streetscape diversity, with a combination of retained heritage fabric, and marginally taller contemporary streetwalls. Narrow and moderate sites can accommodate 4 levels. Where a sensitive residential interface exists to the rear, the building should step down in height by 1 level toward the rear boundary. New buildings should maintain the sense of openness to the sky through lightweight or retractable canopy treatments. All new apartments should be oriented with adequate spatial separation from adjoining development sites in accordance with equitable development principles. Apartments oriented with primary aspect to side boundaries should be discouraged.



minimum typical site dimensions for a S44UL allotment on High Street

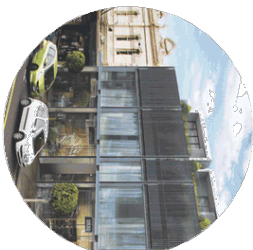
GUIDELINES



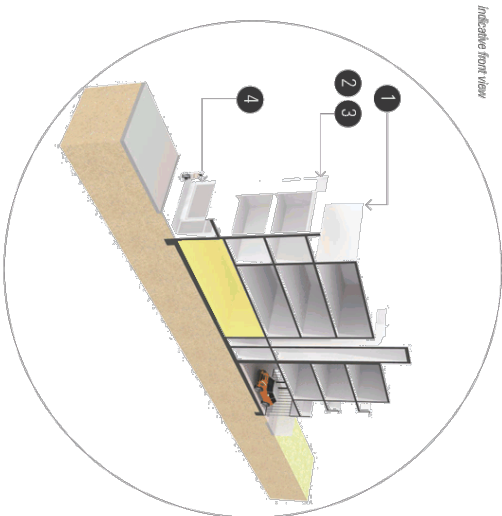
- 1 maximum built form height of 4 storeys with 3 storey streetwall based on an appreciation of the established and predominantly street wall/ heritage fabric, key views from the public realm within High Street and side streets.
- 2 the preferred setback above the streetwall is 3 m to ensure reduced visual prominence, whilst accepting a degree of visibility. For an existing building of heritage significance, refer to Clause 22.04 Heritage Policy of the Stormington Planning Scheme (and associated reference documents) for further guidance.
- 3 building setbacks should be simplified to provide for good building composition and avoid a wedding cake profile, whilst party walling should be maintained to at least 3 levels to encourage a strong street edge profile.
- 4 primary active frontages be maintained at the ground level of High Street with attractive frontages to the rear lane incorporating tactile materials, signage, lighting and low level softening vegetation in planter box format to provide a secondary address.
- 5 a 1.5 m minimum rear lane offset is required at the ground level and first level to facilitate the ongoing function of the laneway with all vehicle access from the rear lane.
- 6 rear building profile should incorporate sufficient setback to achieve the required building separation between building lines and primary outlooks.
- 7 rear elevations should step down mid-block from 4 to 3 storeys where a sensitive interface occurs beyond the ROW (this does not apply to rear non-residential interfaces).
- 8 above the first level, rear elevations should be setback a minimum of 2.0 m from the rear boundary for the residential interfaces and 4.0 m from the rear boundary for residential interfaces. This setback should avoid a mid-block profile. Buildings should maintain sufficient spatial separation to ensure good management of privacy outcomes. Balconies must be contained within their respective setbacks.



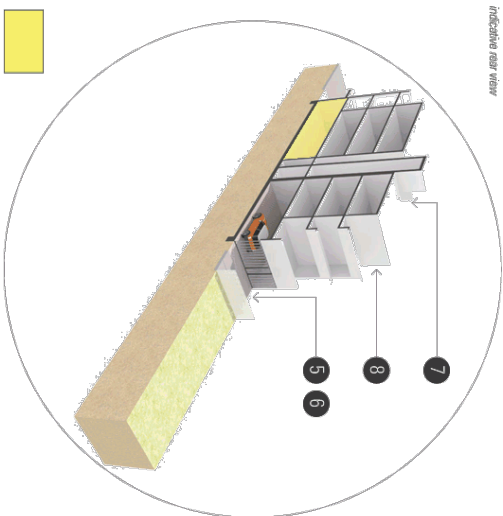
high quality architecture is permitted to project above the parapet line by 1 level in height



encourage party walls with apartments oriented front and back wherever possible



indicative front view



indicative rear view



encourage active uses at the ground level

HIGH ST

M/L

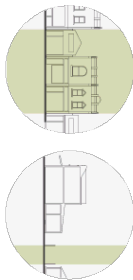
ATTRIBUTES

(m) medium sites are sites which have 6m-18m street frontage (width).
(l) large sites are sites which have street frontage (width) greater than 18m.

High Street has a more varied character, influenced by a gently sloping street profile, varied parapet height and non-contributory buildings intermingled with valued period stock. New built form on larger allotments should maintain and enhance the streetscape diversity, with a combination of retained heritage fabric, and marginally taller contemporary streetwalls. Larger allotments can accommodate a smaller 5th level envelope subject to the achievement of a high design quality. Where a sensitive residential interface exists to the rear, the building should step down in height by 1 level toward the rear boundary. New buildings should maintain the sense of openness to the sky through lightweight or retractable canopy treatments. All new apartments should be oriented with adequate spatial separation from adjoining development sites in accordance with equitable development principles. Apartments with primary aspect to side boundaries may be possible on larger allotments, so long as equitable development principles are adhered to.



GUIDELINES

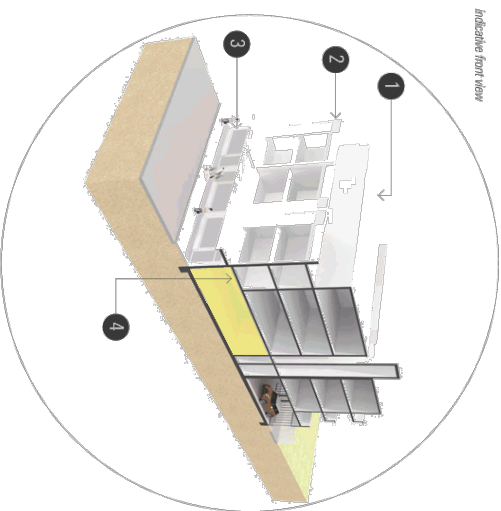


- 1 new building forms to incorporate existing heritage fabric where possible, with maximum built form height of up to 5 storeys with 3 storey streetwall to maintain key views from the public realm within High Street and side streets. A 5th level element may be warranted if a high quality design and internal amenity standard is achieved on medium to large allotments.
 - 2 preferred setback above the streetwall of 3m to ensure reduced visual prominence, whilst accepting a degree of visibility. For an existing building of heritage significance, refer to Clause 22.04 Heritage Policy of the Stonnington Planning Scheme (and associated referenced documents) for further guidance.
 - 3 building setbacks should be simplified to provide for good building composition and avoid a 'wedding cake' profile, whilst party walling should be maintained to at least 3 levels to encourage a strong street edge profile.
 - 4 allow variation in the streetwall and make provision for a parapet height discrepancy of 1 level to achieve positive diversity, except where neighbouring heritage built form necessitates a site specific design response (refer to Clause 22.04 Heritage Policy of the Stonnington Planning Scheme (and associated reference documents) for further guidance).
 - 5 primary active frontages be maintained at the ground level of High Street with attractive frontages to the rear lane incorporating tactile materials, signage, lighting and low level softening vegetation in planter box format to provide a secondary address.
 - 6 a 1.5m minimum rear lane offset is required at the ground level and first level to facilitate the ongoing function of the laneway with all vehicle access from the rear lane.
 - 7 rear building profile should incorporate sufficient setback to achieve the required building separation between building lines and primary outlooks.
 - 8 rear elevations should step down mid-block 1 level in height where a sensitive interface occurs beyond the ROW (this does not apply to rear non-residential interfaces).
- above the first level, rear elevations should be setback a minimum of 3.0 m from the rear boundary for non-residential interfaces and 4.5 m from the rear boundary for residential interfaces. This is to manage building bulk, avoid a wedding cake profile, and provide sufficient spatial separation to ensure good management of privacy outcomes. Balconies must be contained within their respective setbacks.

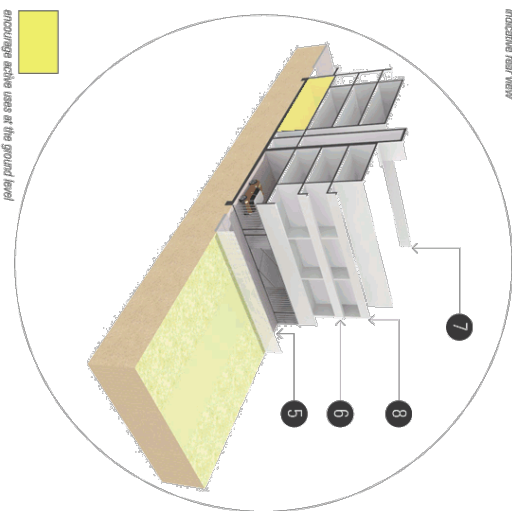


step the parapet from its height to avoid diversity

provide surveillance and a sense of address to laneways



indicative front view



indicative rear view

encourage active uses at the ground level

WATTLETREE WEST

M/L

ATTRIBUTES

Applicable on all allotments situated along Wattletree Road, on the west side of Glenferrie Road.

An eclectic precinct dominated by the Art Deco Clocktower (on Wattletree Road) and the only street frontage of Malvern Central, this precinct comprises a mix of period dwellings, contemporary infill and commercial built form consistently serviced by a ROW to the rear. Notably the majority of dwellings remaining in the precinct are in use as offices or medical facilities, some in association with Cabrini. New built form in the precinct should seek to strengthen the street edge in a distinctive way given the lack of existing parapet datum, with taller streetwalls and party-wall construction, with buildings accommodating medical and office facilities in purpose built spaces, whilst maintaining adequate streetscape activation with foyer or common areas. New buildings of an urban profile should consider their temporary exposure over lower scale built form with adequate treatment of exposed walling.

GUIDELINES



- 1 building forms to adopt a 4 story, infill streetwall to promote surveillance as well as a 3m minimum setback to accommodate shrubs and small canopy trees within the frontage to contribute to the urban forest. A 5th level possible on larger sites subject to the achievement of high design and internal amenity standards.
- 2 facilitate a combination of landscaping and active frontages, including weather protection at entrance points along Wattletree Road.
- 3 all vehicle access or secure surface parking to be maintained off rear lane where possible, except on larger allotments where parking entry from Wattletree Road is necessitated for legibility purposes for patients or visitors to medical premises.
- 4 a 1.5 m minimum rear lane offset is required at the ground level and first level to facilitate the ongoing function of the laneway with all vehicle access from the rear lane.
- 5 above the first level, rear elevations should be setback a minimum of 3.0 m from the rear boundary for non-residential interfaces and 4.5 m from the rear boundary for residential interfaces. This is to manage building bulk, avoid a wedding cake profile, and provide sufficient spatial separation to ensure good management of privacy outcomes. Balconies must be contained within their respective setbacks.
- 6 ensure that high levels of surveillance are achieved from the design of upper level apartments / tenancies to enhance the Wattletree Road street environment.



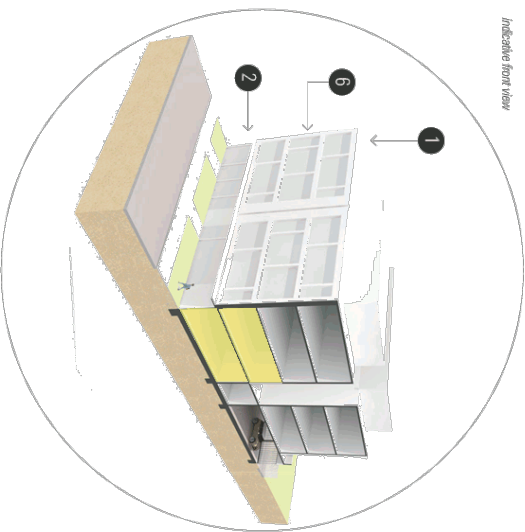
provide a taller streetwall to maximise surveillance



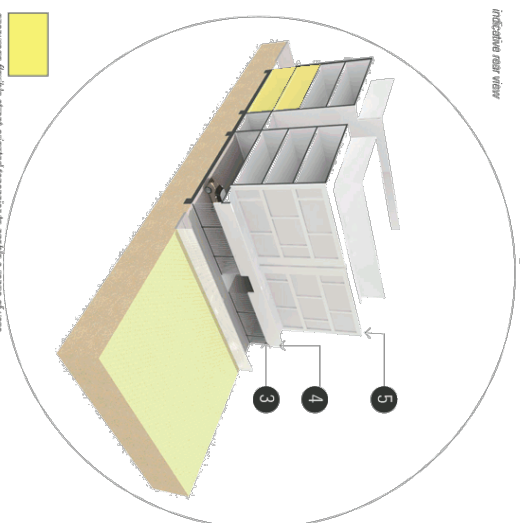
achieve a strong urban profile in new buildings



indicative front view



indicative rear view



encourage flexible street-oriented tenancies to enable a range of uses

WATTLETREE EAST

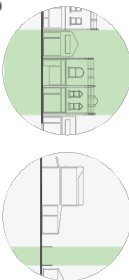
M/L

ATTRIBUTES

Applicable on all allotments situated along Wattletree Road, on the east side of Glenferrie Road.

Wattletree East is distinctive from Wattletree West due to the dominant residential character and landscape quality within the Residential Growth Zone. Whilst some infill medical built form is noted, the primary stock is a combination of semi-detached dwellings in a garden and some multi-unit development. New built form in the precinct should seek to maintain the garden setting with more intensive built form sited off at least one side boundary with a garden lane effect, whilst small front and rear setbacks are to be maintained to accommodate small tree planting. Buildings will provide for adaptable ground levels which can accommodate either medical or office facilities, or home occupation components of maisonette dwellings.

GUIDELINES



- 1 building forms to adopt a 3 storey street elevation profile positioned with a 3m minimum setback to accommodate shrubs and small canopy trees within the frontage to contribute to the urban forest.
- 2 a recessed 4th level to be treated as a 'roof form' element and distinguished in materiality.
- 3 provision of a setback to one side boundary of at least 2.5m to create a garden lane effect and streetscape rhythm.
- 4 a rear setback should be provided of sufficient dimensions to enable the planting of at least one medium canopy tree abutting the rear lane
- 5 1.5m minimum rear lane offset is required to facilitate the ongoing function of the laneway with all vehicle access or secure surface parking accessed from the rear lane except where sites with a broad frontage (> 18m) permit side access for legality of medical facilities.
- 6 above ground level, rear elevations should be setback a minimum of 4.5m from the rear boundary to manage building bulk, avoid a wedding cake profile, and provide sufficient spatial separation to ensure good management of privacy outcomes.
- 7 activate ground level frontages through medical uses, or provide planter box buffers to ground level residences.



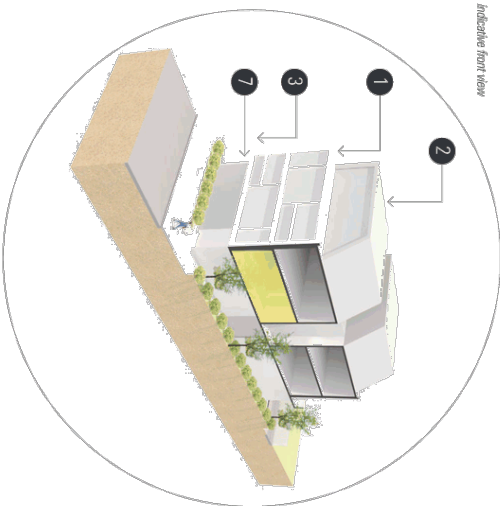
minimum typical site dimensions for a MED/LM to LARGE allotment on Wattletree Road East



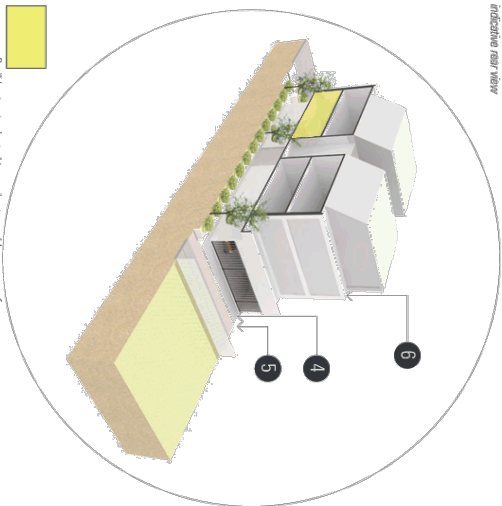
provide front setback with landscape treatments



encourage recessed upper storey forms



indicative front view



indicative rear view

encourage flexible street-oriented terraces to enable a range of uses

7 Neighbourhood Framework Plans

Precinct 1: Civic Centre



Figure 11 aerial context view



Malvern library



Terminal view along Glenferrie road to malvern town hall



Malvern square adjacent to Georgian's

Key Site Features

- Administrative and civic centre of the City of Stonnington;
- Existing function and image as the northern entry to the centre from the broad grain residential character of Glenferrie Road to the north;
- Significant heritage asset in Malvern Town Hall;
- Significant source of trips and custom in the school and other institutional buildings;
- Community orientated uses, including library, police station and public open space at Malvern Cricket Ground;
- Poor connectivity and image of back of house areas around the Malvern Cricket Ground; and
- Existing but poorly utilised public square at Malvern Square to the north western corner of Glenferrie Road and High Street junction.

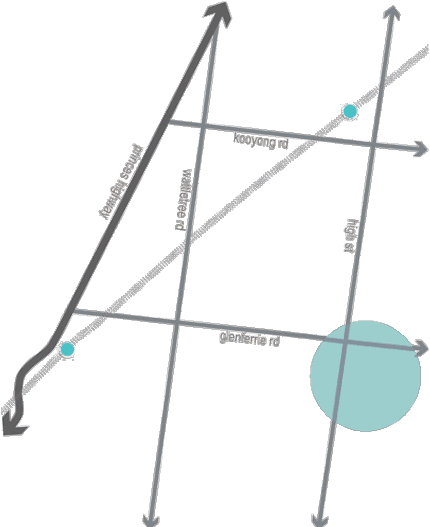


Figure 12 Precinct Identification

[illegible]

The Civic Theatre will retain and strengthen its role as the central focus for municipal, community, educational and recreational activities in Sonnington. It will be a major destination attracting visitors from within and outside the municipality. As an integrated community and civic precinct set within an attractive landscape setting and supported by high quality public open space, the precinct will present itself as inviting to the local community, employees, customers and visitors alike.

- to retain the primary focus as the municipal's centre for civic services, education and recreation, Accommodate current and future requirements for Council and Community services and facilities in a manner which provides easy and direct access for the community;
- to recognise the existing Town Hall and Clocktower as a municipal landmark and to retain its role as a prominent built form in the Centre;
- to protect key vistas to Malvern Town Hall;
- to encourage future development around Malvern Cricket Ground to activate its interface and establish a safe and attractive environment;
- to encourage activities and linkages within the precinct and establish to be more closely integrated with the Town Hall, Malvern Square and Malvern Cricket Ground;
- to encourage activities and develop the existing Malvern Square as a premier public gathering space in the Centre;
- encourage the tactful siting of new public facilities within underutilised land between existing buildings reinforce the public realm and movement corridors;
- to strengthen the 'leafy' street character along the eastern side of High Street.

Precinct 2: Armadale Station



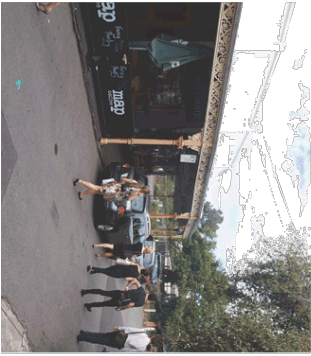
Figure 14 aerial context view



Internal view of Kings arcade



The elaborate facade of Kings arcade



Single storey period built form adjacent to the railway line

Key Site Features

- entry to the Centre from the west along High Street;
- proximity to rail, tram and bus services with a multi-modal opportunity;
- established boutique retail hub around Armadale Railway Station;
- existing Council surface car park off Morey Street;
- pedestrian access between Armadale Station and High Street via Kingsway Arcade; and
- characterised by back of house presentation and informal intimate streetscape setting.

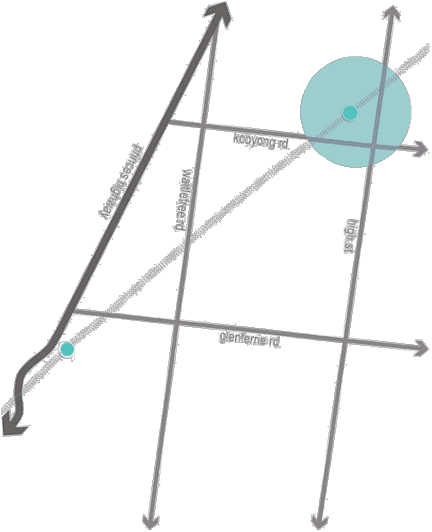
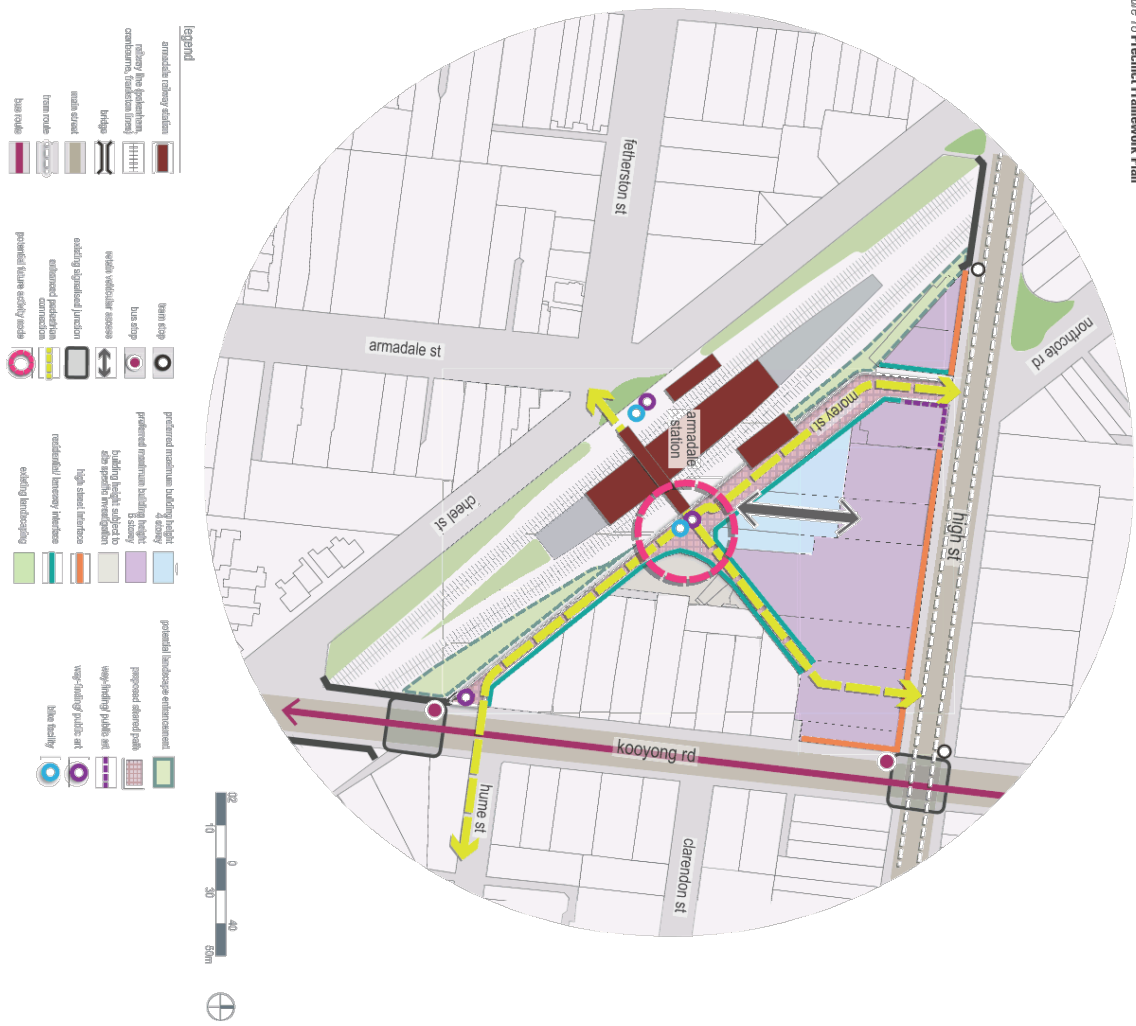


Figure 15 Precinct Identification

Figure 16 Precinct Framework Plan



Vision

Armadale Station Precinct will strengthen its role as a key attractor within the Centre and point of arrival. It will be experienced as a hidden gem offering boutique retail experience complemented by high quality pedestrian realm. The Kingsway Arcade will retain its role to provide intimate connection to High Street and Kooyong Road with improved multi-modal linkages.

Objectives

- Enhance the legibility and image of the precinct as the western entry into the Centre;
- Bolster the identity of this precinct as a unique destination associated with Armadale Station and Kingsway Arcade;
- Establish high pedestrian amenity and gathering space around Armadale Station to facilitate safe access and to accommodate safe pedestrian refuge;
- Strengthen connections through the precinct to enhance the function of multi-modal links, connecting bus, train and tram services through public realm improvements;
- Facilitate north – south pedestrian movement through improved connections to and from the heritage Armadale Station railway bridge; and
- Carefully manage any new vehicle crossovers or building services from Morey Street to prioritise a pleasant pedestrian environment.

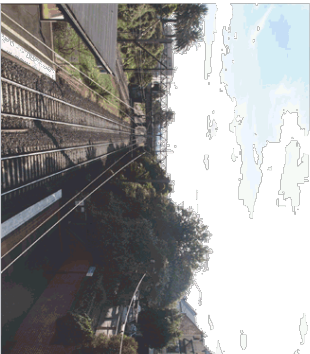
Precinct 3: Malvern Station



Figure 17 aerial context view



view down chanderls avenue from station street



view from harrington rail bridge at malvern station



public realm along station street with median planting

Key Site Features

- Historic role as an entry to the Centre from Dandenong Road to the south;
- Distinctive high-quality streetscape along Clarendon Avenue;
- Existence of higher built form along Station Street to the east of Malvern Station;
- Significant heritage station as a focal point of the precinct;
- Proximity to rail, tram and bus services;
- Prominent highway exposure to the south and more sensitive residential frontage to the north;
- underutilised public space and pedestrian access; and
- Degraded and poorly surveilled public realm south of the railway line and low quality street environment along Dandenong Road.

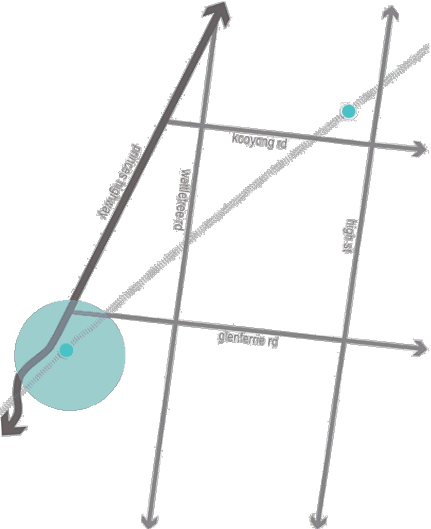


Figure 18 Precinct Identification

[illegible]

This precinct will reframe the southern approach into the Activity Centre from Dandenong Road through its built form definition at the south-eastern corner of Glenferrie Road. It will promote the establishment of a commercial hub with capacity to accommodate higher density residential for properties fronting Dandenong Road. To the north of the railway line, a mixture of uses will remain in this area, with complementary retail and creative industries retained and expanded on. Pedestrian permeability will be enhanced with a better east – west connection across Glenferrie Road, as well as north- south connection across the railway line via Malvern Station. A new shared space around Malvern Station seeks to establish a consistent pedestrian friendly environment connecting Glenferrie Road to the Station.

- Enhance the image of the precinct as the southern of the Centre and maintain its role in supporting the highest intensity of commercial and mixed use activity;
- Establish a strong sense of arrival into the Centre from Dandenong Road with the south eastern corner providing a strategic redevelopment site ;
- Retain individually significant heritage buildings and integrate intensive infill development where possible;
- Encourage active ground floor uses with potential pedestrian access along the southern side of the railway reserve;
- Clearly define desired pedestrian line of movement for pedestrian and cyclists between tram stops, railway station and key destinations, including Malvern Central and Cabrini Hospital;
- Maximise connections to the north – south pedestrian connection through the Railway Station;
- Establish high pedestrian amenity and a publicly accessible meeting/ gathering space as part of an 'activity node' to the north of Malvern Station to connect to the Station Street and Clarendon Avenue; and
- Direct higher intensity built form to the south along Handenong Road with a scale transition to more modest forms to the north, responding to the existing residential interface; and

1. Support the establishment of a 'creative' hub centred around Clarendon Street and Station Street, including potential street market or special events.

Precinct 4: Malvern Central



Figure 20 aerial context view



Landmark art Deco clocktower on wattle tree road



Positive street integration of malvern central at wattle tree road



Laneway interface between malvern central and glenferrie road shops

Key Site Features

- Significant destination within the Centre, with Malvern Central providing a significant retail offering;
- Strong association with Cabrini Hospital to the east and the Wattle Tree Road spine;
- Poor integration, connectivity and legibility between the precinct and Malvern Station;
- Broad underutilised public depot land adjacent to the rail alignment;
- Car dominated environment due to the arrangement of ramping vehicle circulation into Malvern Central; and
- Mixed built form image along Wattle Tree Road, clearly distinct from the consistent character of Glenferrie Road.

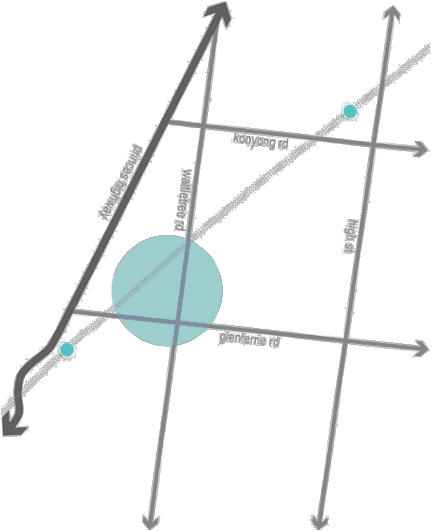
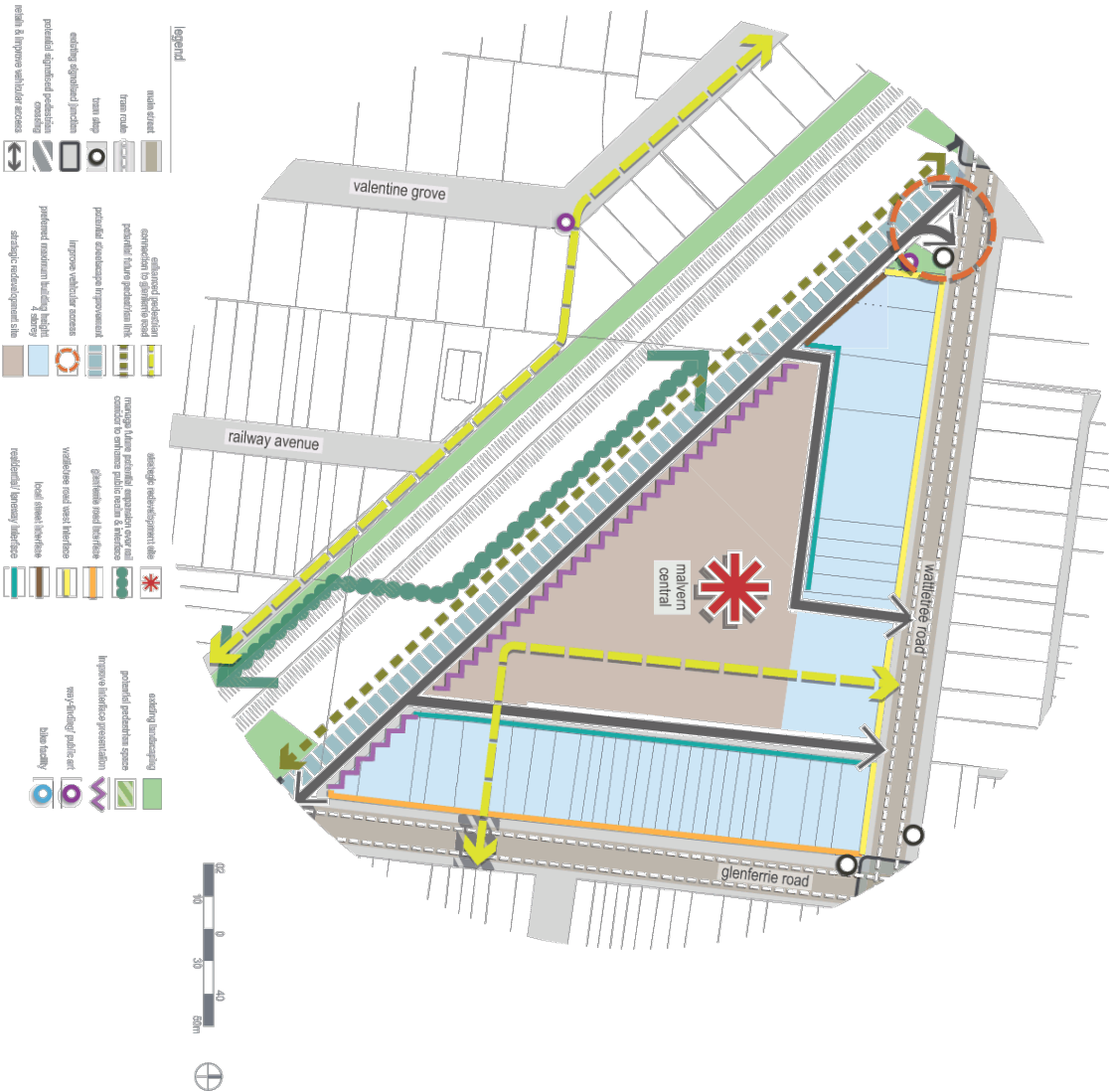


Figure 21 Precinct Identification

Figure 22 Precinct Framework Plan



Vision

The Malvern Central precinct will strengthen its role as the Centre's regional shopping destination. Improved connections will be established to surrounding areas, including to Glenferrie Road and Malvern Station. This will be achieved through way-finding and public realm upgrades to enhance its legibility within the broader activity centre. Future redevelopment of this precinct will accommodate a higher intensity of residential accommodation in a mixed-use format, which will consider the heritage significance of period commercial forms fronting Glenferrie Road.

Objectives

- to achieve a high quality mixed use precinct including retail, office, residential and entertainment to establish a vibrant living and working environment;
- to enhance ground floor retail and pedestrian generated activities along Wattleree and Glenferrie Roads;
- manage positive future expansion of Malvern Central within a vertical footprint;
- explore opportunity for new connections between Malvern Central and Malvern Station via Glenferrie Road with integral public realm and landscape treatment;
- increase the intensity of built form within Wattleree West in response to the reduced heritage values;
- explore opportunities for a shared pedestrian and cyclist path along the northern edge of rail alignment generally between Malvern and Armadale Station framed by new built form to provide surveillance;
- explore opportunities for landscape works along the rail corridor to improve the image of the public works depot;
- to rationalise car park entry into Malvern Central to facilitate safer pedestrian movement; and
- to improve visual presentation of Malvern Central's southern elevation;
- to ensure potential development over the railway corridor considers the community benefit and opportunity for public open space.

Precinct 5: Coldblo Tram Depot



Figure 23 aerial context view



View along Stanhope street to Glenferrie road



View into coldblo tram depot from the entry



View along coldblo street from the residential hinterland to the west

Key Site Features

- Significant cultural and architectural heritage asset in the historical development of the Centre, with accompanying but underappreciated Tramway Museum on Stanhope Street;
- Ongoing function as a public infrastructure depot with complex vehicle and tram movement patterns;
- Strong connection and potential exposure to the Glenferrie Road spine;
- Limited public permeability east-west through the urban block;
- Centrally located along the Glenferrie Road spine, surrounded by traditional residential dwellings to the north, west and south; and
- Expansive car park presentation along Stanhope Street.

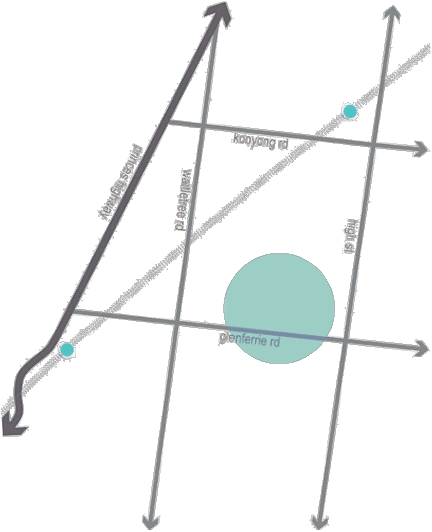


Figure 24 Precinct Identification



The Caddo Train depot will retain its infrastructural role with opportunities for temporary or incremental provision of public access to the site through festivals, open days, or some other regular public event. It will foster community interest through facilitating visual and physical connections and to open up the facility to a wider audience. Future redevelopment of the Train depot will emphasize the adaptive re-use of its existing high valued built forms.

- Explore opportunities for greater appreciation of the heritage buildings within the Coldbro Farm Depot and adjacent Trianway Museum.
- Acknowledge opportunities for temporary community or trader group lead events in key areas of the site.
- Encourage improvement within and to the edges of the precinct, through landscaping and public realm improvements connecting to Glenferrie Road.
- To improve the Coldbro Street and Glenferrie Road junction and its role in facilitating safe and efficient access for all modes of transport.
- To improve pedestrian and cyclist connections throughout the precinct in particular along Union Street and Barkley Street; and
- Ensure that any future disposal of the Coldbro Farm Depot complex is carefully managed through the application of planning and design controls to protect heritage and integrate with the surrounding neighbourhood.

Precinct 6: Drysdale Street Car Park



Figure 26 aerial context view



view of consistent street wall along Glenferrie road



view through a post-war arcade to rear car parking



view to multi-level car park behind Glenferrie road spine

Key Site Features

- Contains key retail anchors, which provide for local convenience shopping within the broader Centre context;
- Important source of custom and point of arrival to the Glenferrie Road local retail strip;
- Community facilities located within the threshold space between retail strip to the west and residential hinterland to the west;
- Proximity to Cabrini Hospital with direct linkages along Winter and Edsall Streets;
- Significant landholding acts as a buffer between residential land and Glenferrie Road commercial built form;
- Back of house presentation along the western end of Drysdale Street;
- Vehicle dominated environment with low levels of pedestrian amenity;
- Connection to Glenferrie Road is via Winter and Edsall Streets, as well as a privately owned arcade; and
- Limited provision of public realm elements such as lighting and pedestrian refuge zones.

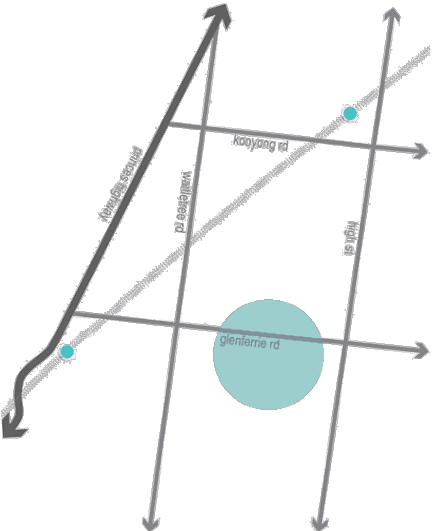
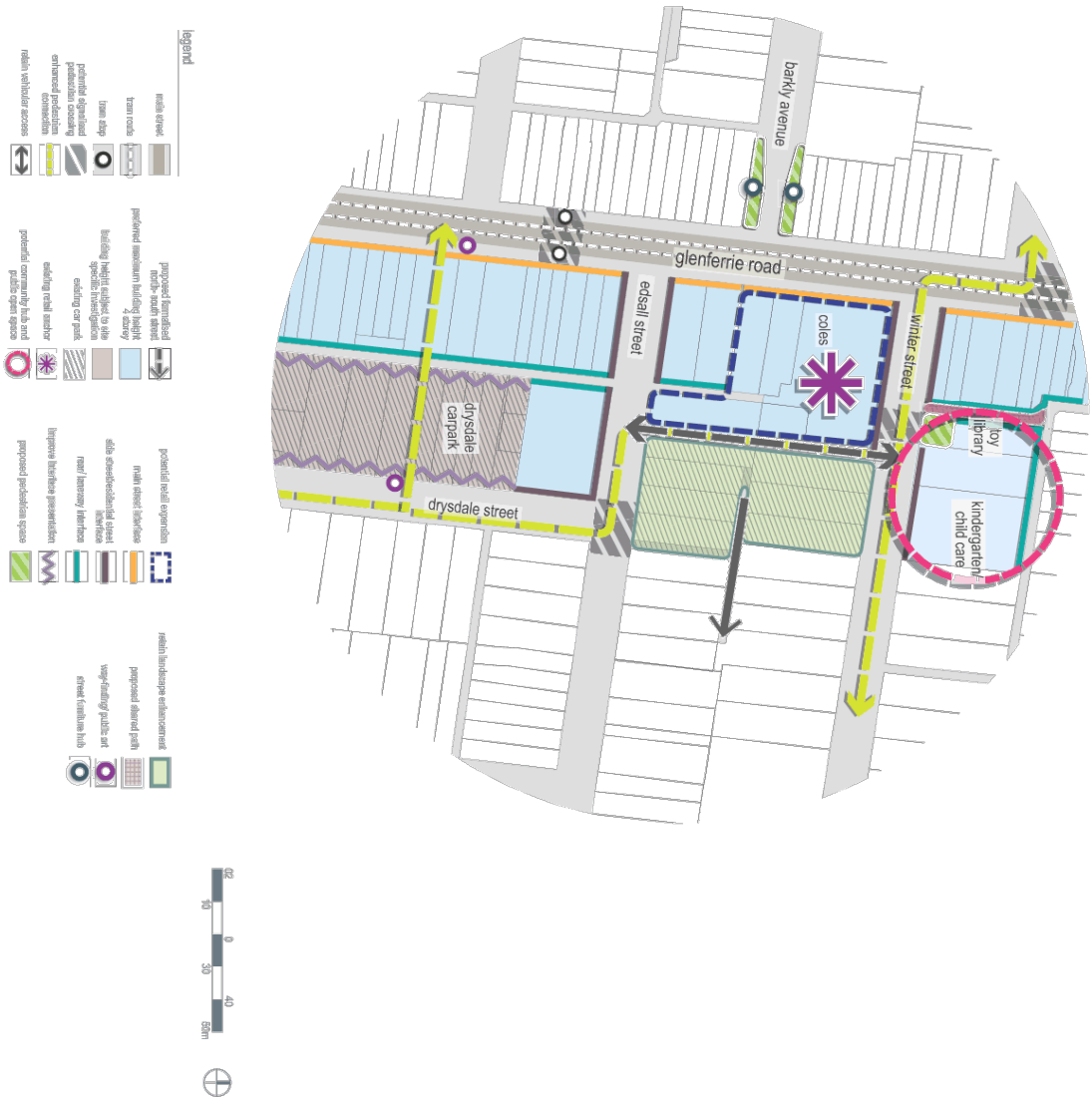


Figure 27 Precinct Identification

Figure 26 Precinct Framework Plan



Vision

Opportunities exist for the anchor retail tenancy to expand and complement the vibrant, pedestrian – focused, mixed-use precinct. It will accommodate a range of retail offer, community facilities, and car parking in addition to modest development in shop-top format. New built form will provide active and attractive presentation to the public laneways and car park to the rear. Re-configuration of the Winter Street car park to introduce dedicated pedestrian paths to minimise potential conflicts. A new public open space will be accommodated around the existing Toy Library to provide a pedestrian refuge and resting area away from the busy main streets.

Objectives

- To reinforce and enhance the role of retail anchor tenancy in this precinct;
- To achieve an enhanced presence of community facilities including the upgrade of the Toy Library and to establish a public space at and around the community facility;
- To encourage shop-top infill development along Glenferrie Road with appropriate presentation to the car park;
- Manage positive expansion of the anchor retail tenancies vertically, and ensure the protection of period commercial forms further to the south;
- Encourage reinstatement of rhythmic active frontage treatments to Drysdale Street and Glenferrie Road, with attractive frontages to the car park interface;
- Intensify built form scale in the back-of-house area to provide surveillance an improved interface to the car park;
- Simplify and improve the quality of north-south pedestrian, bicycle and vehicle access through the at-grade car park including footpath by 20m minimum width along edge of retail tenancy; and
- Incorporate public realm improvements within the surface parking area to enhance the sense of place of an important arrival point to the Centre.

9 Implementation

How will the Structure Plan be implemented?

The implementation of this Structure Plan will occur progressively over a long time period (10-25 years), and will require careful on-going management and continued communication with the local community, business owners and landowners. The co-ordination across a wide range of stakeholders and funding partners is necessary to ensure a productive approach to the development of this important Centre and improvements in keeping with the valuable role the centre plays within the City of Stonnington.

The Glenferrie Road / High Street Activity Centre Structure Plan outlines a series of objectives and strategies that will serve to enhance the character, image and potential of the centre. Key actions, which are directly drawn from stated strategies, will be prepared once the broader directions of the Structure Plan have been considered by the community. It is these actions which will follow the formulation of this document which will be critical to the implementation of this Plan. While many immediate actions will be the responsibility of departments within Stonnington City Council, initiatives will also need to be pursued by other government agencies and landowners as indicated in the tables.

While the Council's lead role in review of development proposals is critical, it is also acknowledged that the centre has a strong history of community engagement and involvement through associations such as the traders associations for Glenferrie Road and High Streets respectively. These groups will all have opportunities to influence the implementation of this plan and their support for the document will add weight and urgency to the actions contained within it.

In addition, the role of private and public organisations in facilitating the development of many of these ideas should not be underestimated through the use of public-private partnerships where appropriate. Where these are supported by the plan, Council has a role to play in facilitating the plans outcomes.

As with many Activity Centres, the Structure Plan area has a basic zoning structure that includes a strip of commercial zoned land surrounded by residential land, with little guidance provided to balance the existing State Planning Policy. The existing policy regarding development in Activity Centres has been developed on planning principles which seek to consolidate increased populations and intensity of development where there are existing nodes of activity, which as is the case with the Glenferrie Road / High Street Activity Centre.

Providing increased densities where there is excellent access to services, facilities and transport also makes for a more equitable and environmentally sustainable society. However, it is important to recognise the local conditions and to ensure that how this density is accommodated actually reflects the existing conditions, particularly within an Activity Centre with such a strong local character. By allowing this Structure Plan to carefully consider and identify areas which will be suitable to accommodate modest increases in density and height the position of protecting other areas within the centre from more intensive development is strengthened.

The first and critical step in realising the objectives of the Structure Plan is the establishment of appropriate planning controls to implement the overarching directions of the Structure Plan, which involves a range of implementation options, including: local policies and specific design controls which require formal implementation into the Stonnington Planning Scheme.

Once the Structure Plan and the associated recommendations for an appropriate suite of local policies and design controls has been considered and formally adopted by Council, it will be necessary to facilitate a Planning Scheme Amendment to formally incorporate the recommendations into the Stonnington Planning Scheme.

Finally, it must be accepted that changes in the centre will unfold over a long time period and the needs and aspirations of the community will change over time. Therefore the Structure Plan must be flexible enough to be adapted to changing circumstances, be they relating to economic, social or environmental factors. Continued monitoring and evaluation of the Structure Plan must be undertaken by the Council to ensure that it remains relevant to current practice and community expectation.

Funding Strategies

The Structure Plan requires a funding strategy to outline the means by which concepts may be implemented over time. Stonnington City Council will need to carefully consider how this will occur as part of the broader funding strategies. The role of a funding strategy will be to identify sources from which funds will be derived for this particular project. A funding strategy will need to look at all income sources and the potential of different sources to contribute to the total cost of implementing the plan. Importantly, in the Glenferrie Road / High Street context, the on-going partnership and collaboration between Council and the many active community associations and other agencies operating in the area will be crucial to identifying and accessing a broad range of funding opportunities to achieve the aims of this plan. Other elements of the funding equation are likely to include the following:

Funding from General Rates - The ability of the Council to fund public works from general rates is limited by budgetary constraints and the current commitment of funds for existing major capital works projects. However, in order to demonstrate its commitment to civic improvements in this important local centre, the Council should to provide some funds from general revenue. This could be either for specific projects, or on a dollar for dollar basis (or some other proportion) to match funds derived from other sources.

State Government Funds - The State Government makes funds available to local government for public works and urban improvements. This document will provide the Council with strong strategic justification to support applications to the State Government for project funds as they become available. It is also noted that some elements of this Structure Plan, such as cycling infrastructure, is outside of the control of Council and needs to be actioned by State Government agencies (eg. VicRoads).

Special Charges - The levying of a special charge to cover the cost of works would be an option for generating funds for works. In situation where existing property owners and businesses benefit (such as the provision of road upgrades or consolidated car parking opportunities for example). A special charge could also be levied to cover the administrative and operational costs of any implementation body established to implement the plan. Political and equity considerations need to be taken into account in deciding to introduce a special charge.

Other Sources - A series of new and innovative approaches to development funding are also available, including the opportunities for 'partnership projects' that demonstrate collaboration and joint venturing between the private and public sector. In addition, funding is available through agencies such as Sustainability Victoria and other organisations that focus specific areas. More standard public / private partnerships could also be pursued by Council to achieve some key identified projects.

Other Actions

In addition to implementing the Structure Plan through the Stonnington Planning Scheme, a number of other actions should be initiated by Stonnington City Council, other authorities and local stakeholders to achieve the 'vision' of this plan. There is also further work which this plan has identified which will need to be undertaken before particular strategies can be pursued. These are also outlined on the following page. As mentioned in the introduction to this section, the development of the initiatives identified above will happen over a period of time as developments are proposed and funding becomes available. Priorities may also change over time, as selected initiatives of the plan become more critical or of greater importance to the Glenferrie Road / High Street community. To assist in the process, however, a number of priorities have been identified which should be the focus for the pursuit of funding. The resolution of some more specific areas through a process of more detailed concept design is also highlighted.

Identified actions will require the cooperation of a number of different parties including landowners and other government authorities. Prioritising the actions allows not only for the key projects to be highlighted but for additional weight to be added when seeking funding for specific projects identified as priorities. As opportunities for funding become available this Structure Plan will demonstrate the strategic importance of noted projects. The following timeframes have been adopted for this project (immediate, as soon as possible, short term, 2015 - 2020, medium term, 2020 - 2025 and long term, 2025-2035). The table outlining these actions can be found on the following page.



10 Action Plan

action table		council responsibility		external / groups	priority
#	action				
1	Identify an opportunity for the two business associations to work together on a project which aims to strengthen the identity of the activity centre while also celebrating the differences between the two 'arms' of High Street and Glenferrie Rd. (this could be a masterplan)	Public Spaces & Capital Works		Traders Associations, local business owners	Short Term
2	Set up working group with Cabrini Hospital and allied health care providers to establish further needs or progress a study, with respect to expansion of medical facilities along the designated Waitletree Road medical related spine.	City Strategy, Economic & Cultural Development, Aged Diversity & Health		Cabrini Hospital, health care providers	Short Term
3	Initiate additional contact with Malvern Central Management regarding pedestrian access upgrades and future redevelopment potential of Malvern Central.	City Strategy, Statutory Planning, Economic & Cultural Development		Malvern Central management	Short Term
4	Undertake detailed design to facilitate and implement upgrades to the public realm within the centre in line with the Structure Plan and with a focus on the pedestrian priority areas, key public nodes and the key movement corridors.	Environment and Public Spaces, Infrastructure Services, Parks, Environment and Buildings, Public Spaces & Capital Works		VicRoads, Traders Groups, local business owners	Short to Medium Term
5	Investigate traffic yield control treatments (textured surface, speed bumps or raised thresholds) at the identified 'pedestrian safety' road areas.	City Strategy, Public Spaces & Capital Works, Transport & Parking.		VicRoads	Medium Term
6	Draft new Local Planning Policy and Design and Development Overlays as part of an amendment to the Stonnington Planning Scheme to implement the recommendations of the Structure Plan.	City Strategy, Statutory Planning			Short Term
7	Work with VicRoads to introduce on-road cycle lanes, road markings, wayfinding signage, cycle and pedestrian facilities and green bicycle boxes at traffic lights within existing street network.	Transport & Parking		VicRoads	Short Term
8	Provide additional bicycle storage facilities in areas of high demand, including at community facilities, parks, and in the vicinity of cafes.	Transport & Parking and Parks, Environment & Buildings		VicRoads	Short Term
9	Commence a detailed review of the community node surrounding Malvern Library and investigate measures to improve pedestrian provision and connectivity with the adjacent Malvern Cricket Ground and the nearby Malvern Town Hall / Town Square.	City Strategy, Public Spaces & Capital Works, Leisure & Libraries			Medium Term
10	Develop a strategy for the intersection of High Street and Glenferrie Road to become the symbolic 'heart' of the centre.	City Strategy, Leisure, Infrastructure Services		VicRoads, Yarra Trams	Short to Medium Term
11	Investigate the provision of a signalised pedestrian crossing on Glenferrie Road between the traffic signals at High Street and the signalised crossing south of Edsall Street to improve pedestrian crossing opportunities within this strip.	Transport and Parking		VicRoads	Medium Term
12	Investigating a new link along the railway line adjacent Malvern Central.	City Strategy, Transport and Parking, Public Spaces & Capital Works		VicTrack, Malvern Central	Medium Term
13	Investigate the benefits of an eastbound evening peak hour tram lane on High Street to coincide with the existing clearway hours of operation.	Infrastructure Services, Transport and Parking		Yarra Trams, Vic Roads	Medium Term
14	Investigate a 'shared space' where all road users utilise the same area to reduce the dominance of motor vehicles in the predominantly pedestrian area of Money Street and Kings Way in the vicinity of Armande Station.	City Strategy, Transport and Parking, Public Spaces & Capital Works		PTV and VicTrack	Medium Term
15	Investigate the implementation of a 40 km/h speed limit through all streets within the Centre.	Transport and Parking		VicRoads, Yarra Trams	Short Term
16	Investigate a redesign of the intersection between Llanest Street car park and Waitletree Road to improve operation and amenity (potentially by converting the Llanest car park access to entry only and providing a footpath).	Transport and Parking, Public Spaces & Capital Works		VicRoads	Medium Term
17	Set up a working group with VicTrack seeking opportunity to provide aesthetic landscaping within the rail corridor.	City Strategy, Transport & Parking and Public Spaces & Capital Works		VicTrack	
18	Develop a set of 'public realm' guidelines for an identified style of street furniture, wayfinding signage and other features to be utilised throughout the Study Area. The design of all elements of street furniture should reflect the identified branding for the centre.	City Strategy, Economic and Cultural Development, Public Spaces & Capital Works		VicRoads	Short Term
19	Develop guidelines for vegetation / tree species selection for use in public realm works throughout the Study Area	Parks, Environment & Buildings		VicRoad, Yarra Trams	Short Term
20	Undertake feasibility of Winter Street community hub and public open spaces	City Strategy, Economic & Cultural Development, Children & Family Services, Public Spaces & Capital Works			Short Term

Figure 29 Implementation Action Plan

